

FHWA-AL-EIS-19-01-SD

# **SUPPLEMENTAL DRAFT ENVIRONMENTAL IMPACT STATEMENT**

**PROJECT NO. DPI-0030(005)  
I-10 MOBILE RIVER BRIDGE AND BAYWAY  
MOBILE AND BALDWIN COUNTIES, ALABAMA**



**U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION  
AND  
ALABAMA DEPARTMENT OF TRANSPORTATION**

**IN COOPERATION WITH:  
U.S. Army Corps of Engineers, Mobile District  
and  
U.S. Coast Guard, Eighth District**

**VOLUME III: APPENDICES L-P**

**March 2019**

# **APPENDIX L**

## **Section 106 Consulting Party Consultation**





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# **APPENDIX L-1:**

## **Consultation on Determination of Effects**

## **Consultation on Determination of Effects**



STATE OF ALABAMA  
ALABAMA HISTORICAL COMMISSION  
468 SOUTH PERRY STREET  
MONTGOMERY, ALABAMA 36130-0900

FRANK W. WHITE  
EXECUTIVE DIRECTOR

June 30, 2014

TEL: 334-242-3184  
FAX: 334-240-3477

Ms. Alfredo Acoff  
Environmental Technical Section  
Alabama Department of Transportation  
P.O. Box 303050  
Montgomery, Alabama 36130-3050

RE: Determination of Effects AHC 00-1934 and 00-0352 *Cultural Resources Assessment ALDOT Project: DPI-0030 (005) I-10 Mobile River Bridge and Bayway Widening, Mobile and Baldwin Counties*

Dear Ms. Acoff:

Thank you for your continued efforts to coordinate with this office regarding the above-referenced project. We understand the great complexities of this project and appreciate your willingness to work with us through the Section 106 process.

We agree with ALDOT's finding for no adverse visual effect to the following: Old Dauphin Way Historic District, Campground Historic District / MLK Heritage Neighborhood, Oakdale Historic District, Maysville Historic District, USS Alabama, and BAE Maritime Historic District.

We are concerned about the potential visual impacts of the proposed bridge to certain historic areas in downtown Mobile, specifically at locations 26, 27, 32, 44, 45, 46, 48, 49, and 51. However, we request ALDOT to explore opportunities to incorporate context sensitive design features and a reasonable tree planting plan to minimize any potential visual impacts on historic resources.

We also understand that ALDOT is committed to address lighting, vibrations, bridge aesthetics, and access to Battleship Park in future agreements with SHPO and other consulting parties.

While we are concerned with potential impacts to the Union Hall, which is eligible for the National Register under Criterion A for its association with Alabama's labor history, we understand that ALDOT has documented the property according to HABS standards. We also request that ALDOT pursue a contextual study of the history of organized labor in Alabama that includes the number of extant union halls in the state.

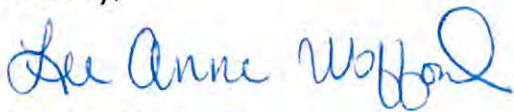
Regarding archaeology, sites where landowner permission was not acquired for the Phase I survey must be addressed with a Phase I survey when a route is selected and the site acquired. Mobile is a city with many layers of occupation that may be very deep in the area proposed for the bridge. When sites where buildings now sit that become a part of this project, demolition must take place in a manner that preserves any

Ms. Alfredo Acoff  
June 30, 2014  
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archaeology that may be present on the site. Properties determined potentially eligible for the NRHP will require the development of Phase II testing and prior approval from SHPO before any further work occurs. ALDOT should avoid properties determined eligible for the NRHP, but if that is not possible, then ALDOT/SHPO must agree on mitigation strategies.

We appreciate your commitment to helping us preserve Alabama's cultural resources. Should you have any questions, please contact Lee Anne Wofford at 334.230.2659 or [LeeAnne.Wofford@preserveala.org](mailto:LeeAnne.Wofford@preserveala.org). Please have the AHC tracking number referenced above available and include it with any correspondence.

Sincerely,



Lee Anne Wofford  
Deputy State Historic Preservation Officer





## Mobile Historic Development Commission

P.O. Box 1827  
Mobile, AL 36633-1827



July 1, 2014

Ms. Heather Dunn  
Alabama Department of Transportation  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, AL 36110



Dear Ms. Dunn:

On behalf of the Mobile Historic Development Commission (MHDC), I would like to thank ALDOT for the work that has been done on the EIS for the I10 River Bridge. We are particularly appreciative of the Phase 3 archaeological investigations currently underway.

The MHDC would like to voice its concerns with the finding of no adverse affect. However, the Commission also recognizes the need for the bridge and desires to work with ALDOT and all Consulting Parties to move the project forward.

With the bridge still to be designed, the Commissioners believe that most of its concerns could be handled through a programmatic agreement that will allow the bridge to proceed. These are:

- The Commission is concerned with the effect of the construction on the historic resources in the area and believes a monitoring system should be implemented. This would be a direct effect that could possibly harm the physical structures.
- The Commission is also concerned about environmental issues that would diminish the integrity of various historic resources' location: noise; stormwater; and particularly lighting.
- There is also concern about the above effects on the historic resources during construction and the Commission would like to see the plan for minimizing these.
- The Commission believes the viewshed will be compromised particularly where open spaces or vistas occur and with multi-story buildings, thus diminishing the integrity of the various historic resources' setting and feeling. A judicious landscaping plan scattered through the nearby historic districts could alleviate some of this effect.



The Commission is of the opinion that the design of the bridge will have a significant impact on the City as a whole including the nearby historic districts. As the most visible structure in the City it will become a symbol of Mobile. The MHDC looks forward to working with ALDOT during the design phase. The Commission does note that:

- The overall design of the bridge must be architecturally appealing.
- Since the location of the bridge will be in one of the most historic parts of the city in the area first settled by Europeans, the treatment of how the large area below the bridge is handled should be of great concern.

The Commission believes a formal document such as a programmatic agreement would be of benefit to all concerned parties. By allowing the Commission and other Consulting Parties to be part of the design process, many of the concerns expressed in this letter could be minimized.

The Mobile Historic Development Commission looks forward to working with ALDOT and believes that together Mobile can have a bridge that everyone is proud of.

Sincerely,



Jean Dodge  
President

**From:** Herndon Inge III [<mailto:hinge@herndoninge.com>]  
**Sent:** Monday, July 07, 2014 2:41 PM  
**To:** Dunn, Heather M.  
**Subject:** Mobile River crossing- ALDOT Project DPI-0030(005)

Ms. Dunn,

I disagree that the Project will have "No" adverse indirect effect or direct use. The visual impact, disruption during construction, noise, vibration during construction and after construction, and air pollution will affect the historic districts and the Old Southern Market. My view from my office window will be affected. There will be adverse impacts within 200 feet of the historic districts, and historic places, as well as visual impact from further than 200 feet.

I incorporate by reference my letters to ALDOT dated June 21, 2005, July 2, 2008, September 13, 2010, September 28, 2010, November 4, 2010, February 3, 2012 and March 5, 2012, with all attachments.

Adverse impacts to historic places and districts would be lessened if the bridge was located farther North or farther South from any historic district.

Please notify me if this e-mail does not satisfy the "written comments" requirement.

Herndon Inge  
"Stop the Bridge"

BAE Systems T (251) 690-7020  
Southeast Shipyards Alabama LLC F (251) 690-7125  
Main Gate, Dunlap Drive  
Post Office Box 3202  
Mobile, AL 36652-3202 USA

**BAE SYSTEMS**

July 9, 2014

Alabama Department of Transportation  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, AL 36110



Attention: Heather Dunn

Reference: Section 106 Consulting Parties:  
Determination of Effects for ALDOT Project DPI-0030(005) I-10 Mobile  
River Bridge and Bayway Widening EIS Mobile and Baldwin Counties, Alabama

Dear Ms. Dunn,

In response to your May 28<sup>th</sup> letter, please be advised that BAE Systems Southeast Shipyards Alabama LLC re-urges the concerns with respect to Route C which were raised by our company under its' former names, Atlantic Marine Alabama, LLC and Atlantic Marine, Inc.

Additionally, we note that with respect to Route B, this route crosses a bridge which this company owns and is maintained by this company along with the Alabama State Port Authority and Austal USA. We suggest that you consult with those two parties regarding the effects of proposed Route B on their usage of the bridge.

To reiterate, this company does believe that Route C would have a direct adverse effect on BAE Maritime Historic District.

Very truly yours,

BAE Systems Southeast Shipyards Alabama LLC

WALTER R. MEIGS  
Legal Counsel

-----Original Message-----

From: Betsy Merritt [mailto:emerritt@savingplaces.org]

Sent: Thursday, July 17, 2014 9:12 AM

To: Dunn, Heather M.

Cc: Adams, Wanda; Acoff, Alfredo; Carol Legard; cvaughn@achp.gov; Reid Nelson; Mark.Bartlett@dot.gov; Lynne.Urquhart@dot.gov; 'MaryAnn.Naber@dot.gov' (MaryAnn.Naber@dot.gov); LeeAnne.Wofford@preserveala.org; Frank.White@preserveala.org; Amanda.Hill@preserveala.org; alabamatrust@uwa.edu; bemis@cityofmobile.org; hinge@herndoninge.com

Subject: RE: I-10 Request for Extension of Time

This message confirms the National Trust for Historic Preservation's formal disagreement with many of the proposed determinations of No Adverse Effect for the I-10 project, pursuant to 36 CFR 800.5(c)(2)(I).

We agree with the detailed comments of the Alabama Historical Commission dated November 15, 2012, which concluded that a number of adverse effects on historic properties would result from the proposed project.

#### Visual Effects

In general, the methodology for assessing visual effects understates the impacts, and fails to convey the full magnitude of the adverse visual impact on Mobile's historic districts and neighborhoods. For example, the photos show the edge of the Church St. East Historic District, but do not demonstrate the way in which the proposed bridge would loom over and dominate the historic district as a whole, and would be highly visible from many places within the district.

We also agree with the SHPO that the project would have adverse visual impacts from night lighting, and that the visual impact analysis Relies too heavily on tree cover, which could be lost as the result of a storm or other event.

Adverse visual effects include:

- Church St. East Historic District, including especially the Admiral Semmes Hotel, Christ Episcopal Church, and Fort Conde Village, for the reasons described in the SHPO's comments.
- Lower Dauphin St. Historic District (eastern end of the district, especially Royal and Government streets).
- Union Hall ("substantial" adverse visual effects)
- Old Southern Market and City Hall (a National Historic Landmark) ("very serious" adverse visual impact)

#### Noise

The information provided did not include sufficient documentation to adequately assess noise impacts. The report acknowledged noise "impacts" on the on the Church St. East and Oakdale Historic Districts, but did not disclose sufficient



data (such as what assumptions were made about the noise levels of the No Build alternative) to support a determination of No Adverse Effect.

#### Vibration

The potential for construction vibration impacts was dismissed in a single conclusory paragraph with no data whatsoever. This approach of "we'll figure it out later" is utterly inadequate to support a determination of No Adverse Effect. A much more sophisticated and scientific analysis is needed.

#### Access

We agree with the SHPO that the project presents a serious potential for adverse impacts on access to the USS Alabama.

In light of the numerous objections raised to the No Adverse Effect determinations by a variety of consulting parties, we look forward to engaging in consultation to resolve these disagreements, pursuant to 36 CFR 800.5(c)(2).

In addition, we believe that release of the Draft EIS prior to resolving these disagreements would be premature, because the DEIS would be inadequate to disclose to the public the nature and magnitude of the project's potential impacts on historic properties.

Thank you for considering the National Trust's comments.

Sincerely,  
Elizabeth S. Merritt

Elizabeth S. Merritt, Deputy General Counsel  
National Trust for Historic Preservation  
(202) 588-6026  
[www.preservationnation.org](http://www.preservationnation.org)

WE HAVE MOVED:  
2600 Virginia Ave. NW, Suite 1100  
Washington, DC 20037

Note my new e-mail address:  
[emerritt@savingplaces.org](mailto:emerritt@savingplaces.org)

---

From: Betsy Merritt  
Sent: Monday, July 07, 2014 12:21 PM  
To: [dunnh@dot.state.al.us](mailto:dunnh@dot.state.al.us)  
Cc: [adamsw@dot.state.al.us](mailto:adamsw@dot.state.al.us); [acoffa@dot.state.al.us](mailto:acoffa@dot.state.al.us); Carol Legard; [cvaughn@achp.gov](mailto:cvaughn@achp.gov); Reid Nelson; [Mark.Bartlett@dot.gov](mailto:Mark.Bartlett@dot.gov); [Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov); 'MaryAnn.Naber@dot.gov' ([MaryAnn.Naber@dot.gov](mailto:MaryAnn.Naber@dot.gov)); [LeeAnne.Wofford@preserveala.org](mailto:LeeAnne.Wofford@preserveala.org); [Frank.White@preserveala.org](mailto:Frank.White@preserveala.org); [Amanda.Hill@preserveala.org](mailto:Amanda.Hill@preserveala.org); [alabamatrust@uwa.edu](mailto:alabamatrust@uwa.edu); [bemis@cityofmobile.org](mailto:bemis@cityofmobile.org); [hinge@herndoninge.com](mailto:hinge@herndoninge.com)  
Subject: I-10 Request for Extension of Time

Dear Ms. Dunn,

On behalf of the National Trust for Historic Preservation, I would like to request additional time to comment on the proposed Determination of Effects by Alabama DOT regarding the I-10 bridge project.

The Section 106 regulations require that the consulting parties be given 30 days "from receipt" of a finding of No Adverse Effect to review and comment on the findings. See 36 C.F.R. § 800.5(c). As you can see from the attached copy of the letter, The National Trust did not receive this letter (including extensive additional documentation on a CD) until June 16. Therefore, we request until July 16 to review and comment on these findings.

I anticipate that we will be notifying you of our disagreement, pursuant to 36 C.F.R. § 800.5(c)(2)(i).

Please let me know if you have any questions. Thank you.

Sincerely,

Elizabeth Sherrill Merritt | DEPUTY GENERAL COUNSEL  
P 202.588.6026 | M 202.297.4133

NATIONAL TRUST FOR HISTORIC PRESERVATION

The Watergate Office Building

2600 Virginia Avenue NW Suite 1100

Washington, DC 20037

[www.PreservationNation.org](http://www.preservationnation.org/)<<http://www.preservationnation.org/>>

WE'VE MOVED!

Note my new e-mail address:

[emerritt@savingplaces.org](mailto:emerritt@savingplaces.org)<<mailto:emerritt@savingplaces.org>>



Preserving America's Heritage

July 24, 2014

Ms. Heather Dunn  
Alabama Department of Transportation  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, Alabama 36110

Ref: *Determination of Effects for ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening EIS  
Mobile and Baldwin Counties, Alabama*

Dear Ms. Dunn:

This letter is to provide you with the Advisory Council on Historic Preservation's (ACHP's) views regarding the determinations of effects to historic properties for the referenced undertaking. In a letter dated May 28, 2014, the Alabama Department of Transportation (ALDOT) requested comments from the Section 106 consulting parties on its determinations of effects as required in Section 800.5(a) of our regulations, "*Protection of Historic Properties*" (36 CFR Part 800) for the I-10 Mobile River Bridge and Bayway Widening Project. After being granted an extension of the due date for comments, on July 27, 2014, the National Trust for Historic Preservation (NTHP) responded, objecting to ALDOT's determinations that there would be "no adverse effect" to many of the historic properties due to the potential for indirect, adverse, visual, auditory, and vibratory effects on historic properties. Although the ACHP is a formal consulting party for this undertaking, we did not receive a copy of the letter circulated to the consulting parties and the accompanying CD for comment. However, on June 7, 2014, the ACHP requested a copy of the ALDOT's determination of effects, which we received on June 8, 2014.

We have reviewed the determination of effects report and the objections raised by the National Trust for Historic Preservation. The ACHP agrees with the National Trust; with views expressed by the Alabama State Historic Preservation Office (SHPO) in its November 15, 2012 comments; and with the Mobile Historic Development Commission in its letter of July 1, 2014. All agree that there is insufficient information for ALDOT to document that there will be "no adverse effects" to historic properties related to noise, views, and vibrations. To the contrary, we find that there is the potential for visual impacts, noise, and vibrations to adversely affect the characteristics that qualify historic properties for listing in the National Register of Historic Places. A considerable amount of time has passed since the ACHP has been contacted by FHWA or ALDOT about this project. Given the extensive gap in communications, the documentation we were provided should have included information about the proposed undertaking, the preferred alternative, and specific studies that were completed, if any, to evaluate the full range of direct and indirect effects on nearby historic buildings and districts.

ADVISORY COUNCIL ON HISTORIC PRESERVATION

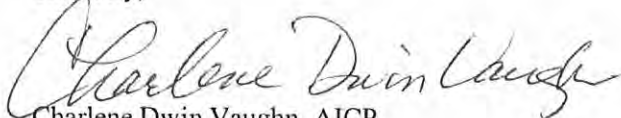
401 F Street NW, Suite 308 • Washington, DC 20001-2637  
Phone: 202-517-0200 • Fax: 202-517-6381 • [achp@achp.gov](mailto:achp@achp.gov) • [www.achp.gov](http://www.achp.gov)

In light of the concerns raised by consulting parties, and our inability to fully understand the potential effects, we are requesting additional information to support ALDOT's findings of effect. It is our understanding that the Federal Highway Administration (FHWA) has recently approved a Draft Environmental Impact Statement (DEIS) for the project, and that it will soon be available for review and comment. To the extent that the DEIS contains additional information supporting your findings, we encourage you to share the document with the Section 106 consulting parties, or advise them how it can be accessed. As the overall determination of effect for the undertaking is "adverse," ALDOT should continue consultation in accordance with 36 CFR 800.6, and explore ways to avoid, minimize, and/or mitigate adverse effects to historic properties. We also recommend that FHWA and ALDOT host a meeting in the near future to further discuss and resolve concerns regarding indirect effects.

Thank you for providing us an opportunity to comment on the determinations of effect for the referenced undertaking. Please note that the ACHP will continue to participate in the Section 106 consultation, and should be provided copies of correspondence related to the completion of the Section 106 review.

By copy of this letter, we are sharing these views with the National Trust for Historic Preservation, Federal Highway Administration, Mobile Historic Development Commission, and the Alabama Historic Commission. We request that you forward it to the other Section 106 consulting parties for which we do not have email addresses. If you have any questions, please contact Carol Legard at 202-517-0218 or via e-mail at [clegard@achp.gov](mailto:clegard@achp.gov).

Sincerely,



Charlene Dwin Vaughn, AICP  
Assistant Director  
Office of Federal Agency Programs  
Federal Permitting, Licensing, and Assistance Section



**DISPOSITION OF COMMENTS RECEIVED FROM SECTION 106 CONSULTING PARTIES (JUNE/JULY 2014)**

**Alabama Historical Commission, June 30, 2014**

	<b>Comment</b>	<b>Response</b>
1.	We agree with ALDOT's finding for no adverse visual effect to the following: Old Dauphin Way Historic District, Campground Historic District/MLK Heritage Neighborhood, Oakdale Historic District, Maysville Historic District, USS Alabama, and BAE Maritime Historic District.	Comment noted.
2.	We are concerned about the potential visual impacts of the proposed bridge to certain historic areas in downtown Mobile, specifically at locations 26, 27, 32, 44, 45, 46, 48, 49, and 51. However, we request ALDOT to explore opportunities to incorporate context sensitive design features and a reasonable tree planting plan to minimize any potential visual impacts on historic resources.	As noted in the Draft Section 106 MOA in Appendix L, an Aesthetic and Landscape Plan will be required as part of the proposed project to minimize potential visual impacts on historic resources. ALDOT, in coordination with the Aesthetic Steering Committee, has created landscaping requirements for the project within their own right-of-way; however, many of the areas where trees could be planted to preserve or enhance the downtown tree canopy are within City right-of-way. Therefore, ALDOT is providing mitigation funding to the <i>Right Tree, Right Place</i> program to make sure that appropriate trees are placed in accordance with the City's requirements.
3.	We also understand that ALDOT is committed to address lighting, vibrations, bridge aesthetics, and access to Battleship Park in future agreements with SHPO and other consulting parties.	As discussed in Section 4.13 of the Supplemental DEIS and in the Draft Section 106 MOA in Appendix L, commitments to measures to address lighting, vibrations, bridge aesthetics, and access to Battleship Park have been developed in consultation with the Section 106 Consulting Parties.
4.	While we are concerned with potential impacts to the Union Hall, which is eligible for the National Register under Criterion A for its association with Alabama's labor history, we understand that ALDOT has documented the property according to HABS standards.	As discussed in Section 4.13.6 of the Supplemental DEIS, the Union Hall was demolished by its owner. By e-mail dated June 27, 2016, SHPO informed ALDOT

that this property is no longer eligible for NRHP listing.	
As discussed in Section 4.13.8 of the Supplemental DEIS and in the Draft Section 106 MOA in Appendix L, the approach to performing archaeological surveys has been coordinated with the SHPO. On sites where demolition is occurring within ALDOT right-of-way, trained personnel are on-site to monitor and document demolition to ensure archaeological remains are preserved.	<p>Regarding archaeology, sites where landowner permission was not acquired for the Phase I survey must be addressed with a Phase I survey when a route is selected and the site acquired. Mobile is a city with many layers of occupation that may be very deep in the area proposed for the bridge. When sites where buildings now sit that become a part of this project, demolition must take place in a manner that preserves any archaeology that may be present on the site. Properties determined potentially eligible for the NRHP will require the development of Phase II testing and prior approval from SHPO before any further work occurs. ALDOT should avoid properties determined eligible for the NRHP, but if that is not possible, then ALDOT/SHPO must agree on mitigation strategies.</p>

**Mobile Historic Development Commission, July 1, 2014**

	<b>Response</b>
<p><b>Comment</b></p> <p>1. The MHDC would like to voice its concerns with the finding of no adverse effect. However, the Commission also recognizes the need for the bridge and desires to work with ALDOT and all Consulting Parties to move the project forward.</p>	Comment noted.
<p>2. With the bridge still to be designed, the Commissioners believe that most of its concerns could be handled through a programmatic agreement that will allow the bridge to proceed. These are:</p> <ul style="list-style-type: none"> <li>- The Commission is concerned with the effect of the construction on the historic resources in the area and believes a monitoring system should be implemented. This would be a direct effect that could possibly harm the physical structures.</li> <li>- The Commission is also concerned about environmental issues that would diminish the integrity of various historic resources' location: noise; stormwater; and particularly lighting.</li> </ul>	<p>Section 4.14.3 of the Supplemental DEIS and the Draft Section 106 MOA in Appendix L contain information on a construction monitoring system for vibration impacts and proposed mitigation measures. Sections 4.10 and 5.5.3 of the Supplemental DEIS address potential noise impacts. Section 4.8.2 addresses stormwater runoff. Section 4.12 and the Draft Section 106 MOA in Appendix L address lighting.</p>
<ul style="list-style-type: none"> <li>- There is also concern about the above effects on the historic resources during construction and the Commission would like to see the plan for minimizing these.</li> </ul>	<p>Construction impacts are discussed in Section 4.14 of the Supplemental DEIS.</p>

<p>As discussed in Section 4.13 and the Draft Section 106 MOA in Appendix L, an Aesthetic and Landscape Plan will be required and implemented to minimize adverse visual effects on historic resources. ALDOT, in coordination with the Aesthetic Steering Committee, has created landscaping requirements for the project within their own right-of-way; however, many of the areas where trees could be planted to preserve or enhance the downtown tree canopy are within City right-of-way. Therefore, ALDOT is providing mitigation funding to the <i>Right Tree, Right Place</i> program to make sure that appropriate trees are placed in accordance with the City's requirements.</p>	<p>The Commission believes the viewshed will be compromised particularly where open spaces or vistas occur and with multi-story buildings, thus diminishing the integrity of the various historic resources' setting and feeling. A judicious landscaping plan scattered through the nearby historic districts could alleviate some of this effect.</p>
<p>As discussed in Section 6.8 and in the Draft Section 106 MOA in Appendix L, an Aesthetic Steering Committee has been developed to provide input on the aesthetics of the project.</p>	<p>3. The Commission is of the opinion that the design of the bridge will have a significant impact on the City as a whole including the nearby historic districts. As the most visible structure in the City it will become a symbol of Mobile. The MHDC looks forward to working with ALDOT during the design phase. The Commission does note that:</p> <ul style="list-style-type: none"> <li>- The overall design of the bridge must be architecturally appealing.</li> </ul>
<p>As discussed in Section 6.8 and in the Draft Section 106 MOA in Appendix L, an Aesthetic Steering Committee has been developed to provide input on the aesthetics of the project, which includes treatment of the area beneath the bridge.</p>	<ul style="list-style-type: none"> <li>- Since the location of the bridge will be in one of the most historic parts of the city in the area first settled by Europeans, the treatment of how the large area below the bridge is handled should be of great concern.</li> </ul>
<p>A Draft Section 106 MOA has been developed in consultation with the Consulting Parties and is included in Appendix L. It will be finalized prior to the FEIS/ROD.</p>	<p>4. The Commission believes a formal document such as a programmatic agreement would be of benefit to all concerned parties. By allowing the Commission and other Consulting Parties to be part of the design process, many of the concerns expressed in this letter could be minimized.</p>
<p>Comment noted.</p>	<p>5. The Mobile Historic Development Commission looks forward to working with ALDOT and believes that together Mobile can have a bridge that everyone is proud of.</p>

**Herndon Inge, Stop the Bridge Coalition, July 7, 2014**

<b>Comment</b>	<b>Response</b>
<p>1. I disagree that the project will have “no” adverse indirect effect or direct use. The visual impact, disruption during construction, noise, vibration during construction and after construction, and air pollution will affect the historic districts and the Old Southern Market. My view from my office window will be affected. There will be adverse impacts within 200 feet of the historic districts, and historic places, as well as visual impact from further than 200 feet.</p>	<p>As discussed in Section 4.13.1 of the Supplemental DEIS, the determination of effects was changed from “no adverse effect” to “adverse effect.” As discussed in Section 4.13, the proposed project would have adverse visual effects on the Church Street East Historic District and Lower Dauphin Street Historic District. Mitigation measures for the adverse visual effects on the Church Street East Historic District and the Lower Dauphin Street Historic District are defined in the Draft Section 106 MOA in Appendix L. Construction impacts are discussed in Section 4.14 of the Supplemental DEIS. Noise impacts are discussed in Sections 4.10 and 5.5.3 of the Supplemental DEIS. Air quality is discussed in Section 4.11 of the Supplemental DEIS.</p>
<p>2. Adverse impacts to historic places and districts would be lessened if the bridge was located farther north or farther south from any historic district.</p>	<p>A full range of reasonable alternatives, including 14 different alignments at locations to the north and south, was evaluated in the Alternatives Screening Evaluation. The results of the Alternatives Screening Evaluation are contained in Appendix B of the DEIS. Alternative B’ was identified as the Preferred Alternative because it moves the alignment further away from historic resources in downtown Mobile while minimizing impacts on the maritime industry.</p>

**BAE Systems, July 9, 2014**

<b>Comment</b>	<b>Response</b>
<p>1. Please be advised that BAE Systems Southeast echoes the concerns with respect to Route C which were raised by our company under previous names.</p>	<p>The Preferred Alternative would avoid impacts to the BAE Systems Historic District.</p>
<p>2. Additionally, we note that with respect to Route B, this route crosses a bridge which this company owns and is maintained by this company along with the</p>	<p>The Preferred Alternative would not impact the bridge noted in the comment letter.</p>

Response	Comment
	Alabama State Port Authority and Austal USA. We suggest you consult with those two parties regarding the effects of proposed Route B on their usage of the bridge.

**National Trust for Historic Preservation, July 17, 2014**

Response	Comment
As discussed in Section 4.13.1 of the Supplemental DEIS, the determination of effects was changed from "no adverse effect" to "adverse effect." As discussed in Section 4.13, the proposed project would have adverse visual effects on the Church Street East Historic District and Lower Dauphin Street Historic District.	<p>1. This message confirms the National Trust for Historic Preservation's formal disagreement with many of the proposed determinations of No Adverse Effect for the 1-10 project, pursuant to 36 CFR 800.5(c)(2)(1).</p> <p>In general, the methodology for assessing visual effects understates the impacts, and fails to convey the full magnitude of the adverse visual impact on Mobile's historic districts and neighborhoods. For example, the photos show the edge of the Church St. East Historic District, but do not demonstrate the way in which the proposed bridge would loom over and dominate the historic district as a whole, and would be highly visible from many places within the district.</p>
Lighting requirements for the project are discussed in Section 4.12 and in the Draft Section 106 MOA in Appendix L.	<p>2. We also agree with the SHPO that the project would have adverse visual impacts from night lighting, and that the visual impact analysis relies too heavily on tree cover, which could be lost as the result of a storm or other event.</p>
As discussed in Section 4.13.1 of the Supplemental DEIS, the determination of effects was changed from "no adverse effect" to "adverse effect." As discussed in Section 4.13, the proposed project would have adverse visual effects on the Church Street East Historic District and Lower Dauphin Street Historic District, including the specific resources listed in the NTHP's letter. As discussed in Section 4.13.5, Union Hall was lost due to demolition by its owner.	<p>3. Adverse visual effects include:</p> <ul style="list-style-type: none"> <li>- Church St. East Historic District, including especially the Admiral Semmes Hotel, Christ Episcopal Church, and Fort Conde Village, for the reasons described in the SHPO's comments.</li> <li>- Lower Dauphin St. Historic District (eastern end of the district, especially Royal and Government streets).</li> <li>- Union Hall ("substantial" adverse visual effects)</li> <li>- Old Southern Market and City Hall (a National Historic Landmark) ("very serious" adverse visual impact)</li> </ul>
Sections 4.10 and 5.5 of the Supplemental DEIS and Appendix J contain more information on noise impacts.	<p>4. The information provided did not include sufficient documentation to adequately assess noise impacts. The report acknowledged noise "impacts" on the on the Church St. East and Oakdale Historic Districts, but did not disclose sufficient data (such as what assumptions were made about the noise levels of the No Build alternative) to support a determination of No Adverse Effect.</p>

	<b>Comment</b>	<b>Response</b>
5.	The potential for construction vibration impacts was dismissed in a single conclusory paragraph with no data whatsoever. This approach of "we'll figure it out later" is utterly inadequate to support a determination of No Adverse Effect. A much more sophisticated and scientific analysis is needed.	Section 4.14.3 of the Supplemental DEIS and the Draft Section 106 MOA in Appendix L contain more detailed information on the Vibration Study performed and proposed mitigation measures.
6.	We agree with the SHPO that the project presents a serious potential for adverse impacts on access to the USS Alabama.	Access to the USS ALABAMA Battleship Memorial Park is discussed in Section 4.13.5 of the Supplemental DEIS and in the Draft Section 106 MOA in Appendix L.
7.	In light of the numerous objections raised to the No Adverse Effect determinations by a variety of consulting parties, we look forward to engaging in consultation to resolve these disagreements, pursuant to 36 CFR 800.5(c)(2).	Additional consultation has occurred since this letter was received. The consultation is summarized in Sections 4.13 and 6.4.1 of the Supplemental DEIS, and copies of correspondence and meeting minutes and transcripts are contained in Appendix L.

**Advisory Council on Historic Preservation, July 24, 2014**

	<b>Comment</b>	<b>Response</b>
1.	We have reviewed the determination of effects report and the objections raised by the National Trust for Historic Preservation. The ACHP agrees with the National Trust; with views expressed by the Alabama State Historic Preservation Office (SHPO) in its November 15, 2012 comments; and with the Mobile Historic Development Commission in its letter of July 1, 2014. All agree that there is insufficient information for ALDOT to document that there will be "no adverse effects" to historic properties related to noise, views, and vibrations. To the contrary, we find that there is the potential for visual impacts, noise, and vibrations to adversely affect the characteristics that qualify historic properties for listing in the National Register of Historic Places.	As discussed in Section 4.13.1 of the Supplemental DEIS, the determination of effects was changed from "no adverse effect" to "adverse effect."
2.	In light of the concerns raised by consulting parties, and our inability to fully understand the potential effects, we are requesting additional information to support ALDOT's findings of effect. It is our understanding that the Federal Highway Administration (FHWA) has recently approved a Draft	Copies of the DEIS were sent to the Section 106 Consulting Parties on July 30, 2014. The DEIS was also placed on the project website at <a href="http://www.mobileriverbridge.com">www.mobileriverbridge.com</a> .

	<b>Comment</b>	<b>Response</b>
	<p>Environmental Impact Statement (DEIS) for the project, and that it will soon be available for review and comment. To the extent that the DEIS contains additional information supporting your findings, we encourage you to share the document with the Section 106 consulting parties, or advise them how it can be accessed.</p>	
3.	<p>As the overall determination of effect for the undertaking is "adverse," ALDOT should continue consultation in accordance with 36 CFR 800.6, and explore ways to avoid, minimize, and/or mitigate adverse effects to historic properties. We also recommend that FHW A and ALDOT host a meeting in the near future to further discuss and resolve concerns regarding indirect effects.</p>	<p>Additional consultation has occurred since this letter was received. The consultation is summarized in Sections 4.13 and 6.4.1 of the Supplemental DEIS, and copies of correspondence and meeting minutes and transcripts are contained in Appendix L.</p>

## **APPENDIX L-2:**

**September 23, 2014**

**Section 106 Consulting Party Meeting**



**September 23, 2014 Section 106 Consulting Party Meeting**



# ALABAMA DEPARTMENT OF TRANSPORTATION

*Design Bureau*

1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050  
Phone: 334-242-6178 FAX: 334-269-0826



*Robert Bentley*  
Governor

*John R. Cooper*  
Transportation Director

August 22, 2014

Title, First Name, Last Name  
Agency/Organization  
Address Line 1  
Address Line 2  
City, State, Zip

**RE: Section 106 Consulting Parties Consultation Meeting**  
ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening EIS  
Mobile and Baldwin Counties, Alabama

Dear Sir or Madam:

The Draft Environmental Impact Statement (DEIS) for the I-10 Mobile River Bridge and Bayway Widening was approved by the Federal Highway Administration on July 22, 2014. As a Consulting Party, you were provided a distribution copy of the approved DEIS. The comment period on the DEIS ends November 7, 2014. Public Hearings will take place on Tuesday, September 23, 2014 at the Alabama Cruise Terminal and Monday, September 29, 2014 at the Five Rivers Delta Resource Center, both from 4:00pm – 8:00pm. If you should need additional information regarding the DEIS or the proposed project, a website has been established at [www.mobileriverbridge.com](http://www.mobileriverbridge.com).

The purpose of this letter is to invite you to participate in a Section 106 Consulting Parties Consultation Meeting. The meeting will be held on Tuesday, September 23, 2014 at 9:00 a.m. The location is:

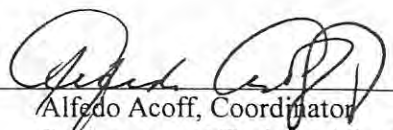
**Alabama Department of Transportation Southwest Region**  
**1701 I-65 West Service Road North**  
**Building N**  
**Mobile, AL 36618**

We proposed a finding of No Adverse Effect from the project to historic properties in correspondence to the Consulting Parties dated May 28, 2014 and in the DEIS. We received replies indicating reservations and/or disagreement with this finding. This consultation meeting has been scheduled to allow FHWA and ALDOT to consult with you to resolve the disagreement. The consultation meeting will focus on three areas. First, we will begin by giving an overview of the project and a discussion of the basis of our finding of No Adverse Effect. Then, we will afford you the opportunity to share how the project may alter, directly or indirectly, any of the characteristics of a historic property qualifying it for inclusion in the NRHP in a manner that would diminish the integrity of the property's location, design, setting, materials, feeling or association. And finally, we will conclude the meeting by identifying the next steps in the consultation process.

Please feel free to contact Ms. Heather Dunn at [dunnh@dot.state.al.us](mailto:dunnh@dot.state.al.us) if you should have questions or comments. We look forward to your participation in the Section 106 Consulting Parties consultation meeting on September 23, 2014. Thank you for your continued cooperation and interest in this project.

Sincerely,

William F. Adams, P.E.  
State Design Engineer

By:   
Alfredo Acoff, Coordinator  
Environmental Technical Section

cc: FHWA  
ALDOT Southwest Region  
Volkert & Associates, Inc.  
Heather Dunn, ETS Project Manager  
Cultural Resources File

Mr. Norman Pitman, President  
Mobile Historic  
Development Commission  
PO Box 1827  
Mobile, AL 36633-1827

Ms. Lee Anne Wofford  
Deputy State Historic Preservation Officer  
Alabama Historic Commission  
468 South Perry Street  
Montgomery, AL 36130

Commissioner Connie Hudson  
President  
Mobile County Commission  
205 Government Street  
Mobile, AL 36644-1001

The Honorable Charles Gruber  
Chairman  
Baldwin County Commission  
201 E Section Avenue  
Foley, AL 36535

Ms. Mary L. Cousar  
6 St. Joseph St.  
Mobile, AL 36602

Mr. Douglas Burtu Kearley  
Ten Wisteria Avenue  
Mobile, AL 36607

Reverend Canon Beverly F. Gibson  
Christ Church Cathedral  
115 South Conception Street  
Mobile, AL 36602

USS Alabama Commission  
Battleship Memorial Park  
PO Box 65  
Mobile, AL 36601-0065

Ms. Charlene Dwin Vaughn, AICP  
Advisory Council on Historic Preservation  
401 F Street NW, Suite 308  
Washington, D.C. 20001-2637

Ms. Elizabeth Merritt  
National Trust for  
Historic Preservation  
2600 Virginia Ave. NW, Suite 1100  
Washington, DC 20037

Ms. Carol Legard  
Advisory Council on Historic Preservation  
401 F Street NW, Suite 308  
Washington, D.C. 20001-2637

The Honorable Dane Haygood  
Mayor, City of Daphne  
PO Box 400  
Daphne, AL 36526

Mr. Walter Meigs  
BAE Systems/Southeast  
Shipyards Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

Ms. Elizabeth S. Sanders  
Downtown Mobile Alliance  
PO Box 112  
Mobile, AL 36601

Mr. Herdon Inge  
PO Box 40188  
Mobile, AL 36640

Ms. Lauren Vanderbijl  
Historic Mobile Preservation Society  
300 Oakleigh Place  
Mobile, AL 36604

Restore Mobile, Inc. c/o Mr. Jarrod J. White  
Cabaniss Johnston, LLP  
Mobile Office, Riverview Plaza  
63 South Royal Street, Suite 700  
Mobile, Alabama 36652

Mr. John Hildreth  
National Trust for Historic Preservation  
William Aiken House  
456 King Street – 3<sup>rd</sup> Floor  
Charleston, SC 29403

The Honorable Sandy Stimpson  
Mayor, City of Mobile  
PO Box 1827  
Mobile, AL 36633-1827

The Honorable Michael McMillan  
Mayor, City of Spanish Fort  
PO Box 7226  
Spanish Fort, AL 36527

Mr. Ray Harris  
Signal Shipyard/Bender Shipbuilding  
& Repair Co.  
601 S. Royal Street  
Mobile, AL 36602

Ms. Caroline Etherton  
Colonial Dames and Conde-Charlotte  
Museum House  
PO Box 1968  
Mobile, AL 36633

Ms. Ann Bedsole  
6 St. Joseph St.  
Mobile, AL 36602

Ms. Katherine Frangos  
Friends of the Museum  
PO Box 2068  
Mobile, AL 36602

Ms. Wendy Crocker  
BAE Systems/Southeast  
Shipyards Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

**Project: DPI-0030(005)**  
**I-10 Mobile River Bridge and Bayway Widening**  
**Mobile and Baldwin Counties, Alabama**

**RESUME OF MEETING**

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**DATE OF MEETING:** Tuesday, September 23, 2014 (9:00 am)  
**LOCATION:** ALDOT, Southwest Division, Training Facility  
**PURPOSE:** Section 106 Consulting Party Consultation Meeting

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<b>ATTENDANCE</b>	<b>REPRESENTING</b>	<b>EMAIL</b>	<b>TELEPHONE</b>
Mark Bartlett	FHWA	<a href="mailto:Mark.bartlett@dot.gov">Mark.bartlett@dot.gov</a>	334-274-6350
Mary Ann Naber	FHWA	<a href="mailto:Maryann.naber@dot.gov">Maryann.naber@dot.gov</a>	202-366-2060
Lynne Urquhart	FHWA	<a href="mailto:Lynne.urquhart@dot.gov">Lynne.urquhart@dot.gov</a>	334-274-6371
Jeff Shelley	FHWA	<a href="mailto:jeff.shelley@dot.gov">jeff.shelley@dot.gov</a>	334-274-6362
Chris Wilson	ACHP	<a href="mailto:cwilson@savingplaces.org">cwilson@savingplaces.org</a>	202-517-0229
Lee Anne Wofford	AHC/SHPO	<a href="mailto:leeanne.wofford@preserveal.org">leeanne.wofford@preserveal.org</a>	334-230-2659
Amanda McBride	AHC/SHPO	<a href="mailto:amanda.mcbride@preserveala.org">amanda.mcbride@preserveala.org</a>	334-242-3184
Elizabeth Merritt	National Trust for Historic Preservation	<a href="mailto:emerritt@savingplaces.org">emerritt@savingplaces.org</a>	202-588-6026
David Daughenbaugh	City of Mobile Urban Forestry	<a href="mailto:Daughenbaugh@cityofmobile.org">Daughenbaugh@cityofmobile.org</a>	251-208-5895
Richard Olsen	City of Mobile Forestry and Zoning	<a href="mailto:OLSEN@cityofmobile.org">OLSEN@cityofmobile.org</a>	251-208-7077
Dan Wagner	Christ Church Cathedral	<a href="mailto:dwagner@christchurchcathedral.mobile.org">dwagner@christchurchcathedral.mobile.org</a>	251-438-1822
Jennifer Roselius	MHDC	<a href="mailto:jroselius@drheston.com">jroselius@drheston.com</a>	646-496-6105
Devereaux Bemis	MHDC	<a href="mailto:bemis@cityofmobile.org">bemis@cityofmobile.org</a>	251-208-7281
Keri Coumanis	MHDC	<a href="mailto:caumanis@cityofmobile.org">caumanis@cityofmobile.org</a>	251-208-7272
Dr. John Cleary	USA	<a href="mailto:cleary@southalabama.edu">cleary@southalabama.edu</a>	
Bill Tunnell	Battleship USS Alabama	<a href="mailto:Btunnell@ussalabama.com">Btunnell@ussalabama.com</a>	251-433-2703
Michelle Dees	Housing First, Inc.	<a href="mailto:Michelle.d@hfal.org">Michelle.d@hfal.org</a>	251-450-3345
Herndon Inge	"Stop the Bridge"	<a href="mailto:hinge@herndoninge.com">hinge@herndoninge.com</a>	251-432-1444
John Cooper	ALDOT Director	<a href="mailto:cooperjr@dot.state.al.us">cooperjr@dot.state.al.us</a>	334-242-6776
Vince Calametti	ALDOT SW Region Engineer		334-242-6119
Lindy Sorrell	ALDOT	<a href="mailto:sorrellL@dot.state.al.us">sorrellL@dot.state.al.us</a>	334-242-6709
Zac Cooper	ALDOT	<a href="mailto:cooperZ@dot.state.al.us">cooperZ@dot.state.al.us</a>	334-242-6256
Matt Taylor	ALDOT SW Region	<a href="mailto:taylorm@dot.state.al.us">taylorm@dot.state.al.us</a>	251-680-3335
Don Powell	ALDOT SW Region	<a href="mailto:powelld@dot.state.al.us">powelld@dot.state.al.us</a>	251-470-5220
Edwin Perry, III	ALDOT SW Region	<a href="mailto:perrye@dot.state.al.us">perrye@dot.state.al.us</a>	251-470-8243
Matt Ericksen	ALDOT SW Region	<a href="mailto:Ericksenm@dot.state.al.us">Ericksenm@dot.state.al.us</a>	251-470-8201

ATTENDANCE	REPRESENTING	EMAIL	TELEPHONE
Brian Ingram	ALDOT – Design / Location	<a href="mailto:ingramb@dot.state.al.us">ingramb@dot.state.al.us</a>	334-242-6476
Taylor Stoudenmire	ALDOT – Design / Location	<a href="mailto:stoudenmiret@dot.state.al.us">stoudenmiret@dot.state.al.us</a>	334-242-6117
Alfredo Acoff	ALDOT – ETS / Design	<a href="mailto:acoffa@dot.state.al.us">acoffa@dot.state.al.us</a>	334-242-6143
Heather Dunn	ALDOT – ETS / Design	<a href="mailto:dunnh@dot.state.al.us">dunnh@dot.state.al.us</a>	334-242-6147
Natasha Clay	ALDOT – ETS / Design	<a href="mailto:clayn@dot.state.al.us">clayn@dot.state.al.us</a>	334-242-6315
Pat M. Patterson	ALDOT – ETS / Design	<a href="mailto:pattersonp@dot.state.al.us">pattersonp@dot.state.al.us</a>	334-242-6633
Bill Turner	ALDOT – ETS / Design	<a href="mailto:turnerw@dot.state.al.us">turnerw@dot.state.al.us</a>	334-242-6144
Bonnie Gums	USA Archaeology	<a href="mailto:bgumms@southalabama.edu">bgumms@southalabama.edu</a>	251-460-6562
David Webber	Volkert, Inc.	<a href="mailto:david.webber@volkert.com">david.webber@volkert.com</a>	251-342-1070
Buddy Covington	Volkert, Inc.	<a href="mailto:buddy.covington@volkert.com">buddy.covington@volkert.com</a>	251-342-1070
Kenny Nichols	Volkert, Inc.	<a href="mailto:kenny.nichols@volkert.com">kenny.nichols@volkert.com</a>	251-342-1070
Thomas Lee	Volkert, Inc.	<a href="mailto:thomas.lee@volkert.com">thomas.lee@volkert.com</a>	251-342-1070
Jason Goffinet	Volkert, Inc.	<a href="mailto:jason.goffinet@volkert.com">jason.goffinet@volkert.com</a>	770-298-9709

For reference purposes, the proceedings from the meeting are in chronological order and have been given the following topics:

Meeting Purpose

I. Introductions

II. FHWA Presentation Outlining the Section 106 Process

III. ALDOT Presentation Giving an Overview of the I-10 MRB Project & Consulting Parties Comments to-date.

IV. Discussion

- 1) Alabama Historic Commission (AHC)/ State Historic Preservation Officer (SHPO)
- 2) Mobile Historic Development Commission (MHDC) / City of Mobile
- 3) BAE Systems
- 4) “Stop the Bridge”
- 5) National Trust for Historic Preservation (NTHP)
- 6) Advisory Council on Historic Preservation (ACHP)
- 7) USS Alabama Battleship
- 8) Christ Church Cathedral (CCC)
- 9) City of Mobile Urban Forestry (CMUF)
- 10) City of Mobile Historic Development Commission (MHDC)

V. Presentation of Pilot Study on Vibration

VI. Tree Canopy Discussion

VII. Steps Moving Forward.

**Meeting Purpose:**

The purpose of the meeting was to update the Consulting Parties on the status of the I-10 Mobile River Bridge and Bayway Widening (I-10 MRB) project, present the Section 106 process, explain where the project is to-date in the process, and to discuss any concerns that the attendees have regarding the project.

## **I. Introductions**

The Mr. Bartlett (FHWA) began the meeting by welcoming everyone and thanking everyone for attending. He requested that everyone introduce themselves. After the introductions, Mr. Bartlett (FHWA) introduced Mr. Cooper (ALDOT). Mr. Cooper (ALDOT) stated that he was happy to see everyone here. Mr. Bartlett (FHWA) explained that this meeting was very important, and hopes to have a good discussion today to help define a path forward for the consultation process.

## **II. FHWA Presentation Outlining the Section 106 Process**

Mr. Bartlett (FHWA) asked Ms. Urquhart (FHWA) to give an overview of the Section 106 process and how it is anticipated that the project will proceed through the process. Ms. Urquhart (FHWA) presented a general overview of the Section 106 process and explained where the I-10 MRB project currently is in the process. After providing the overview, Ms. Urquhart (FHWA) explained that the main concern is the visual impacts the project will have on historic districts in Mobile.

## **III. ALDOT Presentation Giving an Overview of the I-10 MRB Project & Consulting Parties Comments to-date.**

Mr. Covington (Volkert) gave a presentation giving an overview of the history and development of the I-10 MRB project. Hard copies of the presentation were distributed to all the attendees before the presentation. Mr. Covington's (Volkert) presentation discussed the following topics:

- 1) Purpose and Need for the I-10 MRB Project.
  - a. Increase the capacity of I-10 to meet existing and predicted future traffic volumes.
  - b. Provide a more direct route for vehicles transporting hazardous materials.
  - c. Minimize impacts to Mobile's maritime industry.
- 2) Alternative Development.
  - a. Fourteen (14) Alternatives were originally developed for the project with input from the public.
  - b. An Alternative Screening process was completed to narrow down the alternatives to carry forward into the DEIS.
  - c. Four Alternatives were carried forward which will include widening the Bayway and four Alternative bridge crossings over the Mobile River.
- 3) Proposed Bridge Design.
  - a. Cable Stayed Bridge.
  - b. Economical – Flexible Design.
  - c. Minimum Vertical Clearance – 215 feet.
- 4) Bridge Height Comparison.

An illustration was presented that provided a comparison of the proposed I-10 MRB to other cable stayed bridges in the United States. The visual illustrated the proposed I-10 MRB to the Golden Gate Bridge (San Francisco, CA) at 220' vertical clearance, Ravenel - Cooper River Bridge (Charleston, SC) at 209' vertical clearance, Sunshine Skyway Bridge (Tampa, FL) at 175' vertical clearance, Crescent City Bridge (New Orleans, LA) at 170' vertical clearance, and the Cochrane-Africatown Bridge (Mobile, AL) at 155' vertical clearance.
- 5) Building Height Comparison in Downtown Mobile.

An illustration was presented that compared the heights of notable buildings in downtown Mobile relative to the height of the proposed bridge pilings (515') and maximum deck height (215'). The

buildings included the RSA Tower (the largest building in downtown Mobile at 725') and the Old Southern Marketing (NRHP Eligible building which stands at 50').

6) Consulting Parties Coordination Overview.

- a. Five consulting parties meetings have been held from 2003 through 2012 (not counting the one being held today).
- b. Twenty-three consulting party members are involved in the project from national originations, tribes, municipalities, to individual citizens, and groups who have an interest in historic resources in Mobile.
- c. A map was shown that illustrated the four Build Alternatives (A, B, B' Prime, and C) and the historic districts within the areas of potential effect of the alternatives. The map also included numbered locations that refer to viewshed points that were studied in the cultural resource reports. This effort was developed through consultation with many of the people in this room with regard to how to assess different points. Several field reviews were conducted with the consulting parties and the SHPO to establish the APE and establish the different viewshed analysis points that are included in the cultural resource reports.
- d. Prior to the DEIS being approved a request for comments on the determination of effects was distributed to the consulting parties on May 28, 2014.

7) Consulting Party Responses

a. SHPO/AHC Summary of Comments – June 30, 2014

- Concerned about potential visual impacts to Lower Dauphin Street and Church Street East Historic Districts.
- Requested opportunities for context sensitive design and tree planning plan.
- Requested that lighting/vibration/aesthetics and access to Battleship Park be included.
- Concerned about the effects of Union Hall. Union Hall is eligible under Criteria A.
- Proposed a study of organized labor in Alabama including extant union halls.
- Stated that an additional archaeology agreement is needed.

Mr. Bill Turner (ALDOT) responded stating that he will coordinate and develop the required archaeological work in the Programmatic Agreement once an alternative is selected.

b. MHDC Summary of Comments – July 1, 2014

- Stated that most concerns can be handled through Programmatic Agreement.
- Requested opportunities for context sensitive design and a tree planting plan.
- Requested that monitoring be conducted to avoid construction impacts.
- Concerned about impacts related to noise, storm water and lighting.
- Requested landscape plan.
- Requested architecturally appealing bridge.
- Asked about treatment of area beneath the bridge.

c. Herndon Inge Summary of Comments – July 7, 2014

- Concerned about impacts to historic districts and the Old Southern Market (visual, disruption during construction, noise, and vibration - during and after construction, air pollution).
- Concerned about adverse impacts within 200 feet of the proposed project and visual impacts from further than 200 feet.
- Stated that impacts would be lessened by locating the bridge farther north or south.

d. BAE Systems Summary of Comments – July 9, 2014



- Agreed that Alternative C would have direct adverse effect on the BAE Maritime Historic District.
  - Would like consultation with BAE, Austal and ASPA if private bridge is impacted.
- e. National Trust for Historic Preservation Summary of Comments – Jul 17, 2014
- Stated that methodology for assessing visual impacts understates impacts on historic districts as a whole.
  - Concerned about adverse visual impacts from night lighting.
  - Stated that tree cover could be lost during storm or other event.
  - Concerned about adverse visual impacts to Church Street East and Lower Dauphin Street Historic Districts, Union Hall and Old Southern Market.
  - Stated that additional information is required to assess noise impacts to Oakdale and Church Street East Historic Districts.
  - Stated that additional information is required to assess vibration impacts and sophisticated scientific analysis needed.
  - Requested further discussion on access to the USS Alabama.
  - Requested consultation to resolve disagreements.
  - Requested more review time requested on behalf of ACHP.
- The FHWA has provided additional review time.
- f. Advisory Council on Historic Preservation Summary of Comments – July 24, 2014.
- Concerned about indirect, adverse, visual, auditory, and vibratory effects.
  - Stated that there is insufficient information to document “no adverse effects” related to noise, views, and vibrations.
  - Concerned about potential for visual, noise and vibrations to adversely affect the characteristics that qualify properties for the NRHP.
  - Requested additional information on the undertaking, preferred alternative and specific studies completed.
  - Requested that the DEIS be shared with the Consulting Parties.
  - Requested that consultation continue to explore ways to avoid, minimize and/or mitigate adverse effects.
  - Requested meeting with FHWA and ALDOT to discuss and resolve concerns.
- 8) I-10 MRB Project DEIS Environmental Commitments
- Mr. Covington (Volkert) explained that the May 28 correspondence was a Determination of Effect and a request for comments from the consulting parties and at that time consulting parties may or may not have been aware of some of the environmental commitments that are listed in the DEIS. He provided explanations of the following commitments:
- Lighting: Design so light levels at the ROW boundary will be less than or equal to the existing light levels (DEIS Section 4.15)
  - Archaeology: Phase II archaeology testing will be coordinated with the SHPO and performed as part of the investigation of the Preferred Alternative in the FEIS if sites cannot be avoided. (DEIS Section 4.16.7)
  - USS Alabama: ALDOT will coordinate with SHPO, USS Alabama Battleship Memorial Park Commission, and the consulting parties to determine location and type of signs for the USS Alabama Battleship Park. (DEIS Section 4.16.4.2)

- Visual: Visual effects and opportunities to incorporate context-sensitive design features will continue to be discussed with SHPO and Section 106 Consulting Parties. A reasonable planting plan will be developed in an effort to maintain the tree canopy (Section 4.16).
- Aesthetics: Coordinate during the design phase with stakeholders, SHPO, and Section 106 Consulting Parties on bridge aesthetics.
- Vibration: The DEIS has a draft vibration affects report/study in it. Construction techniques will be developed to minimize or avoid vibration impacts to historic properties. Monitoring will be conducted before, during and after construction so that no buildings have an adverse effect from vibrations.

With regard to other indirect effects Mr. Covington (Volkert) explained that noise impacts were identified in the Oakdale and Church Street East Historic Districts and that these districts are located in a highly developed environment and in close proximity to the existing transportation network. Properties were reviewed, and the increase in the projected noise levels for the Build Alternatives over the No-Build Alternative is 2 dBA or less, which is imperceptible by most people. With regard to impacts to air quality, Mr. Covington (Volkert) explained that an Air Quality Report is included in the DEIS and the project is not expected to cause air quality impacts.

#### **IV. Discussion**

After the introductions and presentations, Mr. Bartlett (FHWA) opened the meeting to discussions. He invited the AHC/SHPO to speak first.

##### **1) Alabama Historic Commission (AHC)/ State Historic Preservation Officer (SHPO) Comments and Discussion**

Ms. Wofford (AHC/SHPO) will be taking over the project for Elizabeth Brown who retired. Ms. Wofford (AHC/SHPO) stated that she agrees with original comments regarding visual impacts to some of the districts. Ms. Wofford (AHC/SHPO) asked about the noise and where the bridge would start and the rise of the bridge.

Ms. McBride (AHC/SHPO) stated that they are not convinced that there will be not be any impacts or adverse impacts and they will need more information to access impacts to archaeological resources. Concerning archaeology, need to wait until the Phase I and Phase II testing has been completed to access affects.

##### **2) Mobile Historic Development Commission (MHDC)/City of Mobile Comments and Discussion**

Mr. Bemis (City of Mobile MHDC) asked if the rise will be the same from the original design. Mr. Webber (Volkert) stated that the rise has always been 4% but that where the bridge starts is a little different. Mr. Bemis (City of Mobile) asked if we would be pushing it back from Virginia Street to a little further west. Mr. Webber (Volkert) answered that the ramps would start at Broad Street.

Mr. Bemis (City of Mobile) stated that he liked the vibration study and requested that the MHDC be involved in the monitoring of some of the buildings like St. Matthews, Council School, Prince of Peace, etc. He requested that the Maritime Museum GulfQuest and the cruise terminal be monitored as well.

He would like something done that would help with noise mitigation and would like for final lighting design plans to wait until the project is closer to completion. The lighting and noise affects the quality of life of the people who live in the historic districts.

He stated that the MHDC does not agree that there will be no visual effects and requested that ALDOT work with David Daughenbaugh who is tree commission person for the City of Mobile to develop some canopy to mitigate visual effects.

MHDC is also concerned about the use of space under the bridge, how is it going to be utilized, can it be utilized? The MHDC would like to see something happen underneath the bridge that would make it an amenity for the city and not just a no man's land.

Ms. Roselius (MHDC) stated that she is very pleased with the plan and that we are avoiding direct impacts to historic districts and historic buildings. She agrees with comments about monitoring and mitigating damage from vibration and wanted to know if damage does occur what would be in place to help repair or stabilize the structure. MR. Calametti (ALDOT) responded stating that a pre-construction crack survey (pre-analysis) will be conducted to document the existing conditions. The goal will be to not cause damage but when damage occurs, ALDOT will require each contractor to have a bond to repair any damage. She also requested more time to comment after archaeology studies are done and to make sure the commitment in the DEIS with respect to lighting and vibration are incorporated into the FEIS.

### **3) BAE Systems Comments and Discussion**

BAE Systems had no comments.

### **4) "Stop the Bridge" Comments and Discussion**

Mr. Inge (Stop the Bridge) stated that he respects ALDOT/FHWA need for the bridge but voiced concerns from the perspective of a "person, a resident, a homeowner, a property owner". He expressed concerns about the effect the project would have on his quality of life, children, grandchildren, a cemetery, his house and his office. Mr. Inge stated he represents the people who live here and made reference to a project he worked to have stopped in the past, the I-210 Connector project.

He stated that there is confusion regarding the height of the travel deck and stated that the nighttime pictures in the presentation were nice but pointed out that the design of the structure is not yet known. He added we don't know how high it is going to be yet.

Mr. Inge stated that the US Department of Transportation published a report called The Freeway in the City in 1968 that says that highway transportation cannot be allowed to function apart from or in conflict with the environment. He added that in 1997, the FHWA and the Texas Department of Transportation did a report, Social and Economic Effects of Elevated, Depressed, and At-Grade Level Freeways in Texas, which says elevated freeway designs, raise particular questions concerning noise, air quality impacts.

Mr. Inge believes the project will have an adverse visual effect on Church Street East, Lower Dauphin Street, Oakdale, Mayesville, Union Hall, Old Southern Market, and Government Street Presbyterian

Church with the pylon being 500-520 feet tall. He added that visual intrusions can cause a direct adverse impact on National Historic Landmarks and stated that Cooper Riverside Park would no longer exist because pylons and a suspension bridge would be above it.

In closing, Mr. Inge stated that elevated expressways that have been built in other cities near the downtown area are now being torn down and the bridge in its present route was going to be a guillotine.

#### **5) National Trust for Historic Preservation (NTHP) Comments and Discussion**

Ms. Merritt (NTHP) said that their disposition was accurately described in the presentation and that they feel that the impacts will be adverse particularly the visual impacts. The NTHP is also concerned about vibration impacts and access to the USS Alabama. She added that because a preferred alternative has been developed, it is possible to discuss, evaluate, study impacts now and not later. She added that she would like for other bridge designs / types to be explored that are not so visually intrusive. She requested that alignment be shifted further away from downtown and access to USS Alabama be included. Regarding noise impacts, she wants more data regarding what assumptions were used in the analysis. Wants more information concerning why the height was changed and about the businesses that are being relocated and are they historic. She also requested that a commitment be made to the level of acceptable vibration. She pointed out some errors in the slide including references to sections in the DEIS.

The NTHP comments and discussion continued later during the meeting. The following is a summary of the continued discussion. The Ms. Urquhart (FHWA) provided clarification about the assessment of effects. She stated that the project will have a visual effect but that the affect will not diminish the integrity of the historic districts given that they exist in an urban developed environment. Ms. Merritt (NTHP) responded stating it is hard to quantify visual effects but if you have a predominant perspective that believes the bridge is a visual intrusion and is highly undesirable you can have disinvestment in the districts over the long term. This could translate into people not wanting to live or work there. She added that the visual impact should not just be assessed from the perspective of having your back to the resource and looking out. It should also be evaluated from the perspective of being in the historic district and looking at the bridge and how that will effect economic decisions in the future. It may result in disinvestment and indirect and cumulative impacts that need to be evaluated. Mr. Bartlett (FHWA) added that some of the commitments the department has made should help the bridge fit into the environment including visual treatments, etc. He added that the benefits of the bridge such as traffic diversion of heavy trucks will also be a benefit.

Mr. Inge (Stop the Bridge) initiated a discussion of a low-build bascule bridge option with the interstate closing for a period of time with both tunnels open. He added that the ship traffic can be accommodated with a low-build option.

Ms. Merritt (NTHP) asked about other bridge designs that may have a lower visual profile than a cable-stay bridge. Mr. Covington (Volkert) responded stating that early on in the project input was received from the community about what type of bridge they would like to see and comments were received about what the bridge should not look like – a “heavy-type” structure. A cable-stay seemed to best-fit

the design constraints / requirements. Ms. Naber (FHWA) added that it has been her experience that cable-stay bridges represent the least obtrusive visual footprint whereas truss-type bridges are much more massive and represent a much greater intrusion on the visual environment. Mr. Bartlett (FHWA) added that the PA can include lighting considerations including limitations on the up-lights on the cables.

Ms. Merritt (NTHP) asked about why the height changed. It was explained that the bridge deck changed from 190' to 215' to accommodate the cruise industry and maritime interests. Mr. Bartlett (FHWA) added they do not want to introduce anything into the ship channel that would limit the port economically in the future. We are building a structure over the ship channel that will there for a very long time. Ms. Merritt (NTHP) asked about how much the project was extend to accommodate a 4% grade and the additional height. Mr. Bartlett (FHWA) responded stating about 625' on each end.

#### **6) Advisory Council on Historic Preservation (ACHP)**

Mr. Wilson (ACHP) gave an overview of the roll of the ACHP relative to the Section 106 process. He added that they have no record of prior coordination from ALDOT or the FHWA regarding this project and added that they were not included in the initiation and identification processes. He stated that they stand by their previous statements regarding the assessment of effects and referenced 800.5 – Assessing Adverse Effects. He added that it is hard for him to understand that there will be no visual effects to Mobile associated with a bridge being constructed that is well over 500' tall – bridge will be the 2<sup>nd</sup> tallest structure in Mobile. He said that we need to agree to an adverse effect so that we can move forward with discussing mitigation adding that he doesn't believe you can discuss mitigation without admitting to an adverse effect. He highly recommended that once it is determined that the project will have adverse effects that we begin to involve the National Park Service because of the multiple NHL's in this project.

Mr. Bartlett (FHWA) and Ms. Acoff (ALDOT) stated that they would follow up with documentation regarding prior coordination with the ACHP. Ms. Acoff (ALDOT) requested that the ACHP verify their address for future coordination.

A participant asked if there is a way to incorporate commitments in a Programmatic Agreement (PA) without agreeing to an adverse effect. Mr. Wilson (ACHP) responded stating that the reason for a PA or a Memorandum of Agreement (MOA) is because you are addressing adverse effects. Ms. Naber (FHWA) disagreed stating that PA's do not necessarily require an adverse effects determination. In the I-10 MRB case, we know we are going to need a PA because we must commit to archaeological research in areas that are not readily accessible right now. As part of the PA, we would include commitments to measures that would minimize the hard or the potential for harm to resources. Mr. Wilson (ACHP) asked if there are examples like the I-10 MRB where there were no adverse effects. Ms. Naber (FHWA) responded and cited bridges in Charleston, Savannah, and St. Augustine (reconstructed bridge) are examples.

Mr. Bartlett (FHWA) stated that they do not believe there is no effect; the FHWA just doesn't believe it is true adverse effect. Ms. Naber (FHWA) added that the criteria includes diminishment of contributing aspects and when you put the project in the context of where it is located, which in this case is in an urban / modern setting with existing 500' tall buildings and lighting, will the addition of a bridge

structure diminish the visual quality of the contributing historic resources. She added that at this stage (step 3 in the Section 106 process) is to talk not just about mitigating identified adverse effects but to also discuss measures that we can put in the plans that will avoid additional adverse effects.

#### **7) USS Alabama Battleship**

Mr. Tunnell (USS Alabama) requested additional access to Battleship Parkway be considered in the project.

#### **8) Christ Church Cathedral (CCC)**

Mr. Wagner (CCC) understands the need for the project but expressed concern with vibration caused by the project because they experienced vibration from the tunnel construction, construction of government plaza, and the demolition of the old county jail. He also expressed concerns about what will be beneath the bridge after it is constructed and the need to maintain continuity between neighborhoods in the community.

#### **9) City of Mobile Urban Forestry (CMUF)**

Mr. Daughenbaugh (CMUF) expressed concern regarding existing trees that the project may impact and once more design information is available the trees that will be impacted will need to be identified. Permits from the Mobile City Tree Commission may be necessary. He also stated that impacts to commercial properties landscapes that are required by the city will need to be evaluated (referenced Austal's Parking lot and Landscaping). Mr. Daughenbaugh also mentioned that there is an oversize load corridor in the vicinity of the project that the project may affect. The corridor is Baker Street / Yeend Street and is associated with cargo from the State Docks. If construction staging is conducted in this area coordination with the State Docks will be necessary.

Mr. Olsen (CMUF) reiterated concerns about the treatment under the bridge. What land use will be permitted under the bridge? He would like this to be considered as part of the design process.

#### **10) City of Mobile, Mobile Historic Development Commission (MHDC)**

Ms. Coumanis (MHDC) stated that they are concerned with what occurs to the land beneath the bridge especially along Canal Street.

### **V. Presentation of Pilot Study on Vibration**

Dr. Cleary (USA) introduced himself and gave an overview of the history of the study. He stated that a draft report was included in the DEIS and a final report will be included in the FEIS.

Dr. Cleary stated that the pile driving for the study occurred south of the cruise terminal in the shipyard formally known as Bender close to the Mobile River and the Southern Fish Market. He explained that this location was chosen because the site is where piles would be if the Preferred Alternative was constructed. Vibrations were measured at distances of 50, 100, and 150-feet from the pile site. Dr. Cleary explained that two types of piles were evaluated; a displacement pile which is a 32-inch<sup>2</sup> solid concrete square shaped block. It's called a displacement pile because when the pile is driven it moves a lot of soil out of the way. When you drive a displacement pile you typically create a much larger vibration. The other type of pile that was use is called an "HP" pile or a non-displacement pile which

looks like a beam. USA used two different sized “HP” piles: one measured 14 x 117 and the other measured 12 x 53. The 14 or 12 represent the approximate depth or the flange width and the second number (117 & 53) is the actual weight per foot.

Vibrations were measured on the surface using three different ways to measure 1) displacement, which in soils are difficult to measure, 2) velocity of the soil as it moves, and 3) acceleration in the soil. Acceleration is used in Earthquake Design. In construction vibration they typically use the velocity of the soil. Velocity is referred to as the Peak Particle Velocity (PPV), which measures the peak way the particles (soil) is moving and the measurement unit used is inches-per-second (in/sec). Measurements are taken using a geophone, which measures velocities.

Dr. Cleary explained that the results of the vibration monitoring showed that the 36-inch concrete pile had larger vibration impacts within 50-70 feet of the pile and the attenuation of vibration dissipated quickly. The non-displacement “HP” pile showed lower vibration impacts but exhibited lower vibration dissipation, meaning the vibration did not dissipate as quickly when compared with the concrete piling.

Dr. Cleary stated that the threshold for damage risk to modern structures is 0.5 PPV (in/sec) and the threshold for damage risk to sensitive structures at 0.1 PPV (in/sec). He added that research shows that at about 0.2 PPV (in/sec) is considered annoying to people. The recommended vibration limits is 0.5 PPV (in/sec) for modern structures and 0.1 PPV (in/sec) for potentially sensitive structures. The recommended survey distance (how far from the site do you need to monitor for vibration) is 150 feet for modern structures. The study results indicate that at 150 feet from the pile site the vibration was well below the 0.5 PPV (in/sec) and at 250 feet there would be very little potential for vibration damage.

After Dr. Cleary’s presentation, the meeting was opened for questions and discussion. The following summarizes the proceedings.

Mr. Wilson (ACHP) asked what provisions are in-place if some vibration damage actually occurs? What would be done to rectify it and change the construction technique? Dr. Cleary stated that there are several different pile driving techniques and the study used ALDOT’s technique of pre-boring or pre-jetting a hole to a certain depth before pile driving. This technique lessens the vibration because the deeper you go the less the vibration is. You can also use a cushion of plywood between the pylon and the hammer.

Mr. Inge (Stop the Bridge) stated that if we were not “married” to B Prime then we would not have to worry about vibration because we would be a mile away not 150 feet. Mr. Inge referenced the Dog River Bridge as an example for pylons and asked how much hammering would be needed to get the pylons of the Mobile River Bridge to the right height. Dr. Cleary responded stating number of pylons would depend on the many factors; soil type, the length of the pylon, and loads (length of the bridge). The construction technique of the foundation has an effect of vibrations, so the right foundation will help reduce the vibrations.

Mrs. Merritt (NTHP) asked where the vibration monitoring would be placed; on the ground or on side of the building. Dr. Cleary stated that for the study vibration monitors were placed on the ground and on

the side of a large steel building 140 feet from the actual pile driving site. Vibration monitoring can be placed on the outside of the building, the ground which is most common, and even sometimes in a basement if there is worry about the structure.

Mrs. Merritt (NTHP) asked if reinforcement of sensitive buildings can be utilized for protection. Dr. Cleary stated that there are ways that you could reinforce the building but by doing so you would change the building. He added that research has studied the effects of digging ditches around the structure but most of the research shows very limited success in limiting vibrations.

Mrs. Merritt (NTHP) expressed concern regarding glass cracking. Is there a situation where monitoring does not record excessive vibration but glass cracks anyway? Dr. Cleary stated that there are situations where the vibration levels are lower than expected but damage still occurs. He added that a pre-construction survey would need to be done to document the exact condition of the buildings.

Mr. Devereaux (MHDC) stated that a lot of old buildings have spread footings that go down pretty far and asked if it would be possible to put the vibration monitors in the ground. Dr. Cleary stated that yes it would be possible but most of the research has shown that by measuring surface vibrations you can effectively protect the structure.

Mr. Inge (Stop the Bridge) asked Dr. Cleary to explain the noise pollution of pile driving immediately adjacent to downtown over a 5 to 10 year construction schedule. Dr. Cleary stated that they did not do a noise analysis for the vibration study but pile driving is noisy and construction is noisy. There have been cases where people have chosen not to do pile driving because of the noise but there is one thing to keep in mind, that even though the construction schedule is 5 to 10 years they will not be pile driving in one area for that entire time. They start at one end and move out so there might be construction noise but it wouldn't be pile driving necessary for 5 to 10 years.

Mr. Calametti (ALDOD) asked Dr. Cleary if he knew enough about the soil conditions to know if a drill shaft would be possible at the pile locations. Dr. Cleary stated that he was not a geotechnical expert but in his opinion a drill shaft could be a potential foundation system. The participant stated that this construction method would greatly reduce or eliminate the noise and vibration. Dr. Cleary stated that yes that the vibration is greatly reduced with drill shaft and with noise you don't have the pile driving occurring so you don't have that constant noise.

Mr. Devereaux (MHDC) stated that during the recent construction of GulfQuest they found that the soil was fill. Dr. Cleary stated that two locations were evaluated for the pile driving study; one was the site they used which is close to where an actual pile would be placed and the other was located close to the Bender Administrative building. Soil borings were collected at both locations (600 feet apart) and the soils were very consistent. The only inconsistency found was a clay layer at 100 feet and the clay layer was deeper (20-30 feet) at the location close to the river where the piling would be located.

Mr. Covington (Volkert) stated that the DEIS addresses traffic noise for existing, build condition, and no-build conditions and there is also a section on construction noise. The construction noise is almost an



environmental commitment, it says construction noise is temporary and the contractor will have to abide / follow the ALDOT construction method addressed specifically the construction noise.

Mrs. Merritt (NTHP) asked Mr. Calametti (ALDOT) about the construction method called the drill shaft technique. Is it generally more or less expensive or the same as the traditional pile driving construction technique? Mr. Calametti (ALDOT) responded that it depends on soil characteristics and added that he has seen two methods used in the area. He added that we will have to have a good geotechnical engineer regardless of the technique used.

Mr. Daughenbaugh (City of Mobile Urban Forestry) asked Dr. Cleary if his study evaluated one pile driver hitting one pile at a time. Or, does it take into account one or more pile drivers used in close proximity at the same time. Dr. Cleary stated that the number of pile drivers would depend on the contractor's construction technique. Mr. Daughenbaugh asked if this would increase the displacement? Dr. Cleary stated that it is possible since there would be two "waves" occurring that generate from two different locations that's why monitoring is so important. Mr. Calametti (ALDOT) added that's why you also set the maximum limits. The contractor has the option of using a cushion or changing the hammer size on the pile driver to decrease the vibration.

Mrs. Naber (FHWA) asked about the recommended distance to where to put the monitors - do you want to put the monitors outside of the area that you might anticipate damage? Dr. Cleary stated that if you have a sensitive structure within 250 feet you would want to monitor the location and it is recommended that a pre-construction and post-construction survey be conducted to make sure you can document any damage caused by the project. Regarding structures beyond 250 feet, you might want to monitor either at 250 feet or whatever the edge of the ROW is to make sure you don't get vibrations greater than expected. At 250 feet you expect that vibrations would never reach over 1 PPV in/sec. Dr. Cleary added that the probability of damage to structures that are greater than 250 from the site is low.

Mrs. Merritt (NTHP) asked if they could get a copy of the vibration study presentation. Mr. Calametti (ALDOT) responded stating that the vibration study presentation will be distributed with the minutes of the meeting.

## **VI. Tree Canopy Discussion**

Mr. Bartlett (FHWA) requested that the City of Mobile Urban Forestry talk about what activities the City currently is pursuing to protect and encourage growth of the canopy. Mr. Daughenbaugh (City of Mobile Urban Forestry) stated that the trees within the City's right-of-way are protected and any impacts to trees will need to be coordinated with the Mobile Tree Commission. The Mobile Tree Commission also provides protection to trees on private property and they have guidelines for plantings in the downtown districts. Mr. Daughenbaugh added that they would like to have something that would visually buffer the impact of the structure being built – particularly from the Claiborne Street and Royal Street corridor views.

## **VII. Steps Moving Forward Discussion**

Mr. Bartlett (FHWA) provided the attendees with an overview of the next steps in the Section 106 Consulting Parties process. He requested that all parties begin to develop an initial Programmatic

Agreement (PA) that includes some of the comments received from the meeting. He added that he would like the consulting parties to provide information on what they want included in the PA. ALDOT has provided their initial commitments but they would like the consulting parties to provide additional input regarding context-sensitive design, aesthetics of the bridge, lighting, etc. Mr. Bartlett explained that the initial PA would be an outline and very bullet oriented so that the basics can be agreed upon before developing the larger PA.

Mr. Wilson (ACHP) added that he thinks there is a “fairly unanimous” opinion among the consulting parties that there are adverse effects.

Mrs. Merritt (NTHP) requested that the commitments be clarified since some of the slides in the presentation were erroneous. Mr. Bartlett (FHWA) responded stating that the commitments are listed in the front of the DEIS and are signed by the Chief Engineer. Mrs. Naber (FHWA) added that the existing commitments signed by the Chief Engineer do not preclude the consideration of additional methods. She added that we are in Step 3 of the Section 106 process and we’re still looking to consider means of avoiding any adverse effects or impacts.

Submitted by:

Christy Overstreet and Jason Goffinet (Volkert).

**ATTACHMENTS:**

- 1) ALDOT / FHWA Invitation Letters
- 2) Sign-In Sheets
- 3) Agenda
- 4) FHWA Section 106 Presentation (omitted from Supplemental DEIS - available upon request to ALDOT)
- 5) ALDOT I-10 Mobile River Bridge Consulting Parties Consultation Meeting Presentation (omitted from Supplemental DEIS - available upon request to ALDOT)
- 6) University of South Alabama Department of Civil Engineering “Vibrations Due to Pile Driving at the Mobile River Bridge Site” Presentation (omitted from Supplemental DEIS - available upon request to ALDOT)
- 7) Court Reporter Transcript

**ATTACHMENT 1: ALDOT / FHWA Invitation Letters**



# ALABAMA DEPARTMENT OF TRANSPORTATION

Design Bureau

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Robert Bentley  
Governor

John R. Cooper  
Transportation Director

August 22, 2014

COPY

Title, First Name, Last Name  
Agency/Organization  
Address Line 1  
Address Line 2  
City, State, Zip

**RE: Section 106 Consulting Parties Consultation Meeting**  
ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening EIS  
Mobile and Baldwin Counties, Alabama

Dear Sir or Madam:

The Draft Environmental Impact Statement (DEIS) for the I-10 Mobile River Bridge and Bayway Widening was approved by the Federal Highway Administration on July 22, 2014. As a Consulting Party, you were provided a distribution copy of the approved DEIS. The comment period on the DEIS ends November 7, 2014. Public Hearings will take place on Tuesday, September 23, 2014 at the Alabama Cruise Terminal and Monday, September 29, 2014 at the Five Rivers Delta Resource Center, both from 4:00pm – 8:00pm. If you should need additional information regarding the DEIS or the proposed project, a website has been established at [www.mobileriverbridge.com](http://www.mobileriverbridge.com).

The purpose of this letter is to invite you to participate in a Section 106 Consulting Parties Consultation Meeting. The meeting will be held on Tuesday, September 23, 2014 at 9:00 a.m. The location is:


**Alabama Department of Transportation Southwest Region**  
**1701 I-65 West Service Road North**  
**Building N**  
**Mobile, AL 36618**

We proposed a finding of No Adverse Effect from the project to historic properties in correspondence to the Consulting Parties dated May 28, 2014 and in the DEIS. We received replies indicating reservations and/or disagreement with this finding. This consultation meeting has been scheduled to allow FHWA and ALDOT to consult with you to resolve the disagreement. The consultation meeting will focus on three areas. First, we will begin by giving an overview of the project and a discussion of the basis of our finding of No Adverse Effect. Then, we will afford you the opportunity to share how the project may alter, directly or indirectly, any of the characteristics of a historic property qualifying it for inclusion in the NRHP in a manner that would diminish the integrity of the property's location, design, setting, materials, feeling or association. And finally, we will conclude the meeting by identifying the next steps in the consultation process.

Please feel free to contact Ms. Heather Dunn at [dunnh@dot.state.al.us](mailto:dunnh@dot.state.al.us) if you should have questions or comments. We look forward to your participation in the Section 106 Consulting Parties consultation meeting on September 23, 2014. Thank you for your continued cooperation and interest in this project.

Sincerely,

William F. Adams, P.E.  
State Design Engineer

By:   
Alfredo Acoff, Coordinator  
Environmental Technical Section

cc: FHWA  
ALDOT Southwest Region  
~~Volkert & Associates, Inc.~~  
Heather Dunn, ETS Project Manager  
Cultural Resources File

Mr. Norman Pitman, President  
Mobile Historic  
Development Commission  
PO Box 1827  
Mobile, AL 36633-1827

Ms. Lee Anne Wofford  
Deputy State Historic Preservation Officer  
Alabama Historic Commission  
468 South Perry Street  
Montgomery, AL 36130

Commissioner Connie Hudson  
President  
Mobile County Commission  
205 Government Street  
Mobile, AL 36644-1001

The Honorable Charles Gruber  
Chairman  
Baldwin County Commission  
201 E Section Avenue  
Foley, AL 36535

Ms. Mary L. Cousar  
6 St. Joseph St.  
Mobile, AL 36602

Mr. Douglas Burtu Kearley  
Ten Wisteria Avenue  
Mobile, AL 36607

Reverend Canon Beverly F. Gibson  
Christ Church Cathedral  
115 South Conception Street  
Mobile, AL 36602

USS Alabama Commission  
Battleship Memorial Park  
PO Box 65  
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Ms. Charlene Dwin Vaughn, AICP  
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Washington, D.C. 20001-2637

Ms. Elizabeth Merritt  
National Trust for  
Historic Preservation  
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Ms. Carol Legard  
Advisory Council on Historic Preservation  
401 F Street NW, Suite 308  
Washington, D.C. 20001-2637

The Honorable Dane Haygood  
Mayor, City of Daphne  
PO Box 400  
Daphne, AL 36526

Mr. Walter Meigs  
BAE Systems/Southeast  
Shipyards Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

Ms. Elizabeth S. Sanders  
Downtown Mobile Alliance  
PO Box 112  
Mobile, AL 36601

Mr. Herdon Inge  
PO Box 40188  
Mobile, AL 36640

Ms. Lauren Vanderbijl  
Historic Mobile Preservation Society  
300 Oakleigh Place  
Mobile, AL 36604

Restore Mobile, Inc. c/o Mr. Jarrod J. White  
Cabaniss Johnston, LLP  
Mobile Office, Riverview Plaza  
63 South Royal Street, Suite 700  
Mobile, Alabama 36652

Mr. John Hildreth  
National Trust for Historic Preservation  
William Aiken House  
456 King Street – 3<sup>rd</sup> Floor  
Charleston, SC 29403

The Honorable Sandy Stimpson  
Mayor, City of Mobile  
PO Box 1827  
Mobile, AL 36633-1827

The Honorable Michael McMillan  
Mayor, City of Spanish Fort  
PO Box 7226  
Spanish Fort, AL 36527

Mr. Ray Harris  
Signal Shipyard/Bender Shipbuilding  
& Repair Co.  
601 S. Royal Street  
Mobile, AL 36602

Ms. Caroline Etherton  
Colonial Dames and Conde-Charlotte  
Museum House  
PO Box 1968  
Mobile, AL 36633

Ms. Ann Bedsole  
6 St. Joseph St.  
Mobile, AL 36602

Ms. Katherine Frangos  
Friends of the Museum  
PO Box 2068  
Mobile, AL 36602

Ms. Wendy Crocker  
BAE Systems/Southeast  
Shipyards Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

**From:** Heisler, Timothy (FHWA)

**Sent:** Wednesday, September 03, 2014 6:43 PM

**To:** Chickasaw Nation (HPO@chickasaw.net)

**Subject:** DPI-0030(005), I-10 Mobile River Bridge and Bayway Widening, Mobile & Baldwin Counties, State of Alabama

The Draft Environmental Impact Statement (DEIS) for the Mobile River Bridge was signed on July 22, 2014. FHWA would like to make you aware that three important meetings are upcoming:

1. Consulting party meeting at 9:00 a.m. on Sep 23, 2014, ALDOT's 9<sup>th</sup> Division Training Room, 1701 West I-65 Service Rd North, Mobile, AL 36618
2. Public involvement meeting at 4:00 pm on Sep 23, Mobile Cruise Terminal, 201 South Water St, Mobile, AL 36002
3. Public involvement meeting at 4:00 pm on Sep 29, 5 Rivers Delta Resource Center, 30945 Five Rivers Blvd, Spanish Fort, AL 36527

A copy of DEIS may be found at <http://www.mobileriverbridge.com/> or if you would like a CD or hard copy, please let me know.

Thanks,

***Tim Heisler***

**Transportation Engineer**

**Federal Highway Administration – Alabama Division**

**9500 Wynlakes Place**

**Montgomery, Alabama 36117**

**Phone: 334.274.6370**

***timothy.heisler@dot.gov***

**From:** [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov) [mailto:[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)]

**Sent:** Wednesday, August 20, 2014 4:22 PM

**To:** [kcarleton@choctaw.org](mailto:kcarleton@choctaw.org); [lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com); [pbowlegs@outlook.com](mailto:pbowlegs@outlook.com); [rthrower@pci-nsn.gov](mailto:rthrower@pci-nsn.gov); [AlisonSwing@semtribe.com](mailto:AlisonSwing@semtribe.com); [celestine.bryant@actribe.org](mailto:celestine.bryant@actribe.org); [chascoleman75@yahoo.com](mailto:chascoleman75@yahoo.com); [earlii@tunica.org](mailto:earlii@tunica.org); [espain@mcn-nsn.gov](mailto:espain@mcn-nsn.gov); [GeoffreyWasson@semtribe.com](mailto:GeoffreyWasson@semtribe.com); [jjacobs@mcn-nsn.gov](mailto:jjacobs@mcn-nsn.gov); [kara.gann@kialegeetribe.net](mailto:kara.gann@kialegeetribe.net); [ukbthpo-larue@yahoo.com](mailto:ukbthpo-larue@yahoo.com); [kokua.aina57@gmail.com](mailto:kokua.aina57@gmail.com); [harjo.n@sno-nsn.gov](mailto:harjo.n@sno-nsn.gov); [rdushane@estoo.net](mailto:rdushane@estoo.net); [russtown@nc-chokeee.com](mailto:russtown@nc-chokeee.com); [lstapleton@unitedkeetoowahband.org](mailto:lstapleton@unitedkeetoowahband.org)

**Cc:** [Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov); Dunn, Heather M.

**Subject:** DPI-0030(005), I-10 Mobile River Bridge and Bayway Widening, Mobile & Baldwin Counties, State of Alabama

The Draft Environmental Impact Statement (DEIS) for the Mobile River Bridge was signed on July 22, 2014. FHWA would like to make you aware that three important meetings are upcoming:

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A copy of DEIS may be found at <http://www.mobileriverbridge.com/> or if you would like a CD or hard copy, please let me know.

Thanks,

***Tim Heisler***

Transportation Engineer

Federal Highway Administration – Alabama Division

9500 Wynlakes Place

Montgomery, Alabama 36117

Phone: [334.274.6370](tel:334.274.6370)

[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)



**ATTACHMENT 2: Sign-In Sheets**

Project DPI-0030(005)  
 I-10 Mobile River Bridge and Bayway Widening  
 Mobile and Baldwin Counties, Alabama  
 Volkert Contract No. 911602.12

**SIGN-IN SHEET**  
 Section 106 Consulting Party Consultation Meeting  
 September 23, 2014

DATE: September 23, 2014 (9:00 a.m.)  
 LOCATION: ALDOT, Ninth Division, Training Facility

Name	Representing	Email	Phone
JASON GOFFINET	VolKERT, INC	jason.goffinet@volkert.com	770-298-9709
Thomas Lee	Volkert, Inc.	Thomas.Lee@volkert.com	251-342-6706
JOHN D'ARRICKE	ALDOT	-	334-242-6119
Lindy Sorrell	ALDOT	sorrell@dot.state.al.us	334-242-6709
Zac Cooper	ALDOT	cooperz@dot.state.al.us	334-242-6256
MATT TAYLOR	ALDOT - VIZ	taylor.m@dot.state.al.us	251-680-3335
Don Powell	ALDOT - SW Region	powell@dot.state.al.us	251-470-5220

Name	Representing	Email	Phone
Jeff Shelley	FHWIA	Jeff.Shelley@dot.gov	334 274-6362
Edwin Perry III	ALDOT - SW Region	perrye@dot.state.al.us	251-470-8243
Kenny Nichols	Volkert	kenneth.nichols@volkert.com	251-342-1070
David Webber	Volkert	david.webber@volkert.com	251-342-1070
Brian Ingeman	ALDOT	turnerw@dot.state.al.us	334-242-6144
Taylor Stovennire	ALDOT - Design - Location	ingramb@dot.state.al.us	334-242-6476
Buddy Cornsion	Volkert	stovenniret@dot.state.al.us	334-242-6117
Alfred. Azis	ALDOT	buddy.cornsion@volkert.com	334-242-1070
Heather Dunn	ALDOT	azisfa@dot.state.al.us	334-242-6143
Mark Mc Clay	ALDOT	dunnh@dot.state.al.us	334-242-6147
Pat M. Patterson	ALDOT Eng. Design Bureau	claym@dot.state.al.us	334-242-6311
Hendon Inge	Stop the Bridge	patm.patterson@dot.state.al.us	334-242-6633
Richard Olson	City of Mobile	hinge@herndoninge.com	432-1444
Lee Anne Wofford	AL Historical Comm	olsen@cityofmobile.org	206-5895
		lpranne.wofford@preseneala.org	334.232.2059



Name	Representing	Email	Phone
Amanda McBride	AHC / SUPD	amanda.mcbride@preserveala.org	334-242-3184
Elizabeth Merritt	National Trust for Historic Preservation	emerritt@savingplaces.org	202-588-6026
Chris Wilson	ACHP	cwilson@achp.gov	202-517-0229
Matt Erickson	ALDOT	erickson@dot.state.al.us	251-470-8201
Vince Calametti	ALDOT		
Bonnie Gwms	USA Archaeology		251-460-6562
DAN WAGNER	Christ Church Catholic	dwagner@christchurchcatholic.org	251-438-1822
Jennifer Roselius	MHDC	jroselius@christchurchcatholic.org	646-496-6105
JOHN CLEARY	USA	CLEARY@SOUTH ALABAMA DNR	
KERI CORMANIS	COM / MHDC	CORMANIS@cityofmobile.org	208-7217
Brian Tunnell	Bathsheba CASS ALABAMA	btunnell@cassalabama.com	251-433-2703
MARK BARTZETT	FHWA	mark.bartzett@dot.gov	334-274-6350

Consulting Party Meeting  
 September 23, 2014  
 ALDOT Southwest Region Bldg N  
 Mobile, Alabama

No.	Name	Company	Signature
1	Devereaux Bemis	MHDC/City of Mobile	<i>Devereaux Bemis</i>
2	David Daughenbaugh	City of Mobile Urban Forest	<i>David Daughenbaugh</i>
3	Michelle Bees	Housing First, Inc	<i>Michelle Bees</i>
4	MARYANN NABER	FHWA	
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**ATTACHMENT 3: Meeting Agenda**

**Agenda**  
**Consulting Parties Consultation Meeting**  
**September 23, 2014**

**I. Introductions**

Mr. John R. Cooper  
Transportation Director,  
Alabama Department of Transportation

Mr. Mark D. Bartlett, P.E.  
Division Administrator,  
Federal Highway Administration

**II. Status of the Project**

FHWA - Lynne Urquhart: Status of the environmental process, discussion of the 106 process

Volkert – Buddy Covington: Overview of Consulting Party coordination, responses received from the May 28, 2014 correspondence and DEIS environmental commitments

- **SHPO/AHC**
- **MHDC**
- **BAE Systems**
- **Herndon Inge (Stop the Bridge)**
- **National Trust for Historic Preservation**
- **Advisory Council on Historic Preservation**
- **Others**

**III. Next Steps**

- Discussion of possible Programmatic Agreement/MOA
- Input on mitigation measures/commitments
- Future coordination activities

**ATTACHMENT 7: Meeting Transcript**



SECTION 106  
CONSULTING PARTIES CONSULTATION MEETING

Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening  
Mobile and Baldwin Counties, Alabama

Held at the  
Alabama Department of Transportation  
1701 West I-65 Service Road North, Building N,  
Mobile, Alabama  
on September 23, 2014  
commencing at 9:06 a.m.

SECTION 106 CONSULTING PARTIES CONSULTATION MEETING

September 23, 2014 - Mobile, Alabama

<p style="text-align: center;">Page 2</p> <p style="text-align: center;">INDEX</p> <p>1</p> <p>2</p> <p>3 MARK BARTLETT, FHWA - page 3 Opening Statements and introductions</p> <p>4 LYNNE URQUHART, FHWA - page 8 Review of Section 106 process</p> <p>5 BUDDY COVINGTON, Volkert - page 13 Overview of project</p> <p>6 LEE ANNE WOFFORD, AHC - page 29 AMANDA McBRIDE, AHC/SHPO - page 31</p> <p>7 DEVEREAUX BEMIS, COM/MHDC - page 32 JENNIFER ROSELIUS, MHDC - page 35</p> <p>8 HERNDON INGE - page 36 ELIZABETH MERRITT, NTHP - page 47</p> <p>9 CHRIS WILSON, ACHP - page 54 MARY ANN NABER, FHWA - page 61</p> <p>10 BILL TUNNELL, USS ALABAMA - page 67 DAN WAGNER, CHRIST CHURCH - page 68</p> <p>11 DAVID DAUGHENBAUGH, COM URBAN FORESTRY - page 70 RICHARD OLSEN, COM - page 72</p> <p>12 ELIZABETH MERRITT, NTHP - page 72 HERNDON INGE - page 80</p> <p>13 ELIZABETH MERRITT, NTHP - page 84 HERNDON INGE - page 89</p> <p>14 ELIZABETH MERRITT, NTHP - page 91 KERI COUMANIS, COM/MHDC - page 93</p> <p>15 JOHN CLEARY, USA - page 94 DAVID DAUGHENBAUGH, COM URBAN FORESTRY - page 131</p> <p>16 CHRIS WILSON, ACHP - page 136</p> <p>17</p> <p>18</p> <p>19</p> <p>20</p> <p>21</p> <p>22</p> <p>23</p>	<p style="text-align: center;">Page 4</p> <p>1 you have anything you'd like to -- welcoming comments?</p> <p>2 JOHN COOPER: No. I'd just like to say</p> <p>3 welcome. Good to see all of you. Good to be here</p> <p>4 this morning.</p> <p>5 MARK BARTLETT: All right. Very good. So</p> <p>6 what I'd like to do is I'd like to go around the room</p> <p>7 just so everybody knows who is here. Again, I'm Mark</p> <p>8 Bartlett with the Federal Highway Administration.</p> <p>9 HERNDON INGE: I'm Herndon Inge. I am the</p> <p>10 founder, president, secretary and only member of the</p> <p>11 consulting party called Stop the Bridge.</p> <p>12 ELIZABETH MERRITT: I'm Betsy Merritt. I'm</p> <p>13 with the National Trust for Historic Preservation in</p> <p>14 Washington, DC.</p> <p>15 BONNIE GUMS: I'm Bonnie Gums with the</p> <p>16 Center for Archaeological Studies at the University of</p> <p>17 South Alabama here in Mobile.</p> <p>18 PAT PATTERSON: Pat Patterson. I'm with the</p> <p>19 Alabama Department of Transportation, cultural</p> <p>20 resource specialist.</p> <p>21 AMANDA McBRIDE: Amanda McBride,</p> <p>22 environmental review coordinator with the Alabama</p> <p>23 Historical Commission and Alabama SHPO.</p>
<p style="text-align: center;">Page 3</p> <p style="text-align: center;">PROCEEDINGS</p> <p>1</p> <p>2 MARK BARTLETT: We'll get started here in a</p> <p>3 few minutes. We were hoping that we were going to</p> <p>4 have a phone call coming in from one of the tribes</p> <p>5 that was interested in participating by phone. But</p> <p>6 we'll let them call in as needed.</p> <p>7 I appreciate every one of you being here.</p> <p>8 My name is Mark Bartlett. I'm the division</p> <p>9 administrator for the Federal Highway Administration.</p> <p>10 And again, I'd like to say thank you for coming. We</p> <p>11 received some comments on our notice of effect letter</p> <p>12 that we sent out to the consulting parties and just</p> <p>13 wanted to have a sit-down to continue that</p> <p>14 conversation in conjunction with the public hearing</p> <p>15 that we're hosting tonight.</p> <p>16 As we begin the process, what we'd like to</p> <p>17 do is go around the room and kind of introduce</p> <p>18 ourselves. We've got plenty of room up at the table.</p> <p>19 So if you guys want to come on up and fill in the</p> <p>20 table, that would be great.</p> <p>21 But before we get started, I'd like to ask</p> <p>22 -- Mr. Cooper has joined us this morning, Director</p> <p>23 Cooper from the department. I'd like to just ask do</p>	<p style="text-align: center;">Page 5</p> <p>1 LEE ANNE WOFFORD: I am Lee Anne Wofford,</p> <p>2 Deputy State Historic Preservation Officer with the</p> <p>3 Alabama Historical Commission.</p> <p>4 JOHN CLEARY: John Cleary. I'm a faculty</p> <p>5 member of the University of South Alabama, civil</p> <p>6 engineering.</p> <p>7 CHRIS WILSON: Chris Wilson. I work for the</p> <p>8 Advisory Council on Historic Preservation in</p> <p>9 Washington, DC. I'm glad to see a few people I've</p> <p>10 talked to over the phone in person.</p> <p>11 DEVEREAUX BEMIS: I'm Devereaux Bemis with</p> <p>12 the Mobile Historic Development Commission and the</p> <p>13 City of Mobile.</p> <p>14 JENNIFER ROSELIUS: Jennifer Roselius also</p> <p>15 with the Mobile Historic Development Commission.</p> <p>16 RICHARD OLSEN: Richard Olsen, City of</p> <p>17 Mobile, Urban Development and Urban Planning.</p> <p>18 DAVID DAUGHENBAUGH: David Daughenbaugh,</p> <p>19 City of Mobile, the Urban Forestry Commission.</p> <p>20 VINCE CALAMETTI: My name is Vince</p> <p>21 Calametti. I'm the region engineer for the Southwest</p> <p>22 Region, Alabama DOT.</p> <p>23 MARY ANN NABER: I'm Mary Ann Naber. I'm</p>

2 (Pages 2 to 5)

SECTION 106 CONSULTING PARTIES CONSULTATION MEETING

September 23, 2014 - Mobile, Alabama

<p style="text-align: right;">Page 6</p> <p>1 the Federal Preservation Officer for the Federal 2 Highway Administration. Our headquarters are in 3 Washington. 4 LYNNE URQUHART: Lynne Urquhart, Federal 5 Highway Administration, here in the Alabama Division. 6 KENNY NICHOLS: I'm Kenny Nichols with 7 Volkert. 8 ZAC COOPER: Zac Cooper with ALDOT 9 Visualization. 10 MATT TAYLOR: Matt Taylor with ALDOT 11 Visualization. 12 LINDY SORRELL: Lindy Sorrell with the 13 Alabama Department of Transportation in Visualization. 14 DON POWELL: I'm Don Powell. I'm the 15 Southwest Region preconstruction engineer. 16 EDWIN PERRY: Edwin Perry, ALDOT, Mobile 17 design engineer. 18 JEFF SHELLEY: I'm Jeff Shelley with the 19 Federal Highway Administration. 20 MATT ERICKSON: Matt Erickson, ALDOT, 21 assistant region engineer. 22 BUDDY COVINGTON: I'm Buddy Covington. I'm 23 with Volkert Environmental.</p>	<p style="text-align: right;">Page 8</p> <p>1 here and everybody participating in this meeting. We 2 feel like this meeting is very important, and we hope 3 we have a good discussion today that will help us 4 define the path forward for the consultation process. 5 As we begin the process, what we'd like to 6 do is kind of ask Lynne Urquhart to come up and give 7 us an overview of where we're at on the 106 process 8 and how we're proceeding with the project. 9 LYNNE URQUHART: This is the wrong 10 presentation. 11 BUDDY COVINGTON: It's on the bottom. 12 LYNNE URQUHART: I'm not really competent 13 with these Macs and the laptops. They always confuse 14 me. 15 I'm going to do a quick review of the 16 Section 106 process and where we're currently at in 17 the process. 18 As most if not all of you know, the Section 19 106 process came from the National Historic 20 Preservation Act passed in 1966 and established a 21 consultation process commonly referred to as the 22 Section 106 process. This process requires federal 23 agencies to consider the effects of their projects on</p>
<p style="text-align: right;">Page 7</p> <p>1 JOHN D'ARVILLE: J.D. Arville, ALDOT Design 2 and Visualization. 3 DAVID WEBBER: David Webber with Volkert. 4 DAN WAGNER: Good morning. I'm Deacon Dan 5 Wagner from Christ Church Cathedral Episcopal Church. 6 ALFEDO ACOFF: I'm Alfredo Acoff, ALDOT, 7 environmental coordinator with the staff. 8 HEATHER DUNN: Heather Dunn, environmental 9 coordinator, Alabama Department of Transportation. 10 NATASHA CLAY: Natasha Clay, environmental 11 coordinator, Alabama Department of Transportation 12 BILL TURNER: I'm Bill Turner. I'm with the 13 ALDOT Environmental Technical section and I'm an 14 archeologist. 15 TAYLOR SToudenMIRE: Taylor Stoudenmire. 16 I'm with ALDOT Design Bureau Location section. 17 BRIAN INGRAM: Brian Ingram, ALDOT Design 18 Bureau Location engineer. 19 JASON GOFFINET: Jason Goffinet with Volkert 20 Environmental. 21 THOMAS LEE: Thomas Lee with Volkert 22 Environmental. 23 MARK BARTLETT: Thank you so much for being</p>	<p style="text-align: right;">Page 9</p> <p>1 historic properties eligible for the National 2 Register. It seeks to balance the historic 3 preservation concerns with the needs of the federal 4 agency project. Consultation among affected 5 stakeholders is for resolving conflicts between the 6 federal projects and the historic preservation 7 interests in the project area. For highway projects, 8 the ultimate responsibility for complying with the 9 Section 106 process rests with the Federal Highway 10 Administration, but individual actions may be 11 performed by the state DOT, ALDOT. 12 These are the four steps in the Section 106 13 process. Like I said, I'm sure most of y'all are 14 fully familiar with this. But for the few people here 15 that are not, the first step in the Section 106 16 process is for the federal agency to determine if the 17 proposed project has potential to cause effects to 18 historic properties. This is called initiating the 19 process and includes identification of consulting 20 parties that will participate in the process. 21 The second step, historic properties must be 22 identified. An area of potential effect is 23 delineated, including the area or areas where the</p>

3 (Pages 6 to 9)

SECTION 106 CONSULTING PARTIES CONSULTATION MEETING

September 23, 2014 - Mobile, Alabama

<p style="text-align: right;">Page 10</p> <p>1 project may cause effects to historic properties. 2 Historic properties are then identified 3 within the area of potential effect by assessing 4 whether they meet one or more of the criteria for 5 eligibility A through D. 6 Right, Pat? 7 The third step involves deciding whether the 8 project will have an effect on the identified historic 9 properties. An adverse effect occurs when a feature 10 of a historic property is altered in a manner that 11 diminishes the integrity of the property's location, 12 design, setting, materials, workmanship, feeling or 13 association. 14 And finally, when an adverse effect is 15 identified, modifications are evaluated in an attempt 16 to minimize or mitigate the adverse effect. 17 This is the step we're in right now for this 18 project. This step has four parts. The first part is 19 applying the criteria of adverse effect. And you 20 basically do that in consultation with SHPO and taking 21 into account views provided by the consulting parties 22 and the public. FHWA and ALDOT looked at this 23 project's effect on the historic property to see if it</p>	<p style="text-align: right;">Page 12</p> <p>1 So after applying the criteria of adverse 2 effect, we arrived at part B, the making a finding of 3 no adverse effect. And for part C we provided that 4 finding to the SHPO, the Advisory Council on Historic 5 Places and consulting parties by letter so that they 6 might review that information. This is where the 7 project is currently at. 8 In July we heard back from some of the 9 consulting parties indicating that you had concerns, 10 so we are meeting with you now to gather further 11 information on your concerns. 12 One of the problems with some of the 13 concerns expressed, not all but some, was that they 14 did not clearly describe how the proposed project will 15 alter any of the characteristics of the historic 16 property that qualify the property for inclusion in 17 the register. Without that information, it may be 18 challenging or even impossible to adequately address 19 those concerns. So I urge you to take this 20 opportunity to expand on that information provided. 21 After everyone has had a chance to express 22 their concerns and discuss the issues, there will be a 23 discussion of the steps needed to resolve step 3 of</p>
<p style="text-align: right;">Page 11</p> <p>1 would diminish the integrity of the property's 2 location, design, setting, materials, workmanship, 3 feeling or association. 4 As an example of this, one of the main 5 issues of the project appeared to be visual impact to 6 the districts. 7 And if I say any of this wrong, Mary Ann or 8 Pat, please correct me. 9 This seems to focus on the district's 10 setting, feeling and/or association. When we looked 11 at the setting, feeling or association of the 12 districts in proximity to Water Street, they did not 13 appear to contribute to the district. On the other 14 side of Water Street there does not appear to be 15 anything structural or otherwise supporting the 16 integrity of the historic district. Therefore, we 17 could not identify how the proposed project would 18 diminish the integrity of any of the characteristics 19 that qualify the property for inclusion on the 20 National Register. How could adding another 21 structure, even a large one, negatively impact the 22 district's integrity when nothing there actually 23 contributes to the district's integrity?</p>	<p style="text-align: right;">Page 13</p> <p>1 the process and arrive at the assessment results. 2 And that's all I have. So just very quick. 3 And Buddy? 4 MARK BARTLETT: Before we do that, I did 5 fail one of my challenges that I made for myself this 6 morning in introductions, to do a little housekeeping. 7 First off, I want to remind everybody that there are 8 restrooms around the corner here if you go out the 9 doors and come back around. And there's also a 10 breakroom that sits directly behind this door, and 11 they do have vending machines in there. If you guys 12 need anything, feel free. Buddy? 13 BUDDY COVINGTON: All right. I'm going to 14 give a little overview of the project. Some of this 15 some of you all have seen in some of our prior 16 consulting parties meetings. Some folks are new. So 17 this will explain, you know, basically how we 18 conceived the project, how we developed the 19 alternatives, and how we got to really where we are at 20 this stage. And then I'm going to further go into 21 detail about the responses that we have received from 22 our recent correspondence on the determination of 23 effects letters and discuss some of the concerns that</p>

4 (Pages 10 to 13)

SECTION 106 CONSULTING PARTIES CONSULTATION MEETING

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<p style="text-align: right;">Page 14</p> <p>1 the consulting parties that responded have about the 2 project thus far. 3 So the purpose and need is pretty simple, 4 three steps: to increase the capacity of I-10 to meet 5 predicted future traffic volumes, provide a more 6 direct route for hazardous materials. Right now 7 hazardous materials cannot go through the I-10 Wallace 8 Tunnels. They have to route through downtown or north 9 of town across the Cochrane Bridge. And certainly do 10 those two things while minimizing impacts to the very 11 important maritime industry that exists along the 12 river in Mobile. 13 In general, the need, the original design 14 for the I-10 Wallace Tunnel is for a traffic volume of 15 about 36,000 vehicles per day. That has well been 16 exceeded. In 2013 you can see it's about 73,000 17 vehicles per day. In the summertime there are peaks 18 that are much higher than that. And it's projected to 19 be in 2030 up around the range of 130,000. 20 Early on in the NEPA process we looked at, 21 okay, what are our alternatives to get across the 22 Mobile River and to provide an alternate route for 23 hazardous materials, increase the capacity across the</p>	<p style="text-align: right;">Page 16</p> <p>1 that has alternative B Prime identified as the 2 preferred. 3 This is a matrix of some of the impacts 4 associated with the alternatives. Basically we're 5 looking at currently an estimate of about 770 million 6 for the alternatives, in that range. They're all 7 similar in length. You really get to a difference in 8 impacts when you talk about economic loss. And what 9 you see on here on economic loss, alternative A, B and 10 B Prime are similar at about 6 million dollars a year 11 and alternative C is much higher in impact/economic 12 loss at 200 million dollars a year. And what that 13 represents is as you go further south or downstream, 14 there's a greater cost to the maritime industry 15 because in effect you're affecting more by being 16 further south on the coast. 17 A few other items. We've got archaeological 18 sites that are involved on alternative B and B Prime. 19 Each have one. Alternative C has four potential 20 archeological sites. And a little caveat -- we're 21 going to talk some more about archeology -- but there 22 are some places where we haven't had access yet. 23 As far as 4(f), a direct use of individual</p>
<p style="text-align: right;">Page 15</p> <p>1 river for vehicles and minimize impacts. And we went 2 out to the public and basically said: What do you all 3 think? And we got kind of a spaghetti bowl of 4 different lines. And then it turned out to be about 5 14 different alternatives. And they were wide-ranging 6 alternatives. They went north of town. Some of them 7 used the Cochrane-Africatown Bridge that's existing 8 north of town. Some of them used the Mobile West 9 Bypass. There were a smattering of downtown 10 alternatives, some across mid-bay. Some even went as 11 far as Fort Morgan and Fort Gaines on the south end of 12 the Bay. 13 And we went through an alternative screening 14 process that's well documented in the EIS and the 15 appendices. And we basically came back through that 16 public involvement and screening processes to four 17 alternatives that were carried forward in the 18 environmental process and also appear in the Draft 19 EIS, and they are downtown alternatives A, B, B Prime 20 and C. And of course, at this point and into future 21 studies, the no-build alternative is considered. 22 And our Draft EIS actually identified the 23 preferred alternative. We have an approved Draft EIS</p>	<p style="text-align: right;">Page 17</p> <p>1 structures, alternative B has a direct use of the 2 NRHP-eligible Union Hall and alternative C has a 3 direct use of the NRHP-eligible BAE Maritime Historic 4 District. Alternative A avoids the direct use and the 5 preferred alternative avoids the direct use of 4(f). 6 And then visual impacts, alternative A, 7 being closest to the historic districts in downtown 8 Mobile, we thought had the highest potential for 9 visual impacts. B and B Prime, being further south 10 from Mobile, has a moderate potential. And 11 alternative C, being the furthest south, has the 12 lowest potential of visual impact. 13 This is the entire preferred alternative. 14 And this project goes from Mobile, includes crossing 15 the river, and then it also includes widening the 16 Bayway all the way across to the 98 interchange in 17 Daphne. So in addition to the bridges over the 18 tunnels, six-lane bridges that we're focusing on 19 somewhat with regard to 106 impact, we also widen the 20 Bayway from two lanes in each direction to four lanes 21 in each direction all the way across. 22 Here's just a more detailed look at the B 23 Prime preferred alternative in downtown.</p>

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<p style="text-align: right;">Page 18</p> <p>1           So the type of bridge, proposed bridge, is a 2 cable-stayed bridge and it has an air draft clearance 3 over the Mobile Harbor Navigational Channel of 215 4 feet. In comparison -- we'll have a few different 5 bridges fly in here -- this is a representation of the 6 Golden Gate Bridge at about 220 feet of vertical 7 clearance. The next one is the proposed Mobile River 8 Bridge, cable-stayed bridge, at 215. This is the 9 Cooper Ravenel Bridge in Charleston, and it's 209. 10 The Sunshine Skyway in Tampa is 175. The Crescent 11 City Connection in New Orleans is 170. And then 12 here's our existing cable-stayed bridge in Mobile, the 13 Cochrane-Africatown Bridge, at 155. 14           This kind of transitions more into the 106 15 part of the presentation. And this is a building 16 height comparison of structures in downtown Mobile. 17 The structure on the far left is the RSA Tower. If 18 y'all came in last night, you saw the beautiful lit 19 building on the skyline that changes colors by the 20 seasons. That is the RSA Battle House. 21           The next from the left is this proposed 22 bridge. It indicates on the figure a bridge deck at 23 about 215 feet. And then the pylons, the main bridge</p>	<p style="text-align: right;">Page 20</p> <p>1           alternatives, A, B, B Prime and C, with the historic 2 districts that are within the area of potential effect 3 of the project. There is an area of potential effect 4 indicated on there as a red dash line that's kind of 5 away from the alternatives. And there are also a 6 number of spots on there that we call viewshed points. 7           The information on this map that you saw in 8 Lynne's presentation about step 2 of the 106 process 9 was developed in consultation with me and the folks 10 sitting in this room with regard to how are we going 11 to assess different points. We made field reviews 12 with the consulting parties and with SHPO to establish 13 the APE and to establish the different viewshed 14 analysis points that were a part of the cultural 15 resources reports that are included in the document. 16 But this is a representation of the historic 17 resources, if you will, downtown. 18           Recently, prior to the Draft EIS getting 19 approved, we sent a consulting parties package with a 20 determination of effect. We sent it out on May 28th. 21 And the graph on the chart basically says we have a 22 number of resources, historic districts, in downtown. 23 We also have a few national historic landmarks. They</p>
<p style="text-align: right;">Page 19</p> <p>1           piers that hold up the cables, is about 515 feet or so 2 to the top of those. 3           And then in comparison, some other large 4 buildings, the Riverview Plaza, the existing Cochrane 5 Bridge, and then they step all the way down to like 6 the Old Southern Market, which is a national historic 7 landmark in town, at about 50 feet. 8           So the history of our consulting parties 9 coordination, this has gone on for quite some time, 10 beginning back in 2003, and then additional 11 coordination in 2006. This is kind of we're 12 transitioning into a more formal EIS process. We also 13 added a number of consulting parties through 2010. 14 What was the National Trust and the Alabama Historical 15 Commission has now blossomed out to 23 different 16 consulting parties, and they range from national 17 organizations to tribes to municipalities with 18 interest in the project all the way down to individual 19 citizens and local groups that have an interest in 20 historic resources in Mobile. 21           This is an exhibit from the EIS. And it's 22 been in most of the consulting parties packages since 23 2010. It is a representation of the four</p>	<p style="text-align: right;">Page 21</p> <p>1           are all eligible for the National Register if they're 2 not already on it. And we propose that the 3 determination of effect is no adverse visual effect. 4 And we do have a direct use for the Union Hall with 5 alternative B and the BAE maritime industry for 6 alternative C. 7           So that was what you all were asked to 8 review in May. And I'm going to go through now what 9 responses we received from that coordinated activity. 10           We received a response from SHPO, or the 11 Alabama Historical Commission. And they're concerned 12 about potential visual effects to Lower Dauphin Street 13 and Church Street East Historic Districts and request 14 an opportunity for context-sensitive design and a tree 15 planting plan to be discussed as potential mitigation 16 measures. They also would like for that discussion to 17 include lighting vibrations, aesthetics and access to 18 the Battleship Park in the future agreement. 19           They were concerned about the effects to the 20 Union Hall, and they proposed that a study be 21 organized regarding what the resources are 22 representing organized labor within the state. Here 23 the Union Hall is eligible under criterion A, and it</p>

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<p style="text-align: right;">Page 22</p> <p>1 actually has had some HABs documentation done on it. 2 But they would like to see that go further and see how 3 that relates to union labor structures statewide. 4 And then we'll have to talk -- and I'll 5 probably let Turner talk about this when we get into 6 the discussion about the additional archeology that 7 would have to be agreed upon, possibly in a 8 programmatic agreement. But we have a number of 9 resources in downtown Mobile that we encounter 10 whenever we construct a building or a roadway. And 11 certainly ALDOT is aware of the importance of what is 12 in downtown Mobile and what we find when we go 13 digging. So that could possibly be another item for 14 programmatic agreement, is the step approach to 15 completing the archeology agreement. 16 There's also some nuance in this in that we 17 don't have a selected alternative yet. So there are 18 different effects to archeology based on what we know 19 now and what we may be able to avoid in the future. 20 The Mobile Historic Development Commission 21 responded. They started out saying most concerns 22 would be handled or minimized through a programmatic 23 agreement and requested an opportunity for</p>	<p style="text-align: right;">Page 24</p> <p>1 further than 200 feet from the proposed project. 2 Also Herndon said it might could lessen 3 impacts if we located the bridge further north or 4 further south of the historic resources in Mobile. 5 BAE Systems was basically in concurrence 6 that alternate C would have a direct effect on the BAE 7 Maritime Historic District. And then they also 8 provided a comment that was not necessarily 106 9 related, but they would like coordination with BAE and 10 with Austal and the Alabama State Port Authority on 11 some potential effects to a privately owned bridge on 12 their property. 13 The National Trust, Betsy's group, responded 14 that methodology for assessing visual impacts 15 understates impacts on historic districts as a whole. 16 There would be adverse visual impacts from night 17 lighting. Tree cover could be lost during storm or 18 other event. There would be adverse visual impacts on 19 Church Street East, Lower Dauphin Street, the Union 20 Hall, and the Old Southern Market or Old City Hall. 21 And they requested additional information to 22 assess noise impacts to Oakdale and Church Street East 23 Historic Districts. Additional information requested</p>
<p style="text-align: right;">Page 23</p> <p>1 context-sensitive design and a tree planting plan as 2 potential mitigation. 3 They're concerned about monitoring and 4 avoiding construction impacts, particularly impacts 5 related to noise, storm water and lighting. They were 6 interested in discussing a future landscape plan and 7 assuring that we have an architecturally appealing 8 bridge design. 9 They also commented on what would happen 10 beneath the bridge. I think I'll let Devereaux talk 11 more about that comment. I think what they mean is 12 how are we going to utilize the area under the bridge 13 once it's constructed. 14 Herndon Inge replied concerned about impacts 15 to all the historic districts and the Old Southern 16 Market, which is a national historic landmark. He was 17 concerned about visual impacts to the districts and 18 the market, disruption during construction, noise and 19 vibrations, both during construction and after 20 construction. He was concerned about air pollution 21 potential effects and in general felt like there would 22 be adverse impacts within 200 feet of the proposed 23 project and that the visual impacts would extend</p>	<p style="text-align: right;">Page 25</p> <p>1 to assess vibration and specifically a sophisticated 2 scientific analysis was needed to make that 3 assessment. 4 They wanted further discussion about access 5 to the USS Alabama. Consultation certainly was 6 available to resolve disagreements. And then on 7 behalf of the Advisory Council, they requested some 8 extra time for the Advisory Council to respond. And 9 that request was granted with additional time. 10 The Advisory Council is similar in their 11 response to the National Trust. Their concerns were 12 indirect, adverse, visual, auditory and vibratory 13 effects. They thought there was insufficient 14 information to document the effects determination of 15 no adverse effect related to noise, views and 16 vibrations. They felt potential for visual, noise and 17 vibrations to adversely affect the characteristics 18 that qualify properties for the NRHP. They would like 19 additional information on the undertaking, preferred 20 alternative and specific studies completed. 21 They recommended that we share the Draft EIS 22 with the consulting parties and continue consultation 23 to explore ways to avoid, minimize and/or mitigate</p>

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<p style="text-align: right;">Page 26</p> <p>1 adverse effects. And they also requested a meeting, 2 and that's one of the reasons why we're here today. 3 So in the May 28th correspondence, it was 4 basically just a determination of effect and seeking 5 comment and consulting parties. And at that time you 6 as consulting parties may or may not have been aware 7 of some of the environmental commitments that ALDOT 8 was putting into their Draft EIS. And the Draft EIS 9 has been distributed to the consulting parties but 10 just in a general review sense as far as it was a 11 notice of availability. 12 And so here I want to -- I understand some 13 of y'all may not have read that 2,000-page book cover 14 to cover. And so I wanted to just highlight some of 15 the environmental commitments that are related to 106 16 that are included in the document. 17 With regard to lighting -- and these are 18 somewhat watered down from what's in the document. 19 They're more elaborate in the second page of the 20 document for each one. But ALDOT is committed to 21 designing the light levels so that they will be less 22 than or equal to what they are now, the existing 23 levels, at the right-of-way boundary for the proposed</p>	<p style="text-align: right;">Page 28</p> <p>1 consulting parties on bridge aesthetics. And that 2 would continue on as the design of the project 3 develops. 4 And then vibrations, the Draft Environmental 5 Impact Statement has a draft vibration effects report 6 in it, a study. And ALDOT has conducted some initial 7 piledriving down in the area. And what they would 8 like to do is take that information and evaluate 9 potential vibration impacts for piledriving to help 10 identify both construction methodologies and a buffer 11 to avoid vibration impacts to historic properties. 12 And the meat of this commitment is to provide a 13 monitoring system both before, during and after 14 construction so that no buildings have an adverse 15 effect from vibration. 16 Other indirect effects in the area, noise 17 effects were identified for Oakdale Historic District 18 and for Church Street East Historic District. Both of 19 these districts are located in developed areas in 20 close proximity to the interstate system. The 21 properties were viewed, and the increase in noise over 22 the no-build condition in 2030 and any of the build 23 alternatives was so small that it's almost inaudible.</p>
<p style="text-align: right;">Page 27</p> <p>1 project. 2 Archeology, ALDOT is committed to phase 2 3 archaeological testing, and it will be coordinated 4 with SHPO and be performed as part of the 5 investigation of the preferred alternative if the 6 Final EIS cannot avoid those particular sites. 7 USS Alabama, ALDOT will coordinate with SHPO 8 and USS Alabama Battleship Memorial Park Commission 9 and the consulting parties to determine the location 10 and type of signs for the USS Alabama Battleship Park. 11 And some of these things in parentheses are 12 just notes as to where these commitments appear in the 13 Draft EIS. 14 Visual effects and opportunities to 15 incorporate context-sensitive design will be discussed 16 with the SHPO and 106 consulting parties. And as part 17 of that, ALDOT would like to discuss a reasonable 18 planting plan that will be developed in an effort to 19 maintain the tree canopy. And that's somewhat 20 consistent with comments from both SHPO and the Mobile 21 Historic Development Commission. 22 Aesthetics, coordinate during the design 23 phase with stakeholders within the city, SHPO,</p>	<p style="text-align: right;">Page 29</p> <p>1 So that was the finding on noise. 2 And also there is an air quality report in 3 the Draft EIS, and the project is not expected to 4 cause any air quality impacts. 5 So with that, I think we're going to, I 6 guess, just open it up to additional comments from our 7 consulting parties or the ones who have provided a 8 response and perhaps others who haven't provided a 9 response. 10 MARK BARTLETT: You bet. Thank you, Buddy. 11 That was a great overview. Did everybody get a copy 12 of Buddy's handout? He has a presentation that you 13 guys can get a copy of. There was a lot of 14 information in there. We really appreciate him going 15 through that. 16 I think what we'd like to do now is open it 17 up to the parties that actually submitted information, 18 concerns with the finding that we had sent out to all 19 of the consulting parties. I guess we'd like to begin 20 with SHPO and ask if they have any comments that they 21 would like to share about their concerns. 22 LEE ANNE WOFFORD: I'm Lee Anne Wofford with 23 the Alabama Historical Commission. We got to drive</p>

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<p style="text-align: right;">Page 30</p> <p>1 around yesterday with Devereaux, Chris and Betsy. It 2 was good to see it in person. Elizabeth Brown, who I 3 replaced, she dealt with this project mostly through 4 its history. So I'm sort of coming in and taking over 5 from her. 6 But it was good to see it in person. I do 7 just agree with our original comments. We do have 8 concerns about the visual impacts to some of the 9 districts just from seeing it from certain vantage 10 points and Devereaux pointing out kind of where the 11 bridge would be. I guess we just stand by our 12 comments on the visual effects. 13 The noise, I know that Buddy, when he did 14 his presentation before, they had the really cool 15 graphic of what the bridge was going to look like. 16 Could you talk about again where the bridge starts and 17 the rise, like how it's going to get to the ceiling? 18 ALFEDO ACOFF: We have that. 19 LEE ANNE WOFFORD: You do? Okay. That was 20 a concern that we had. I think Devereaux pointed out 21 a discrepancy when the bridge was going to be lower 22 but the rise was still the same, but now it's higher 23 and the rise is the same? Devereaux, do you remember?</p>	<p style="text-align: right;">Page 32</p> <p>1 the AHC. I don't have much to add to what Lee Anne 2 said. We're still not convinced that there are not 3 going to be any impacts or adverse impacts. We think 4 we need a little bit more information in order to 5 really assess the impacts here. 6 As far as archeology goes, we really need to 7 just wait until all of phase 1 and any phase 2 testing 8 have been completed and we'll address that later. 9 MARK BARTLETT: Very good. Devereaux? 10 DEVEREAUX BEMIS: Sure. I think one thing 11 that y'all could address, which is really good, is the 12 vibration study, making sure you're monitoring it all. 13 We would like to be involved in some of the buildings 14 you're monitoring, St. Matthews, Council School, 15 Prince of Peace. You know, those are the three major 16 historic buildings that are nearby. And, of course, 17 some of the things in Fort Conde. Since the City does 18 own those, we would like to make sure that those are 19 done. I assume that Christ Church wants theirs 20 monitored and, of course, nearby is the museum as 21 well. 22 One thing in talking to my boss the other 23 day, with the Maritime GulfQuest coming onboard, it's</p>
<p style="text-align: right;">Page 31</p> <p>1 DEVEREAUX BEMIS: Well, I guess the question 2 is: Is the rise the same? Because originally it was 3 188 feet? 4 BUDDY COVINGTON: 190. 5 DEVEREAUX BEMIS: 190 and now it's 215. And 6 it seems to be starting from the same place. What's 7 happening, you're increasing the incline? 8 BUDDY COVINGTON: David, do you want to jump 9 in on that? 10 DAVID WEBBER: Well, it's always been 4 11 percent, Devereaux, so it's a little bit different 12 where it starts and ends. 13 DEVEREAUX BEMIS: So you'll be pushing it 14 back from Virginia Street to a little further -- 15 DAVID WEBBER: It's incremental. 16 DEVEREAUX BEMIS: Well, I figured that, but 17 I was just wondering where it was. 18 DAVID WEBBER: It's hard to ascertain. But 19 really Broad Street is just where the ramps are going 20 to start, just east of Broad Street. 21 MARK BARTLETT: Do you have any other 22 comments? 23 AMANDA MCBRIDE: This is Amanda McBride with</p>	<p style="text-align: right;">Page 33</p> <p>1 not part of historic, but I think the City may ask 2 that the cruise terminal and the GulfQuest be 3 monitored as well, as well as other places. 4 One of the things that the study does -- a 5 lot of this is incremental. The study is that you go 6 from -- it's two decibels more, that sort of thing, 7 which is kind of hard to figure out what that actually 8 is with that kind of thing. But I'd like to -- I know 9 you're not going to do any kind of vertical things or 10 anything like that from the Draft EIS. But maybe 11 during the design phase, anything that could be done 12 to help with any kind of noise mitigation. I don't 13 know. I don't know what you would do, if you can do 14 something like that. 15 And we were talking about this in the office 16 the other day. With so many technological changes 17 coming with lighting and that sort of thing, we would 18 suggest -- we realize you'd have the overall lighting 19 plan -- but that the final lighting plan not be done 20 until you get close to completion so if there are 21 technology changes that make things, you know, easier 22 to control and cheaper, for that matter, that could be 23 sort of phased in as you get closer and closer to the</p>

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<p style="text-align: right;">Page 34</p> <p>1 construction.</p> <p>2 Let's see. The one thing we were asking for</p> <p>3 with the landscape plan is we don't agree that there's</p> <p>4 no visual effect because there's certain places where</p> <p>5 the bridge is going to stick out. But, you know, we</p> <p>6 all know the bridge is coming. And what we were</p> <p>7 hoping is that ALDOT would work with David, who is the</p> <p>8 tree commission person for the City of Mobile, to</p> <p>9 develop some canopies that would sort of mitigate that</p> <p>10 visual effect. I think that is one of the things that</p> <p>11 we're very concerned about. And, you know, the</p> <p>12 lighting and the noise are things that we really want</p> <p>13 to see taken care of. Because it really does affect</p> <p>14 the quality of life of the people in the nearby</p> <p>15 historic districts. And I think that's important.</p> <p>16 One of the things that came out of my talks</p> <p>17 with my boss and the mayor is what's going to happen</p> <p>18 underneath the bridge, as Buddy said. You know, how</p> <p>19 is it going to be utilized, can it be utilized. We're</p> <p>20 already -- we've cut ourselves off from our historic</p> <p>21 waterfront already, particularly because it's so</p> <p>22 industrial looking or so industrially used, that we</p> <p>23 would like to see the City, I think -- and this is</p>	<p style="text-align: right;">Page 36</p> <p>1 pre and post -- we do precrack surveys, preanalysis.</p> <p>2 We've got some projects right now -- Little Lagoon in</p> <p>3 south Baldwin County we've done; GM&amp;O Railroad</p> <p>4 Building, we did that for I-165, we did a precrack</p> <p>5 survey. The intent is not to cause any damage. But</p> <p>6 when there is damage, ALDOT requires a bond from the</p> <p>7 contractors, a bond that the contractor has to repair</p> <p>8 to satisfactory any damage.</p> <p>9 JENNIFER ROSELIUS: Okay. Thank you. The</p> <p>10 other point I just wanted to make is to reiterate we</p> <p>11 would like to have more time to comment and review</p> <p>12 after the complete archeology studies are done. And</p> <p>13 just making sure that the commitment in the Draft EIS</p> <p>14 with respect to lighting, noise, vibrations, all of</p> <p>15 this gets incorporated into the final project.</p> <p>16 MARK BARTLETT: I don't know if we had</p> <p>17 anybody come in from BAE Systems.</p> <p>18 Herndon?</p> <p>19 HERNDON INGE: First of all, there was a</p> <p>20 book 15 or 20 years ago that said men are from Venus</p> <p>21 and women are from Mars or whatever it is. Well, I</p> <p>22 was an English major, and you math guys look at things</p> <p>23 one way and I look at it the other way. I respect</p>
<p style="text-align: right;">Page 35</p> <p>1 something that we're just beginning to discuss -- but</p> <p>2 see something happen underneath the bridge that makes</p> <p>3 it an amenity for the City and not just a no man's</p> <p>4 land, which, you know, in ten years we're going want a</p> <p>5 Big Dig instead of having a pleasant space underneath.</p> <p>6 Anything else, Jennifer?</p> <p>7 JENNIFER ROSELIUS. Jennifer Roselius. I</p> <p>8 did want to say that our commission is comprised of a</p> <p>9 lot of stakeholders in this community that are</p> <p>10 interested in preservation, and on the whole we've</p> <p>11 been very pleased with this plan and the fact that</p> <p>12 you're avoiding direct impacts to the historic</p> <p>13 districts and for the most part to historic buildings,</p> <p>14 with the exception of the Union Hall.</p> <p>15 I did have a question. I agree with all the</p> <p>16 comments that have been said about monitoring and</p> <p>17 mitigating the damage from vibrations. If there is</p> <p>18 damage to these structures, what sort of plan would be</p> <p>19 in place to help repair or stabilize those structures?</p> <p>20 MARK BARTLETT: Do you want to talk to that?</p> <p>21 VINCE CALAMETTI: Well, this vibration</p> <p>22 analysis has helped us on previous and current</p> <p>23 projects. Typically what we do is we go in and we do</p>	<p style="text-align: right;">Page 37</p> <p>1 that y'all say that we need to increase the traffic</p> <p>2 flow. I respect that.</p> <p>3 But I'm looking at it from my perspective.</p> <p>4 I'm looking at it as a person, a resident, a</p> <p>5 homeowner, a property owner. I think one of the -- I</p> <p>6 think on page 2 it talks about minimizing impacts to</p> <p>7 Mobile's maritime industry. Okay. Fine. But what</p> <p>8 about my quality of life and my children and my</p> <p>9 grandchildren? And what about the cemetery? And what</p> <p>10 about where I live and my office? That's what I'm</p> <p>11 talking about. I'm not talking about minimize the</p> <p>12 impacts to the maritime industry.</p> <p>13 And the Chamber of Commerce acts as if the</p> <p>14 maritime industry is the only commerce in Mobile.</p> <p>15 Well, it's not. It's the people who live here. And</p> <p>16 that's not one of the fundamental purposes and one of</p> <p>17 the fundamental needs that's listed on page 2, is the</p> <p>18 people that live here, the people that own property,</p> <p>19 the people that walk their dog at night, the people</p> <p>20 that walk their children on the waterfront. Let's</p> <p>21 talk about those people. Because that's who I</p> <p>22 represent in my organization of one.</p> <p>23 So let me tell you another story. Before</p>

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<p style="text-align: right;">Page 38</p> <p>1 many of you were born -- not Vince, but before other 2 people, some of the other people in the room were born 3 -- we fought this battle with the I-210 connector. 4 And the Riverview Hotel at that time was owned by 5 Alcoa; had just built it. And they were talking about 6 the I-210 connector separating Mobile's downtown from 7 the waterfront -- Mobile's downtown from the 8 waterfront. And so Alcoa said look at the Hyatt in 9 Birmingham on I-20 and I-59. The people are not going 10 to rent the hotel rooms on the side of the interstate 11 because the noise and vibration is so bad. 12 And so Alcoa rented a plane and about five 13 of us went to talk with Ray Bass, the state highway 14 director, and we said: You know, we live here. And 15 Alcoa has just spent all this money on the Riverview 16 and, you know, we live here. We are earning a living 17 here. 18 And I'll never forget it. Ray Bass was 19 sitting in his office and he said -- and he stood up 20 and he opened his door and he said: What does it say 21 on my door? He said: It says highway director. 22 That's what -- my job is to represent highways. He 23 said: The bigger and the more expensive, the more I</p>	<p style="text-align: right;">Page 40</p> <p>1 Well, we made that mistake and we now don't 2 need to repeat that mistake. 3 Well -- and Buddy, you did a great job. But 4 my draft of the bridge says the travel deck is 210 5 feet. Well, the reason that I got all over the 6 television about three feet higher than the Golden 7 Gate Bridge is because the travel deck of the Golden 8 Gate Bridge is 220 feet and at one time y'all were 9 talking about 223 feet. So the travel deck is either 10 going to be 210 on my edition or 215 on this edition, 11 but another time we were talking 223. And the 12 supporting structure on my edition is 490 feet, and 13 now it's 25 feet more than that. 14 And I understand -- and the pretty pictures 15 up there, the pretty nighttime pictures, that's nice. 16 But I keep being told: Oh, we haven't decided on a 17 final structure yet. We don't know what it's going to 18 look like, we don't know how tall it's going to be. 19 You know, we're going to get a cruise ship, we're not 20 going to get a cruise ship. 21 Okay. So let's talk about what we have 22 learned or should have learned from history. In 23 1968 -- that's awhile ago -- the U.S. Department of</p>
<p style="text-align: right;">Page 39</p> <p>1 like it. Get the hell out of my office. 2 And that's what happened. And so this lady 3 put him down. And she sued Elizabeth Dole and she 4 sued ALDOT and she said: Okay, that's the way you 5 want to treat us, that's why they have federal judges. 6 So today, with our court reporter, is my 7 opportunity to put it on the record so the federal 8 court can look at what's happening today. Okay. I've 9 been in this battle -- Betsy and I have been in this 10 battle a long time and we're here to stay. Okay. 11 One of the greatest city planners maybe in 12 the world but certainly in the south, Andrés Duany, 13 talked about -- a couple of years ago said he would 14 encourage us to think 20 years in the future as we 15 make decisions about how to develop our city. 16 You go to Charleston, you go to New Orleans. 17 Well, they didn't -- and you go to Washington and 18 Georgetown. They didn't make the decision -- they 19 didn't make the bad decision that we did of urban 20 planning to tear down so many of our historic 21 structures. Because they said: Oh, we need a civic 22 center, we need an auditorium, we need to chop all 23 that down and put in parking lots.</p>	<p style="text-align: right;">Page 41</p> <p>1 Transportation had published a report called 2 The Freeway in the City. It says: 3 "Highway transportation cannot be allowed to 4 function apart from or in conflict with the 5 environment. How can needed mobility contribute to 6 the important social goal of preservation of historic 7 sites? An urban highway should be located and 8 designed so as to enhance rather than destroy a city's 9 best attributes. A freeway should not encroach upon a 10 park, playground, square, plaza or other open spaces. 11 Highway location and design must consider the 12 viewpoint of the area residents." 13 That's me and us. 14 In 1997 the Federal Highway Administration 15 and the Texas Department of Transportation published a 16 finding called Social and Economic Effects of 17 Elevated, Depressed and At-Grade Freeways. And it 18 says: 19 "Elevated freeway designs raise particular 20 questions concerning noise and air quality impacts. 21 Both business and residential survey responses 22 indicate a deterioration in the neighborhood quality 23 due to increases in noise, pollution and crime levels,</p>

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<p style="text-align: right;">Page 42</p> <p>1 even after construction." 2 In the '90s the U.S. Secretary of 3 Transportation, highway improvement -- and this is 4 going to be, I promise -- he promises -- going to be 5 in the Environmental Impact Statement. Excerpts from 6 these publications that some of them are almost 50 7 years old. Anyway, it's there. Read it. 8 Yet the reports that we heard this morning 9 said no adverse visual effect on Church Street East, 10 Lower Dauphin Street, Oakdale, Maysville, Union Hall. 11 No adverse visual effect. Union Hall, Old Southern 12 Market, Government Street Presbyterian Church. I 13 don't see how they could -- anyone could find that 14 with a structure that's going to -- with pylons that 15 are going to be 523 feet tall. 16 Now, you know, I guess what we could do is 17 just put a big tarpaulin all over downtown Mobile with 18 a tree cover and say: Okay, you can't see through the 19 tree cover, so what difference does it make what's 20 going up 523 feet in the air right over downtown? 21 Okay. Section 106 says: Consider the 22 direct and indirect adverse impacts. What I want to 23 talk about is the indirect adverse impacts. Visual</p>	<p style="text-align: right;">Page 44</p> <p>1 Admiral Semmes Hotel, National Register of 2 Historic Hotels, built in 1920. 3 Government Street Presbyterian Church built 4 in 1835. The next time you walk out of a wedding or a 5 funeral at Government Street Presbyterian Church, just 6 visualize this bridge at 525 feet, pylons. 7 Christ Church Cathedral, built in 1835. 8 Fort Conde, built in 1822. 9 LaCledde Hotel, built in 1855. 10 So those are the historic places. 11 Other buildings. Why in the world would RSA 12 call it -- continue to call their hotel the Riverview? 13 They're going to have to call it the bridge pylon 14 view. 15 RSA Tower, you know, the south windows, all 16 they're going to see is the bridge. 17 The RSA BankTrust Building, all they're 18 going to see is the bridge. 19 Commerce Building, south side of the 20 Commerce Building, FBI building, Metro Plaza, Holiday 21 Inn Hotel, the cylinder down there -- all they're 22 going to see is the bridge. And to say no visual 23 impact is sticking your head in the dirt.</p>
<p style="text-align: right;">Page 43</p> <p>1 intrusion can cause a direct and adverse impact on 2 national historic landmarks. It can radically change 3 the feature of the setting that is vital to defining 4 the character of the place. 5 As an example, there was a wind farm that 6 was off Long Island -- that was proposed to be off 7 Long Island. And they said it doesn't -- there's not 8 a direct adverse impact if it's out in the water. But 9 the federal judge in that case said there can be a 10 visual impact on Long Island. And there was. And so, 11 as I understand, it was not -- the wind farm was not 12 built. 13 Well, one day I rode around. And since 14 we're talking about 50 years of highway studies, 15 highway design studies, Cooper Riverside Park, the 16 bridge -- there will be no Cooper Riverside Park 17 because almost above it will be 525 feet of pylons and 18 whatever kind of suspension, cable suspension bridge. 19 The plaza, I don't know what the city is 20 going to do with the old courthouse location at Royal 21 and Government Streets, but that's a plaza. So I've 22 talked about parks, I've talked about plazas. 23 Old City Hall, French market, built in 1856.</p>	<p style="text-align: right;">Page 45</p> <p>1 Fort Conde, Charlotte House, built in 1822 2 to 1824, on the National Register. 3 The Chandler House, built in 1844, on Church 4 Street. 5 The Waring Texas House, built in 1840. 6 The Archbishop's residence built in 1860. 7 Oakleigh Mansion, 1833. 8 The Cluis House on Saint Anthony Street 9 built in 1857. 10 Now we need to talk about the neighborhoods. 11 To say that there's no visual impact is just sticking 12 your head in the dirt. 13 Okay. In my letters to Mr. Calametti I've 14 tried to cite all of the bridges that were built in 15 close proximity to a central business district and 16 according to highway planning at the time, but they're 17 torn down. Embarcadero Freeway. 18 And I've said before that they wanted to put 19 an elevated expressway along -- right between 20 Jackson -- in downtown New Orleans between Jackson 21 Square and the Mississippi River. And a friend of 22 mine that I went to college with was in that fight at 23 the time. This is when I had brown hair at that time.</p>

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<p>1 But he took his son to the levee and he pointed up in 2 the sky and he said: Son, see that elevated 3 expressway? 4 And the son said: No, I don't. 5 And the dad said: That's because of me. 6 And so we don't have to build it and tear it 7 down. We don't have to spend a billion dollars. You 8 know, the price keeps going up and down, and that's 9 fair. Well, almost a billion dollars. We don't have 10 to build a bridge like they did in the Embarcadero 11 Freeway and then tear it down, spend a billion dollars 12 and then tear it down, say: You know, we made a 13 mistake building that bridge. 14 Well, St. Louis, the Alaskan Way Viaduct in 15 Seattle, Portland, Boston. 16 In this 1968 study by the U.S. Department of 17 Transportation -- 18 MARK BARTLETT: Herndon, if I could, if 19 you'll kind of stick to what our impacts are here? 20 HERNDON INGE: That's what I'm talking 21 about. 22 MARK BARTLETT: Well, I understand that. 23 Your general opposition to the overall building of the</p>	<p>1 questions. Would it be okay for me to ask some 2 followup questions? 3 MARK BARTLETT: Sure. 4 ELIZABETH MERRITT: One question relates to 5 the statements that were made by Lynne from the 6 Federal Highway Administration regarding nothing 7 contributes to the integrity of the Church Street East 8 Historic District. And I wonder if it would be 9 helpful to put up a map of the district and shed some 10 light specifically on exactly what you all feel has no 11 integrity. Because I was a little confused by that 12 statement. 13 MARK BARTLETT: Would you like to kind of 14 give us an overview of your perspective? And then 15 we'll kind of walk through each of these areas. 16 Maybe we'll ask Buddy and Lynne to write down the 17 questions so we can talk about them in sequence. But 18 what I'd like to do in this section is to kind of get 19 kind of clarifying statements from your letter that 20 you provided to us as a response. 21 ELIZABETH MERRITT: Okay. Well, my letter 22 was summarized in the slide presentation. In general, 23 we feel that the impacts will be adverse, particularly</p>
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<p>1 project, we appreciate that perspective. We're trying 2 to get to the impacts, direct impacts and how the 3 features that we are impacting will happen. 4 HERNDON INGE: The route selected, the route 5 selected is -- Betsy Merritt -- 6 MARK BARTLETT: We've got a lot of time to 7 talk about it. I guess I would like to ask that you 8 kind of wrap up -- 9 HERNDON INGE: I will wrap up. I will wrap 10 up. 11 MARK BARTLETT: -- to give time for some 12 others. 13 HERNDON INGE: I will wrap up. About 30 14 years ago Betsy wrote in a brief to the federal court 15 that an I-210 connector is going to be a guillotine. 16 And that's what the bridge -- I mean the present route 17 is going to be. It's a guillotine. 18 Okay. I'm done. 19 MARK BARTLETT: Thank you. There will be 20 plenty of time to talk about all of these different 21 aspects. 22 Betsy? 23 ELIZABETH MERRITT: I actually have several</p>	<p>1 the visual impacts, and we're especially concerned 2 about the vibration impacts. I think the proposed 3 approach to many of these issues is we'll figure it 4 out later. And because of the fact that a specific 5 preferred alternative has been identified, it's 6 possible to figure out a lot more now. And it's 7 better not to defer the evaluation of these impacts 8 such as vibration but to do more research now to 9 identify what the adverse impacts truly will be. 10 We also, I guess, in general -- if you want 11 me to sort of generalize our perspective -- we'd like 12 to, in order to address some visual impacts, we'd like 13 to see more exploration of different bridge types, 14 bridge designs, that would not be so visually 15 intrusive. In our view, the cable-stayed bridge is 16 sort of an easy, kind of knee-jerk reaction and that 17 something more creative and less visually intrusive 18 would be important to explore. 19 What about a design competition rather than 20 just copying the formulaic cable-stayed design? That 21 could not only minimize visual impacts by dramatically 22 reducing the height of the bridge structure but could 23 actually give the City something iconic to be proud</p>

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<p style="text-align: right;">Page 50</p> <p>1 of. And so we'd like to see further exploration of 2 that. 3 We'd also like to see further emphasis on 4 trying to shift the alignment of the bridge farther 5 away from downtown as a way of reducing the visual 6 impacts as well. 7 Let's see. Our other objections to the no 8 adverse effect determination, I think, were pretty 9 fairly summarized in the slide. Access to the 10 USS Alabama, which was also raised by the SHPO's 11 office. 12 Noise impacts. We asked for more data about 13 what noise assumptions were used in concluding the 14 two-decibel increase, so we'd be very interested in 15 understanding more about that, including, for example, 16 what assumptions were used about noise levels for the 17 no-build alternative and what assumptions were used at 18 different locations along the structure. 19 I had -- do you want me to go on with my 20 additional questions? 21 MARK BARTLETT: Sure. 22 ELIZABETH MERRITT: I wanted to have some 23 additional information on why the height was changed.</p>	<p style="text-align: right;">Page 52</p> <p>1 commitments has some errors in it. And I don't know 2 if you want to put that slide back up. But the 3 environmental commitments slide was offered during the 4 presentation as a response to our objections by the no 5 adverse effect determination and as a way forward, I 6 guess is how I understood it, to come up with some 7 environmental -- if I understand it correctly, I think 8 the idea is to propose some environmental commitments 9 that would address these impact areas without 10 admitting that they're adverse impacts. 11 But I wanted to just point out some errors 12 on the slide. Do you need additional information? It 13 doesn't have a page number, but it's toward the end. 14 MARK BARTLETT: It's almost at the end, if 15 you start at the bottom. There we go. 16 ELIZABETH MERRITT: There we go. Okay. So 17 the first one is under archeology. It references us 18 to section 4.16.7 in the Draft EIS, which is on page 19 203. And that's actually a section about viewshed. 20 It's not a section about archeology. So that's one 21 thing that needs to be looked at. 22 The next one on the USS Alabama refers to 23 section 4.16.4.2, which is on page 172 of the Draft</p>
<p style="text-align: right;">Page 51</p> <p>1 I wanted to have some additional information 2 on the businesses to be relocated and whether the City 3 has carefully evaluated potential historic 4 significance of any of that area. And so, for 5 example, I know that in the EIS it mentions one 6 business in particular that's been a fourth generation 7 family-owned business in its place for 50 years. Has 8 any evaluation been done as to whether that's 9 historically significant? There's an assumption that 10 all of the business demolition will be nonhistoric. 11 And I just want to make sure that we double check that 12 information. 13 And with respect to vibration, I mentioned 14 it's all we'll just figure it out later. We have lots 15 of specific concerns about vibration. For instance, 16 we feel it's very important to commit to maximum 17 vibration levels of 0.2 -- or 0.5 or less inches per 18 second, which is appropriate for historic properties, 19 and that further research needs to be done about the 20 specific soil conditions here and whether there are 21 conditions that might exacerbate vibration impacts. 22 I'd also like to point out that one of the 23 slides in the presentation about the environmental</p>	<p style="text-align: right;">Page 53</p> <p>1 EIS. And that's a section about Lower Dauphin Street 2 Historic District. It's not about the USS Alabama. 3 BUDDY COVINGTON: Okay. 4 ELIZABETH MERRITT: The next one, visual 5 impacts, refers to section 4.16. Well, that's the 6 general section on historic properties in the Draft 7 EIS. It doesn't really point to anything specific 8 that describes any sort of commitment about visual 9 impacts. 10 So I just think that this needs to be 11 further developed in order for us to be able to 12 understand what specifically you all are proposing as 13 potential environmental commitments that would address 14 the adverse effect information. 15 So we're definitely interested in continuing 16 the discussion, but certainly we continue to believe 17 that the project will have adverse effects in the 18 arena of visual, vibration, access to USS Alabama. 19 And those are the key areas. 20 MARK BARTLETT: Okay. Appreciate it. Sorry 21 about the mixup on the slides there. We were pulling 22 those together to try and help present some of the 23 things that ALDOT has already talked about and</p>

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<p style="text-align: right;">Page 54</p> <p>1 understood from previous meetings, from previous 2 discussions with this group, and tried to clarify. 3 I guess our final presenter here today is 4 Chris. 5 CHRIS WILSON: Thank you. First I want to 6 thank you for having this meeting. This is one of the 7 requests the Advisory Council made, was to address 8 consulting parties and have them convene. Thank you 9 for doing that. 10 So just a very brief little summary of what 11 the Advisory Council is, for those of you who may not 12 be familiar with us, because we've just sort of joined 13 this discussion. The Advisory Council on Historic 14 Preservation was formed at the same time the 15 Preservation Act was created or soon thereafter and 16 after Section 4(f) was created. So I just wanted to 17 put it in context. The reason why the Advisory 18 Council on Historic Preservation exists and the reason 19 why historic preservation offices exist around the 20 country is because of the things that occurred in the 21 1950s and '60s with urban renewal and transportation 22 projects. So it's what this gentleman was referring 23 to earlier that you witnessed firsthand. So this is</p>	<p style="text-align: right;">Page 56</p> <p>1 record of any communication from ALDOT or FHWA about 2 this Mobile bridge project. So when we sent a letter, 3 I think it was in July, we requested to have the EIS, 4 to be brought into the process. And so now we're 5 involved. We missed the initiation, we missed the 6 identification process. And now we're looking at the 7 assessing of effects. So that's just a little bit of 8 background. 9 So why are we involved here? We're involved 10 because Mobile is a very significant city, one of the 11 most important historic cities in the United States. 12 And so we take this very seriously. We stand by our 13 previous comments about the visual effects and the 14 vibration analysis. Those points have been made by 15 the National Trust and the State and City landmarks 16 groups. 17 I just want to refer to our regulation 18 800.5. And they specifically call this out when 19 assessing adverse effects. So 800.5, it says that 20 there can be an adverse effect if the action 21 diminishes the integrity of the property's location, 22 design, setting, materials, workmanship, feeling or 23 association. So we're not going to go into the Park</p>
<p style="text-align: right;">Page 55</p> <p>1 why we are here. This is why the state office gets 2 federal money. This is why the Advisory Council 3 exists. 4 Our role -- we have multiple roles, but we 5 report to the President and we report to Congress. 6 Just recently we published a report on the U.S. Postal 7 Service because of their disposition of hundreds of 8 properties throughout the United States, historic 9 properties that are underutilized and being sold. 10 That was mandated by Congress. We had to within 90 11 days of our Congressional funding produce a report. 12 So we do policy level work with Congress at the 13 present but we also work on Section 106 cases like 14 this one. There's 100,000 cases a year. We get 15 involved in about 8 to 900 of them. And this is one 16 of them. 17 So just from a procedural standpoint, I 18 guess I want to address this to Lynne. We 19 requested -- well, we didn't request. We said that we 20 would be participating in '03 in this project. And 21 I've done a pretty extensive archival search, talked 22 to two predecessors that were assigned to federal 23 highways and working in this area, and we have no</p>	<p style="text-align: right;">Page 57</p> <p>1 Service language that talks about the characteristics 2 of historic properties, but I think the visual effects 3 of a bridge that is well over 500 feet tall, the 4 second tallest structure in Mobile after the large 5 high rise, is very difficult to -- it's very difficult 6 for me to pretend that there are no visual effects. 7 So what I'd like to do is address the 8 800-pound gorilla in the room. The mitigation that 9 was brought forth by one of your members and discussed 10 by the National Trust, when you talk about tree 11 canopies or when you're talking about making changes 12 to impact the visual effects, in my opinion those can 13 only occur as a product of mitigation. Mitigation 14 cannot occur until the DOT and FHWA admit that there 15 is an adverse effect on these NHLs and especially the 16 historic districts which I toured yesterday for about 17 an hour and a half. I think if you can get over that 18 hump of addressing and admitting that there is an 19 adverse effect, at least from a visual standpoint and 20 then with the vibration analysis in more detail, then 21 you can start looking at mitigation. But I don't see 22 how you can mitigate something that you don't admit is 23 creating an adverse effect. So that's our stance.</p>

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<p style="text-align: right;">Page 58</p> <p>1 I had a few other things I wanted to say, 2 but really the points have been made well by others in 3 the room. So I'm just going to complete my comments 4 now. But thanks for having me down here, and I'll be 5 back. 6 MARK BARTLETT: Appreciate it. Very good. 7 And we will follow up with a letter. Alfredo behind 8 you is kind of indicating that maybe we have 9 corresponded with you. 10 CHRIS WILSON: We're late to the game here. 11 ALFEDO ACOFF: What happened -- I was in 12 this process since 1995 -- since 1995. And we did 13 several times invite them -- you know, contact the 14 Advisory Council to participate. At the time y'all 15 did not want to participate. And we sent out 16 information -- the last time I think y'all did not 17 receive information, y'all were moving, that sort of 18 thing, people change and stuff. So we'll check our 19 records also to be sure. But I remember the Advisory 20 Council being involved. And personnel has changed and 21 things. 22 CHRIS WILSON: It's in the EIS. The EIS 23 specifically states the Council said they would</p>	<p style="text-align: right;">Page 60</p> <p>1 to make. If you get to the point where you cross the 2 bridge of admitting that there are adverse effects, I 3 would highly encourage you to bring the Park Service 4 into this discussion because of the multiple NHLs in 5 this project. I don't think it would hurt to initiate 6 a discussion with them now because there are so many 7 and Mobile is such an important district. 8 ALFEDO ACOFF: And may I ask you and the 9 National Trust, be sure we've got your addresses 10 right. 11 ELIZABETH MERRITT: We both moved. 12 CHRIS WILSON: We moved about three months 13 ago. 14 ALFEDO ACOFF: And something is wrong with 15 y'all's mail system. Just make sure we've got the 16 addresses for y'all correct. 17 MARK BARTLETT: Jennifer, do you have a 18 question? 19 JENNIFER ROSELIUS: I just have a question. 20 Chris is obviously much more experienced in this 21 process than I am. But is there a way to incorporate 22 these commitments in a programmatic agreement without 23 a finding? Is that possible?</p>
<p style="text-align: right;">Page 59</p> <p>1 participate in this case. But see, there's a give and 2 take there, a phone call, an email. 3 ALFEDO ACOFF: I did. I did talk to them. 4 That's what I'm trying to say. I recall -- 5 CHRIS WILSON: Well, I'd love if you could 6 share that. Because I talked with my colleagues that 7 I work with. They have no memory. And I went through 8 our databases to find out if there had been anything 9 since '03. 10 But anyway, I guess it doesn't matter 11 because we're here now and we're happy to be here. 12 Because this is one of the few cases we're 13 participating in this year. 14 ALFEDO ACOFF: And several agencies, that's 15 why I make sure they're involved, EPA, Advisory 16 Council, Corps of Engineers. I've been trying to make 17 sure everybody is involved. We're going to miss some. 18 But we were trying to make sure everybody was covered. 19 Because I knew how important this project was to 20 Mobile. And so that's why I was trying to make sure 21 everyone was involved. And I'm glad you're here and 22 I'm glad you are involved now. 23 CHRIS WILSON: And one last point I forgot</p>	<p style="text-align: right;">Page 61</p> <p>1 CHRIS WILSON: The whole nature of the PA is 2 that you're addressing adverse effects; am I right, 3 Mary Ann? 4 MARY ANN NABER: No. Actually I disagree 5 with you there, Chris. Because programmatic 6 agreements are prospective documents which set out a 7 process or set out agreed-upon measures. And it 8 doesn't necessarily just limit itself to measures to 9 minimize adverse effects but also to ensure that there 10 are no adverse effects. And if you look at an 11 agreement like the programmatic agreement we did for 12 the Louisville Bridges project, it incorporates a lot 13 of the kind of language about context-sensitive 14 design, about avoiding vibration impacts, not just 15 dealing with an adverse effect. 16 CHRIS WILSON: But they agreed there were 17 adverse effects in that project. 18 MARY ANN NABER: There were adverse effects. 19 But even in this case we know we have to do a 20 programmatic agreement because we have the outstanding 21 question of archaeological, potential archaeological 22 resources in areas that are not currently accessible 23 to us. And even though the summary of the impact says</p>

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<p style="text-align: right;">Page 62</p> <p>1 that there's an effect to one archaeological site or 2 four archaeological sites, we know that in the areas 3 where we're going to have direct earth moving or -- 4 like I say, any time we move dirt, we have the 5 potential to affect resources. 6 So we know that we're going to need a 7 programmatic agreement to address the process of how 8 we're going to assess those. And as part of that, we 9 would also include the commitments to the measures 10 that would minimize the harm or any potential for harm 11 to resources that we've identified. 12 MARK BARTLETT: Thank you. 13 Have the floor. Go for it. 14 MARY ANN NABER: No, no. 15 CHRIS WILSON: So let me ask her a question. 16 Can you think of any large scale projects like this, 17 significant sites and districts, where there weren't 18 adverse effects identified? I mean Louisville Bridge 19 is not the best example because -- I worked on that 20 project for two years -- there were hundreds and 21 hundreds of properties that were going to be impacted. 22 And that's to our advantage; of course, it's sort of a 23 nightmare project.</p>	<p style="text-align: right;">Page 64</p> <p>1 structure. 2 HERNDON INGE: Just outside, though. That's 3 my point. 4 MARY ANN NABER: And I'm going to also bring 5 up Savannah. 6 CHRIS WILSON: That's also not adjacent to 7 the historic district. I lived in Savannah. 8 MARY ANN NABER: Well, it's pretty darn 9 close. 10 CHRIS WILSON: It's not right next to the 11 city center where this is. It's pretty far away. 12 MARK BARTLETT: What about St. Augustine? 13 MARY ANN NABER: St. Augustine, that was a 14 no adverse effect. That was a no adverse effect, the 15 Bridge of Lions. And that was an entirely 16 reconstructed bridge, reconstructed on its -- 17 CHRIS WILSON: What's the hesitancy? I 18 don't understand the hesitancy -- because you've 19 discussed mitigation -- admitting there's an adverse 20 effect and then moving forward in the 106 process to 21 go to the fourth step that Lynne mentioned, to resolve 22 adverse effects, where you can look at minimization 23 and mitigation.</p>
<p style="text-align: right;">Page 63</p> <p>1 But can you think of a project like this 2 where a large structure is being proposed where there 3 are so many important resources where the agency 4 didn't say there was an adverse effect? I just find 5 that -- looking at this sort of objectively since I'm 6 new to this case, I just don't -- I can't get over 7 that. I can't get past it. 8 MARY ANN NABER: I will say one that comes 9 to mind -- and I think, again, we have to recognize 10 that when there is large-scale excavation of some 11 kind, that even though we have avoided direct impacts 12 to above-ground resources that have been identified or 13 archaeological sites that are previously known, that 14 we have to have some sort of agreement in place. It 15 behooves us as a federal agency to ensure that there 16 is a process in place to address the things -- 17 identification, evaluation and treatment of those 18 potential archaeological sites. 19 So even with a project -- I'm going to say 20 Cooper River Bridge in Charleston. Charleston is one 21 of the cities that Mr. Inge brought up as being 22 exemplary of our historic preservation. And yet just 23 outside Charleston there is a major high rise</p>	<p style="text-align: right;">Page 65</p> <p>1 MARK BARTLETT: I guess from our perspective 2 what I would say is that while we agree there is an 3 effect, we all agree that you will be able to see the 4 bridge, it's all a matter of you believe it's a much 5 more adverse effect than I do. We don't believe that 6 it's a true adverse effect that affects the ability to 7 remain listed on the register or become listed on the 8 register. 9 MARY ANN NABER: And I think, just to 10 interrupt you, Mark, the criteria talks about 11 diminishment of those contributing aspects. And so 12 the question is when you've already got a context that 13 has 500-foot buildings and there's all this lighting 14 and I saw the skyline last night lit up, clearly there 15 are a number of -- it's an urban -- it's a modern 21st 16 century urban environment. By putting one other tower 17 in there that's 500 feet high, and you can see it, is 18 that going to in fact diminish the quality, the visual 19 qualities of a place? 20 And that's the question. And I agree with 21 you. The purpose of our being here now in step 3 of 22 the 106 process and 800.5 is to talk about -- and I'm 23 going to use a broader definition of mitigation than</p>

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<p style="text-align: right;">Page 66</p> <p>1 you do because I'm not just talking about mitigation 2 in terms of addressing identified adverse effects, but 3 are there things that we can do to ensure and 4 incorporate into the project plans now to avoid any 5 impacts that would be adverse.</p> <p>6 CHRIS WILSON: There's another point I 7 wanted to bring up. And that is -- and correct me if 8 I'm wrong, because I know that the consultants are 9 here and they can speak for themselves. But in the 10 EIS -- which I did read and we're very happy to 11 receive it; you probably spent \$30 to mail it to us. 12 As I went through the whole document, I was looking at 13 the end, trying to figure out, okay, who are the 14 people who actually wrote the report and assessed the 15 effects? And to the best of my ability, reading the 16 index, they were principally -- they were 17 archeologists. I didn't see any architectural 18 historians or historic preservation planner --</p> <p>19 ALFEDO ACOFF: No. Pat Patterson's office.</p> <p>20 CHRIS WILSON: Oh, so we do -- I didn't see 21 anyone listed in the index that was listed as an 22 architectural historian or historic preservation 23 planner. It just had the principal and the</p>	<p style="text-align: right;">Page 68</p> <p>1 appreciative if there might be a revision or certainly 2 some possibility of providing access to Battleship 3 Parkway, or the Causeway as some of you know it, 4 simply because we are a self-supporting agency of the 5 State of Alabama, and we feel that the height of the 6 bridge and the fact that there is not going to be an 7 exit until Exit 30 might take many potential customers 8 past us. Or also, to turn around and put on another 9 hat, say that every time there's a delay in the 10 tunnels, that we probably lose business.</p> <p>11 So like I said, just to reiterate, we would 12 love to have a review of the potential of possibly 13 having an access ramp down to our level. And I thank 14 y'all for allowing us to be part of the process.</p> <p>15 DAN WAGNER: I'm the clergy assistant at 16 Christ Church. My name is Dan Wagner. I actually 17 lived in Charleston and worked for the City of 18 Charleston many years ago. And I think after the 19 Canterbury House and the Episcopal retirement 20 building, the City of Charleston said no more high 21 rise buildings.</p> <p>22 Now, our parish, our parish -- you know, I 23 look out my window. And I can see -- I can see the</p>
<p style="text-align: right;">Page 67</p> <p>1 archeologists that worked for them. So the EIS 2 doesn't have all the people who wrote the report, I 3 guess.</p> <p>4 BUDDY COVINGTON: It should have Pat 5 Patterson in there.</p> <p>6 CHRIS WILSON: And does it say archeologist?</p> <p>7 PAT PATTERSON: Master of architecture.</p> <p>8 BUDDY COVINGTON: It says architect.</p> <p>9 CHRIS WILSON: In the EIS?</p> <p>10 PAT PATTERSON: Yes.</p> <p>11 CHRIS WILSON: I'll take another look at 12 that. Because my initial thought was how can an 13 archeologist assess these historic structures.</p> <p>14 ALFEDO ACOFF: We have two other people.</p> <p>15 BILL TUNNELL: I'm Bill Tunnell. I'm the 16 executive director of USS Alabama Battleship Memorial 17 Park. And I'm very proud that the Commission and I 18 are caretakers of two of the four national historic 19 landmarks in Mobile. I'm very proud that SHPO as well 20 as the National Trust have looked after our interests 21 as far as requesting additional access. And that's my 22 point today, too.</p> <p>23 Number one is that we very much will be</p>	<p style="text-align: right;">Page 69</p> <p>1 tunnels backing up. I don't even use I-10; I use the 2 Bankhead Tunnel. I've learned that. So we know 3 there's a need.</p> <p>4 Our concern is obviously vibration because 5 we felt it from all the tunnel constructions, 6 construction of the Government Plaza and also the 7 razing of the old county jail.</p> <p>8 I love the fact that our picture is in here. 9 Thanks. But also I think that we see the need, but we 10 definitely recognize the need for vibration control as 11 much as you can do it. Those are our concerns. Also 12 what's under the bridge, that's been alluded to. And 13 also just living in the shadow of the bridge. Those 14 are our concerns. But we see and definitely recognize 15 the need for improved transportation. So it's the 16 reality of things.</p> <p>17 And I would say, having lived in Charleston 18 and watched some of the construction, that was a long, 19 long process, design/build. And the big thing for 20 that was to reunite two neighborhoods, one 21 neighborhood that was divided by two existing bridges 22 that were destroyed and demoed later. But that's 23 important, too. That's the characteristic of keeping</p>

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<p style="text-align: right;">Page 70</p> <p>1 those neighborhoods intact. I'm not still familiar 2 with all the neighborhoods here in Mobile, but I do 3 know that it's important to maintain just the 4 continuity and the characteristics as best as you can. 5 So it's a difficult thing. 6 MARK BARTLETT: Thank you so much. 7 ALFEDO ACOFF: Anybody else? 8 DAVID DAUGHENBAUGH: David Daughenbaugh, 9 City of Mobile Urban Forestry. I just wanted to 10 follow up on Devereaux's comments and also there's 11 some existing tree issues that this project may 12 impact. And when you get a little bit farther along 13 on your design, we need to identify those trees. And 14 there could be some permit requirements for the Mobile 15 Tree Commission. But also there will be some impacts 16 to existing commercial property where there's 17 landscaping that's required by the City that could be 18 impacted, specifically Austal, their new parking lot, 19 and how the pylons may impact their frontage tree 20 landscaping requirements. So that may be mitigated 21 through the landscaping plan that Devereaux had talked 22 about. 23 But also I do want to bring this up. It's</p>	<p style="text-align: right;">Page 72</p> <p>1 RICHARD OLSEN: Richard Olsen, City of 2 Mobile Urban Development, following up and touching on 3 what Devereaux said about the treatment under the 4 bridge. That's a major concern we would like to 5 address. The treatment under the bridge, lighting, 6 how that property is going to be used, the land use 7 factors all are important to the City. And we want to 8 have that considered in the design process so that 9 there will not be a dark, desolate area that will be 10 frightening to people and potentially a danger. 11 MARK BARTLETT: Okay. Good point. 12 I guess, Betsy, you had a couple of 13 questions. Do you want to kind of ask your questions 14 now? Or do you want to kind of take a little break 15 and come back and regroup and have you ask your 16 questions? 17 ELIZABETH MERRITT: Well, some of the 18 questions I've already posed that I'd be very 19 interested in getting some response to today, like 20 specifics on the Church Street East, nothing 21 contributes to the integrity. 22 And then the question I was just trying to 23 ask kind of relates to that discussion. So I could</p>
<p style="text-align: right;">Page 71</p> <p>1 not really a forestry issue. But we're involved with 2 oversized loads. And there is a corridor that is near 3 your project that could be impacted. And I just 4 wanted to bring that up. It's Baker Street and Yeend 5 Street going into the State Docks. The oversized 6 loads that are going to be coming from that corridor 7 are from Airbus. They're looking at sections that 8 will be 20 feet wide and 30 feet long and they're 9 going to need the entire road of Baker Street. And if 10 the contractors or subcontractors for the construction 11 decided to store equipment or materials near these 12 streets, it would need to be coordinated with Airbus. 13 MARK BARTLETT: Sure. Understood. That's 14 one thing that I failed to mention. We do have a 15 couple of folks here today that are going to address 16 specific issues. Dr. Cleary is going to talk about 17 vibration. He's done some vibration studies for us. 18 And J.D., I think you guys have seen him before. He's 19 the one who has developed some of the visualization 20 with his team. And then we would like to kind of 21 engage a little more in the discussion of the canopy 22 preservation and how we might be able to incorporate 23 that into the project.</p>	<p style="text-align: right;">Page 73</p> <p>1 save it and wrap it into that discussion about 2 integrity and how you evaluate adverse effects. 3 MARK BARTLETT: Okay. Very good. Well, 4 it's about 20 till. We'll take about a ten-minute 5 break to stretch your legs. Like I said, the 6 restrooms are around the corner here. There's a break 7 room. 8 (A RECESS WAS TAKEN FROM 10:38 A.M. 9 10:59 A.M.) 10 MARK BARTLETT: I guess at this point we 11 would like to kind of explore Betsy and ask a couple 12 of questions about the districts and some of the 13 information that was included. So we wanted to give a 14 little more time to that discussion, followup. 15 ELIZABETH MERRITT: We were just having a 16 premeeting. So Lynne, are you going to go ahead with 17 your -- 18 LYNNE URQUHART: I'm going to point to this 19 and talk about it a little bit. I don't know if I 20 misspoke or you misheard. I'm not sure it's really 21 relevant. But you were concerned about me saying 22 there was nothing over there that was contributing. I 23 mean we look at the -- and first of all, I need to say</p>

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<p style="text-align: right;">Page 74</p> <p>1 I am not a historic resources expert. I'm pretty good 2 on Section 106. But the actual historic stuff is not 3 my area of expertise. So Mary Ann, Pat, Bonnie, if I 4 start wandering off into the wrong terminology, y'all 5 are just going to have to rein me in and make sure I'm 6 saying it right. 7 But we looked right here at the edge of the 8 districts in proximity to where the project is. And 9 say, you know, looking out from that, what are we 10 seeing? Well, if you're right here -- and I was 11 talking primarily up along Water Street up there. But 12 I mean if you're right here at the Fort Conde area and 13 looking over here, you're seeing ramps, interstate, 14 industrial stuff. All of this stuff is much more 15 modern than the districts that we are seeking to 16 protect and to preserve. So looking at that, none of 17 those characteristics, none of those buildings are 18 contributing to that district. 19 MARY ANN NABER: To the visual character of 20 that district. 21 LYNNE URQUHART: To the visual character of 22 that district. So looking at adding that bridge in 23 there, yes, it's a visual impact. We have never</p>	<p style="text-align: right;">Page 76</p> <p>1 passing was a suggestion that in order to constitute 2 an adverse effect, that an impact has to essentially 3 sort of threaten the integrity of the district from 4 the National Register. And I just wanted to try to 5 see if we had a consensus that that's not accurate. 6 Is that fair, Mary Ann? 7 MARY ANN NABER: Yes, yes. We all 8 understand and Mark understands that the criteria of 9 adverse effect is based on diminishing any of those 10 contributing characteristics, the integrity of any of 11 those contributing characteristics, and not just 12 threatening its National Register significance. And 13 that's how the assessment was done. 14 ELIZABETH MERRITT: Well, one of the things 15 that we were just talking about over there is that 16 visual impacts are especially difficult to evaluate 17 for purposes like this because they can be -- it's 18 difficult to quantify them the way we do with 19 vibration or noise or a number of different types of 20 impacts because beauty is in the eye of the beholder. 21 And theoretically you could have somebody who looks 22 out and just thinks this structure is the most 23 beautiful thing in the world. But you would have</p>
<p style="text-align: right;">Page 75</p> <p>1 denied that there will be visual impacts to the 2 historic resources from the bridge. But what we have 3 felt is when you put that bridge in there with those 4 other elements of modern -- you know, from the modern 5 times, it does not diminish the integrity of that 6 district. And I'm talking about right here at Fort 7 Conde. But the same thing applies up at Water Street 8 looking across the road at the convention center, the 9 maritime museum, the stuff that's across the river, 10 Austal, the railroad tracks, things like that. 11 So that was what I was attempting to say and 12 talk about. And if you need me to expand on that, I 13 might be outside of my area of able to talk, but I'll 14 give it a shot or make somebody else help me. 15 DAN WAGNER: I would just say that our 16 church is the church on Church Street. We're the 17 church on Conception, and we're right there. And 18 we're old. I'm saying we're not exactly modern. 19 LYNNE URQUHART: You are within the 20 district. 21 MARK BARTLETT: Do you have a question? 22 ELIZABETH MERRITT: Just one other thing I 23 wanted to just get clarification on which was said in</p>	<p style="text-align: right;">Page 77</p> <p>1 other residents and business owners and people who -- 2 let's say visitors who come to these places who would 3 think it's God awful, would think it's a hideous 4 intrusion visually. And it's very difficult to 5 quantify that. 6 But in the extreme, if you have a 7 predominant view that the visual intrusion is a highly 8 undesirable, negative thing, in the long run that 9 translates into disinvestment in the district. And it 10 translates into people not wanting to live there, 11 people not wanting to work there. 12 And at the time back in the 1980s when the 13 elevated freeway was proposed to come all the way down 14 Water Street in front of the NHL City Hall at the 15 time, the Advisory Council made some really 16 interesting comments about that and about the impact, 17 an adverse visual impact, an indirect impact like that 18 and how it can translate into an economic impact 19 through disinvestment if it's considered very 20 undesirable. 21 Again, it's difficult to measure. But it is 22 a reality and it's something that the Advisory Council 23 folks spent a lot of attention on in connection with</p>

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<p style="text-align: right;">Page 78</p> <p>1 the previous visual impacts of the other proposal. 2 So part of the reason why I wanted to go 3 through this exercise is I heard language from Lynne 4 which suggested that she was proposing that part of 5 the Church Street East Historic District be removed or 6 had lost its integrity and that the boundaries needed 7 to be changed or something. I was focusing on that 8 comment of "nothing contributes to the integrity." So 9 I feel like I've gotten -- I've heard you back away 10 from that. 11 LYNNE URQUHART: Yes. I was not -- I don't 12 know enough about how we define the boundaries of 13 districts and stuff like that that I could actually 14 make an intelligent statement about changing the 15 boundaries of the district. 16 MARY ANN NABER: In my understanding of what 17 Lynne said, she was referring to the visual -- the 18 visual character of the district, that there was 19 nothing beyond Water Street, east of Water Street, 20 that contributed to the visual character of the 21 district. Therefore, introducing another element 22 within that same context would not diminish the visual 23 integrity of the district, the setting in that area.</p>	<p style="text-align: right;">Page 80</p> <p>1 environment using some context-sensitive solutions and 2 design firms that could actually incorporate the 3 bridge into the environment. One of the aspects that 4 I keep thinking back about, I keep thinking about all 5 the visual impacts of the bridge proper. But also 6 what about the items where we would be enhancing some 7 of the districts' quality of life as we develop an 8 additional route for the traffic to go across? 9 Hazardous materials that are bypassing the 10 tunnel because of the restrictions are currently using 11 Water Street and other locations in the downtown. 12 These would be now allowed to be used on the bridge, 13 so therefore they would be out of the area where they 14 were traversing these historic districts, relieving 15 some of that traffic ingestion in the downtown area. 16 HERNDON INGE: Another thing -- I don't know 17 if I'm allowed to interrupt. But another is the 18 low-build option. And Bert Eichold has come up with, 19 sort of developed another alternative in addition to 20 route 7, which was not -- or alternative 7 which was 21 not chosen as one of the three chosen routes. But an 22 idea is you've got I-10, you've got the Wallace 23 Tunnels. They're open 24 hours a day. Now, what</p>
<p style="text-align: right;">Page 79</p> <p>1 LYNNE URQUHART: And that's what I was 2 trying to say. And if I mangled it with the wrong 3 language, I apologize. 4 ELIZABETH MERRITT: So I guess in our view, 5 the visual impact should not solely be measured by 6 standing with your back to the historic district and 7 looking out. It should be evaluated based on what do 8 you see when you're in the district and you're seeing 9 streetscape of historic properties and the God awful 10 monstrosity above, if that's what people think it is. 11 And then how does that translate in the long run into 12 economic decisions that people make about where they 13 want to live and where they want to work? And it may 14 in fact result in disinvestment that has an indirect 15 cumulative impact that needs to be taken a look at. 16 MARK BARTLETT: I think one of the areas 17 that I feel like is going to be very productive from 18 some of the commitments the department has made as 19 well as some of the comments that were made in 20 response to a letter that was sent out is the 21 discussion about cooperating to develop the aesthetics 22 of the bridge and working as a group to talk about how 23 we could actually make the bridge fit into the</p>	<p style="text-align: right;">Page 81</p> <p>1 we're talking about is a measure to alleviate the 2 traffic congestion at relatively predictable periods. 3 Okay? 4 Bert Eichold, the county health officer, 5 checked with the State Docks or whoever, maritime 6 industry, and he said that there are usually two to 7 four ships coming under -- coming into Mobile Harbor a 8 day, two to four, sometimes four to six. 9 Well, okay. My wife's beach house is at 10 Fort Morgan and we look at Fairwell Buoy, and there 11 are ships that are waiting out at Fairwell Buoy, 12 waiting to get a bar pilot to come into town -- to 13 come into the harbor, and no big deal. They're just 14 sitting out there waiting on an opportune period when 15 the bar pilot brings it into town. 16 So an alternative is to have a low-build 17 bascule bridge possibly -- and I'll talk about the 18 length of the bridge -- but a low-build option and say 19 the I-10 -- this I-10 route will be closed from 20 midnight to 4, and the two or four ships can come 21 in -- can come between the bascule bridge, between two 22 and four, at midnight. And you've still got the I-10 23 and Wallace Tunnels functioning. And so the other 20</p>

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<p>1 hours a day the bascule bridge can be closed, the 2 ships can wait out at Fairwell Buoy, and the traffic 3 during the peak times can be using the low-build 4 option. I never had thought about that. 5 In the Environmental Impact Statement it 6 says, I think, the longest bascule bridge is 600 feet 7 or 300 feet or something like that. Well, if you look 8 at the Intracoastal Waterway under the Dauphin Island 9 Bridge or if you look at under the Dog River Bridge, 10 the passageway is wide enough for whatever the 11 traffic -- whatever the ship traffic is. And so you 12 don't have to have a 1,200-foot bascule bridge for the 13 two or four ships a day; you can narrow -- you can 14 narrow the gap between the pylons of the low-build 15 bascule bridge and just, you know, have it wide 16 enough, have it within the 300-foot or 600-foot, 17 whatever it is, width and a low-build option. 18 You know, I was an English major. I'm not a 19 traffic engineer. But I'm saying that there are 20 low-build options. And you don't have, you know, 21 535-foot pylons right over downtown Mobile. You're 22 alleviating the traffic during predictable periods 23 with a low-build option.</p>	<p>1 HERNDON INGE: That's what I saw. That's 2 what the EIS -- that's right. And nobody has ever 3 built a 1,200-foot span of a moveable bridge. And 4 that's fair to say. 5 BUDDY COVINGTON: Not to my knowledge. We 6 couldn't figure out how you'd pick it up. And there's 7 maintenance and other things that go in with moveable 8 bridges that we considered but not to a great detail 9 yet at this stage. 10 MARK BARTLETT: That's a valid point, valid 11 question. 12 ELIZABETH MERRITT: What about other bridge 13 types or bridge designs that would be much shorter 14 because they wouldn't have the massive piers of the 15 cable-stayed bridge but that would be -- have similar 16 clearance? Or the idea of doing a design competition? 17 What about that to try to -- 18 MARK BARTLETT: You're talking about some 19 type of a truss-type structure that may not have the 20 towers of the cable-stayed? 21 ELIZABETH MERRITT: Right. That would have 22 a lower visual profile. There are different types 23 that you could consider. But many types are less</p>
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<p>1 And those are the kind of things that we 2 should be able to sit down and say: Okay, how can we 3 solve the traffic needs of the future without pissing 4 off everybody? 5 MARK BARTLETT: All right. Buddy, do you 6 want to speak a little bit to the moveable bridge 7 options that were evaluated? David? Who would be the 8 best person to talk about that? 9 BUDDY COVINGTON: Yeah, me or David. 10 MARK BARTLETT: Somebody from the 11 department? Vince? 12 VINCE CALAMETTI: I'll let Buddy. 13 BUDDY COVINGTON: We looked at different 14 types of bridge alternatives, removable span bridges 15 as design alternatives. And where we looked at those 16 were at the location of the alternatives, the 17 corridors that we were evaluating, A, B, B Prime and 18 C, and predominantly looked at about a 1,200-foot 19 crossing. So although we evaluated other types of 20 bridge designs, we didn't look at them at other 21 locations or shortening the span length, so to speak. 22 We compared them to the same span length that we have 23 on the cable-stayed.</p>	<p>1 visually intrusive than the cable-stayed and could 2 potentially meet the needs of the project. 3 MARK BARTLETT: I think that's something 4 that we could look at. I don't know what the 5 engineering limitations of a truss-type system like 6 that might be similar to, a bascule-type structure. 7 But we can look and evaluate what type of bridge that 8 may be able to have a lower horizon. 9 BUDDY COVINGTON: You kind of made the 10 argument that beauty is in the eye of the beholder. 11 But I think early on we had had some input from the 12 community about what type of bridge would you like to 13 see, and a truss bridge was something that they really 14 weren't interested in at one time. We've had comments 15 to that effect of don't make it look like this bridge 16 in New Orleans or a heavy-type structure. And so we 17 kind of looked at that point at what the span length 18 was going to be and then what was an efficient style 19 of bridge, if you will. And I'm not a bridge 20 engineer. I'm an environmental person, so I'm going 21 by what the document says. And the cable-stayed 22 seemed to work most efficiently for the type of span 23 that was trying to be crossed at that roughly 11 or</p>

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<p style="text-align: right;">Page 86</p> <p>1 1,200-foot range. 2 Is it possible to build a truss that big? 3 It would be an awfully big truss, I would imagine. 4 Again, I'm not a bridge engineer. But it may be 5 possible. So we can look at it. 6 There are a lot of things that might could 7 be built. But should we build them is what we've got 8 to evaluate. 9 MARY ANN NABER: One thing that I've learned 10 in some of the bridge projects that I've worked on -- 11 and again, I'm an English major, too, undergraduate. 12 And so I'm looking at this as more of a layperson, not 13 an engineer. But it's my understanding, and it's been 14 borne out by the experience that I've had, that 15 cable-stayed bridges give you the lightest profile in 16 terms of the deck. Whereas a truss can be very 17 massive in terms of, yes, maybe the superstructure, 18 the towers may be lower, but you've got a lot of other 19 metal. And you have a very massive-looking structure 20 that is even more dominant in the viewshed. When you 21 look at the long view of the bridge, you know, you see 22 a vertical tower and you see a horizontal deck, and 23 the cables are almost invisible from certain</p>	<p style="text-align: right;">Page 88</p> <p>1 215. 2 Do you want to talk a little bit about the 3 consideration for the cruise ships or the other 4 shipping concerns? 5 VINCE CALAMETTI: And that's what it was. 6 It was to facilitate the cruise industry and then the 7 other maritime interests after discussions with the 8 maritime community and the cruise industry. 9 ELIZABETH MERRITT: I thought the cruise 10 industry wasn't using the terminal, wasn't using the 11 cruise terminal. 12 MARK BARTLETT: Currently. And one of the 13 issues was that we did not want to introduce anything 14 in the navigation channel that would prohibit Mobile 15 from competing on an economic level in future years. 16 So I think that was part of the discussion about the 17 elevation change. The consensus among the navigation 18 interests was that 190 would limit Mobile from 19 competing economically upstream of that bridge, and 20 the cruise terminal was upstream of most of the 21 options. And so we introduced the discussion or 22 started talking about how we could introduce an 23 additional feature over the river that would not limit</p>
<p style="text-align: right;">Page 87</p> <p>1 directions and vantage points. So it's a much less 2 obtrusive structure in terms of just the massive 3 materials that are visible. 4 MARK BARTLETT: I think you know that we've 5 kind of got some comments in from the group here about 6 a programmatic agreement. We started talking about 7 how we might be able to minimize some of the impacts. 8 That lighting issue might be one of those that would 9 be able to minimize that view. We could talk about 10 how we could introduce discussion in that programmatic 11 about when the bridge would be allowed to be lit. 12 We're going to have some safety lights. I don't think 13 anybody wants to have an unsafe condition out there. 14 I know the top of the tower will have some type of a 15 beacon on it. We'll have some lights on the bridge 16 for the shipping channel. As far as the up lighting 17 on the strands and those types of things, I think that 18 may be something that we could address as part of the 19 discussion. 20 ELIZABETH MERRITT: Could I follow up with 21 my question about why the height was increased, height 22 of the bridge was increased from 185 to 215? 23 MARK BARTLETT: It was 190 and it went to</p>	<p style="text-align: right;">Page 89</p> <p>1 the navigation. And 215 seems to be the one that 2 allowed the cruise terminal to pursue a larger class 3 of cruise ship and it seemed to be more in the 4 domestic market in the southeast. 5 HERNDON INGE: But if you listen to the 6 political dialogue and debate, they say the cruise 7 industry -- that the cruise industry returning to 8 Mobile is a dead issue, it's a dead issue; that ships 9 are getting bigger and they're not going to use 10 Mobile. That's the discussion about the cruise 11 terminal. You built it without a long-term 12 commitment. Now we've got the debt and it's empty. 13 So you could say, okay, I'll build a bridge with a 14 250-foot vertical clearance because we might get the 15 cruise industry. 16 The cruise industry in Mobile is dead. And 17 so if you have two to four ships a day, then let's 18 talk about the two to four ships. Let's not talk 19 about what might happen because we might need a 20 300-foot vertical clearance. And then the towers will 21 be 700 feet tall. You know, let's accommodate the two 22 or three ships that are actually coming into Mobile. 23 MARK BARTLETT: But I guess the thing is</p>

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<p>1 we're building a structure that's going to be over the 2 navigational channel for a very long time. So would 3 it be appropriate to limit Mobile's economic abilities 4 by building -- 5 HERNDON INGE: But you could use that 6 argument forever. We might get a 300-foot -- we might 7 get a cruise ship that needs a 300-foot vertical 8 clearance, so let's build a bridge -- let's build a 9 bridge even higher. I mean you could use that 10 argument ad infinitum. 11 MARK BARTLETT: I'll leave the discussion up 12 to whether or not a cruise terminal is going to be 13 utilized between you and the city. You can have that 14 discussion outside of the context of this. I guess I 15 really can't speak to whether or not a cruise ship 16 will be coming back to Mobile. But I guess from our 17 perspective, we do not want to do anything that would 18 limit Mobile's economic potential. 19 HERNDON INGE: I just say that the visual 20 impact to the historic districts will be worse if it's 21 a higher bridge. So let's plan on what we know, which 22 is two to four ships a day. Let's plan on what we 23 know. And let's consider the visual impact with a</p>	<p>1 MARK BARTLETT: Yeah. I was going to kind 2 of wrap up, see if Betsy had any more questions. 3 ELIZABETH MERRITT: I had a followup 4 conversation -- some of these will just need to be 5 addressed later about evaluating the historic 6 potential, historic significance of some of the 7 businesses, proposed relocation. That's a followup 8 issue. Vibration we're going to talk about. I think 9 that's -- I'm ready to go to vibration. 10 DEVEREAUX BEMIS: May I? Following up with 11 Betsy, she just mentioned that there were, I think, 12 13 businesses -- 13 ELIZABETH MERRITT: 16. 14 DEVEREAUX BEMIS: -- 16 in B Prime. And 15 they were listed by name. Could we get the addresses 16 of those? If they were in there, I missed them. 17 BUDDY COVINGTON: 16 what? 18 DEVEREAUX BEMIS: The businesses that were 19 going to be relocated, they were listed by name. But 20 if we could get the locations of where they are now -- 21 BUDDY COVINGTON: Uh-huh (positive 22 response). 23 DEVEREAUX BEMIS: -- that would be helpful.</p>
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<p>1 low-build option or just another option or a 2 realignment of the corridor. 3 ELIZABETH MERRITT: Can I ask a followup 4 question? So as a result of the increase in height, 5 how many additional feet then get extended onto the 6 end of the project in order to retain the 4 percent 7 grade? And what determines the 4 percent grade? Is 8 there some -- what's the source of that? Is there 9 some requirement, federal requirement, that it can't 10 be more than 4? 11 MARK BARTLETT: Interstate standards limit 12 the grade that is recommended for interstate ramps. 13 ELIZABETH MERRITT: To 4? So how many feet 14 then get added onto the project by retaining the 4? 15 MARK BARTLETT: It's 100 foot for every four 16 foot you raise the grade. Somebody help me with the 17 math. 18 BUDDY COVINGTON: 15 feet. 19 MARK BARTLETT: If we increase the grade, it 20 would be about 625 feet of additional length on each 21 end to get the additional elevation. 22 JENNIFER ROSELIUS: How about the vibration 23 studies?</p>	<p>1 Thanks. 2 MARK BARTLETT: That's no problem. 3 KERI COUMANIS: I'm Keri Coumanis with the 4 City of Mobile. It's a little unclear to understand 5 how generally with the extension of a rise and the 6 outing of the lanes, how that's going to affect 7 underneath the interstates, particularly with Canal 8 Street, the on and off ramps becoming a major entry 9 point to downtown Mobile. So I just want to piggyback 10 on what Devereaux said earlier that we are very 11 concerned about mitigating the affects of the ramps 12 under there, particularly at Virginia Street and Canal 13 Street. And with the addition of more structure -- I 14 mean it's already desolate. We don't want to make it 15 worse. 16 MARK BARTLETT: Okay. Any other comments or 17 questions? 18 (NO RESPONSE.) 19 MARK BARTLETT: Well, the next area we'd 20 like to talk a little bit about is the vibration 21 issue. And we have a professor from the University of 22 South Alabama, Dr. Cleary, that's going to give us an 23 update on the vibration study that ALDOT has been</p>

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<p style="text-align: right;">Page 94</p> <p>1 working on. 2 JOHN CLEARY: Okay. My name is John Cleary, 3 and I'm an assistant professor at the University of 4 South Alabama in civil engineering. My specialty is 5 structural engineering, structural dynamics, 6 vibrations and also concrete properties. 7 I was excited to hear so many people mention 8 the vibrations because I get really excited to talk in 9 general, particularly about what I do. I just really 10 like to talk about what I do. So I'm really happy to 11 be here today and be able to give you an overview of 12 the project that we completed. I think the 13 preliminary report was included in the Draft EIS. 14 We've also done a final report which did a little bit 15 more of analysis on the vibrations that we measure. 16 Let me just very briefly, if you look at 17 this title of our study, it's a really long title. 18 And it was really two projects that were combined into 19 one. Dr. Eric Steward, who's a geotechnical engineer, 20 was looking at a phenomenon that's called pile setup, 21 and that's when you drive a pile, over time you get 22 increases in capacity. So he was looking at that. 23 And I was looking at piledriving vibrations</p>	<p style="text-align: right;">Page 96</p> <p>1 We drove our piles right in this region 2 right here. And part of the reason we put it there is 3 because a contractor could come in off the river there 4 and also we felt like it represented where the bridge 5 pilings would be when they actually build the 6 structure, at least close to that location. So you 7 can kind of see that in this picture. 8 We measured vibrations coming out from those 9 pile locations at these approximate distances, 50, 100 10 and 150 feet from the project site. 11 I also want to mention, too, I do realize 12 that not everyone in here is an engineer. I'm going 13 to try not to use any language that you wouldn't 14 understand, any too technical language or kind of 15 industry speak. If I say something that you don't 16 understand, please just let me know and I'll make sure 17 I use terminology that normal people understand, not 18 just me. 19 There are two types of piles we utilize in 20 this project. The first is what we call a 21 displacement pile. So a displacement pile is 22 something like this big concrete pile. This is the 23 pile we actually use. It's 36-inch square. We</p>
<p style="text-align: right;">Page 95</p> <p>1 particularly for this project. And because they were 2 so interrelated, we combined it into a single project 3 through the ALDOT research office. That's kind of why 4 it's such a long title. Mine really is the addendum 5 that's added to it. And then Dr. Steward also worked 6 with me a lot at looking at these soil properties in 7 the area. 8 We'll get to the next slide here. So the 9 project location, we all know we're in south Alabama 10 hopefully. The actual location that we are driving 11 piles at is just south of the cruise terminal here in 12 the Bender Shipbuilding yard. I think that's what it 13 was formally referred to as. And if I can get it a 14 little bit closer here, too, there's a Google picture 15 of it and then a picture one of my grad students drew. 16 Right here is where the cruise terminal is, 17 just outside that picture. So I rotated the 18 orientation here so north is now to the left. We were 19 just south of the cruise terminal there. The Mobile 20 River is here. This is the Bender Shipbuilding 21 building, a steel building, another steel building. 22 This is Southern Fish &amp; Oyster, which is located real 23 close to the project site.</p>	<p style="text-align: right;">Page 97</p> <p>1 usually call this a displacement pile. And as you 2 drive it into the ground, you can move a lot of soil 3 out of the way. Now, when you drive a displacement 4 pile, you're going to create typically much larger 5 vibrations than the other type of pile utilized, which 6 is called an HP pile or a nondisplacement pile. And 7 it kind of looks like a beam almost. It looks kind of 8 like what we call a W section. The size of these, you 9 can see here they're kind of funny numbered. HP means 10 H pile. The first number, the 14 or the 12, is the 11 approximate depth and the approximate flange width. 12 They're approximately square. And then the next 13 number is actually the weight per foot. So that 14 dictates somewhat the thickness of all these materials 15 here. 16 So again, this is called a nondisplacement 17 pile because you can see the relative amount of soil 18 that you have to displace compared to this one when 19 you're driving it in. So we wanted to do both types 20 of piles to see what type of vibrations we would get 21 from those. 22 I want to talk a little bit, too, about 23 vibrations in general and how we measure them.</p>

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<p style="text-align: right;">Page 98</p> <p>1 Somebody had mentioned inches per second for the soil 2 velocity. That is a measure we typically use. In 3 general, when you drive a pile, if you look at this 4 picture, this is what we call the toe of the pile. 5 You're going to get a lot of vibrations if you're 6 displacing soil coming from the toe of the pile. 7 There's also frictional forces between the soil and 8 the pile. As you're trying to move that pile, you get 9 vibrations coming out from that as well. So as those 10 vibrations propagate outward, they're eventually going 11 to reach the surface, which is where we measure the 12 vibration. 13       And to measure vibration, there's really 14 three things you can use. You can either use 15 displacement -- which generally, particularly in 16 soils, is very difficult to actually measure. You can 17 use the velocity of the soil as it moves. And you can 18 use the acceleration of the soil. 19       So in earthquake design we always use ground 20 acceleration. That's the commonly used parameter to 21 measure vibrations. 22       In construction vibrations we're typically 23 going to use the velocity of the soil. And the reason</p>	<p style="text-align: right;">Page 100</p> <p>1 here. And I've got a plot that shows this better. 2 But you can see the relative amount of vibrations that 3 we saw at different distances from the pile. We 4 actually had four data collectors. When we drove the 5 36-inch concrete pile, we put one at approximately 69 6 feet at the border of the property there with Southern 7 Fish to help ensure that we weren't causing damage to 8 the building while we were doing the study. 9       And here are the results from the HP piles, 10 which were generally much lower. And I think if we 11 look at the plot, we'll see it a lot more clear there. 12       So there's just a plot of those vibrations 13 as we measured them. You can see that what we call 14 the attenuation of vibrations or reduction with 15 distance from the pile is fairly rapid for the 36-inch 16 concrete pile. But when we're within 50 to 70 feet, 17 we had fairly large vibrations. But as we got away, 18 they attenuated very quickly. 19       The nondisplacement steel H piles we can see 20 generally had much lower vibrations, although at lower 21 vibrations you typically have lower attenuation rates, 22 meaning that the vibrations don't attenuate as quickly 23 as you would see with those larger vibrations there.</p>
<p style="text-align: right;">Page 99</p> <p>1 for that really is the early studies in the late '70s 2 and early '80s done by the U.S. Bureau of Mines on 3 vibrations from blasting found very good correlations 4 between damage potential and the velocity. So that's 5 the primary reason we're going to use velocity instead 6 of acceleration and it's what most of the research has 7 done. 8       How we refer to this velocity is we call it 9 peak particle velocity. So that little particle of 10 soil can be moving in one of three directions. It can 11 be moving by the pile. Here's the soil, it could be 12 moving like this, kind of front to back, left to 13 right, or up and down. We really just look at 14 whatever the maximum of all those are. We do know 15 which one -- which way it was moving, but we really 16 want to know what the maximum is. That's why we call 17 it the peak particle velocity. The measurement we use 18 in the United States is inches per second on velocity. 19       To measure these actually we use what's 20 called a geofoam. The geofoam is a standard 21 instrumentation that measures velocities. 22       Go down one. So the results of our study, 23 we drove the piles, again we said in this region right</p>	<p style="text-align: right;">Page 101</p> <p>1       The next slide I'll put up -- I'll put it up 2 now and then talk about it. What you see here are a 3 couple of red lines and a blue line or black line 4 there. The red lines are the recommendations that we 5 are making from this study on thresholds for damage 6 that we should limit to ensure that we don't have 7 damage to structures. So the first one we set is at 8 .5 inches per second. That would be for modern, 9 well-built structures. The next one that we set was 10 at .1 inches per second. And this is for what I would 11 call sensitive structures. 12       I don't really like to use the term 13 "historic structure," because just because it's 14 historic doesn't mean it's more susceptible to 15 vibration damage. And just because it's modern 16 doesn't mean it's less susceptible. Really the 17 condition of the building is important there. So 18 generally historic buildings are more of a concern 19 because they are older and they were generally built 20 with what we call more brittle materials, brick and 21 concrete as opposed to steel. So we do look at those 22 more. But just because it's a historic building 23 doesn't necessarily mean it's susceptible to vibration</p>

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<p>1 damage.</p> <p>2 The black line I put on there just as kind</p> <p>3 of an indicator. A lot research shows that at about</p> <p>4 .2 inches per second vibrations are very annoying. In</p> <p>5 fact, you can detect them at very low levels. So</p> <p>6 oftentimes if you're in a modern building, you might</p> <p>7 have a vibration level maybe around .2 inches per</p> <p>8 second and people are very annoyed by that. That's</p> <p>9 very bothersome to them. But you're not actually</p> <p>10 going to cause structural damage to the structure</p> <p>11 necessarily. You have a very low potential for</p> <p>12 structural damage in a modern well-built structure</p> <p>13 right there.</p> <p>14 And then kind of concluding with that is the</p> <p>15 vibration levels that we are recommending to prevent</p> <p>16 damage would be .5 inches per second for the modern</p> <p>17 structure and 0.1 inches per second for potentially</p> <p>18 sensitive structures.</p> <p>19 The survey distance, what we refer to as the</p> <p>20 survey distance, is really how far from the project</p> <p>21 site do you need to measure those vibrations or any</p> <p>22 structure within that what we call survey distance do</p> <p>23 we need to very closely monitor to make sure we don't</p>	<p>1 There are some people that say for historic structures</p> <p>2 you should set it at .5 inches per second and that's</p> <p>3 sufficient. At the other end, California says for</p> <p>4 historic monuments, I believe, for what are called</p> <p>5 continuous vibrations, to set it at 0.08 inches per</p> <p>6 second. The Federal Transit Administration for</p> <p>7 continuous vibrations -- I'll say in a second what I</p> <p>8 mean by that -- is set at 0.12 inches per second.</p> <p>9 There's an FTA document that sets it there.</p> <p>10 And there is a distinction between</p> <p>11 continuous and noncontinuous or intermediate</p> <p>12 vibrations, what we call transient vibrations.</p> <p>13 Traffic, for example, creates an almost continuous</p> <p>14 vibration pattern. And that has a much higher</p> <p>15 potential of causing damage if you have continuous</p> <p>16 vibrations than some sort of vibrations that have kind</p> <p>17 of pauses in between.</p> <p>18 So when you drive a pile, for example, you</p> <p>19 get not quite continuous vibrations, but you do</p> <p>20 approach continuous vibrations because the way the</p> <p>21 pile hammer works is it's hammering over and over and</p> <p>22 over again until they get a pile driven and then it</p> <p>23 stops. So it's not quite a continuous vibration</p>
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<p>1 reach those vibration levels.</p> <p>2 For the modern structures, we set that at</p> <p>3 150 feet. Results showed that by the time you got to</p> <p>4 150 feet, you're well below 0.5 inches per second.</p> <p>5 And for potentially sensitive structures, we set that</p> <p>6 at 250 feet. We felt like that that was a good</p> <p>7 location to be at, that we would have very little</p> <p>8 potential for vibration damage.</p> <p>9 So that was a very quick overview. But I</p> <p>10 can open it up for questions. I have a couple of</p> <p>11 exercises at the end as well.</p> <p>12 Yes?</p> <p>13 CHRIS WILSON: Did you compare your study</p> <p>14 with other similar studies around the country that</p> <p>15 dealt with historic structures relating to bridge and</p> <p>16 highway structures and have you done these kind of</p> <p>17 analyses before as relates to historic structures?</p> <p>18 JOHN CLEARY: So if you look at the -- we</p> <p>19 compared lots of literature. There's a lot of</p> <p>20 literature on vibrations, some relate to historic</p> <p>21 structures, some doesn't. What you find if you really</p> <p>22 get into the literature is vibrations limits, the</p> <p>23 recommendations, are kind of varied very widely.</p>	<p>1 pattern, but it's similar to a continuous vibration</p> <p>2 pattern.</p> <p>3 If you're doing something like blasting,</p> <p>4 which a lot of the early research looked at, blasting</p> <p>5 is -- you know, they're going to blow a charge maybe</p> <p>6 once a day and it's one charge and then nothing until</p> <p>7 they clean everything up. So a lot of the early</p> <p>8 studies, particularly with historic structures, were</p> <p>9 looking at blasting where you had this</p> <p>10 noncontinuous -- so they set actually a lot higher</p> <p>11 levels than for the continuous.</p> <p>12 So I tried to err on the side of caution</p> <p>13 when I looked at this. And it really is very</p> <p>14 dependent on the building structure itself. But we</p> <p>15 felt like the .1 inches per second was very</p> <p>16 conservative for that.</p> <p>17 CHRIS WILSON: One last question.</p> <p>18 JOHN CLEARY: Yes?</p> <p>19 CHRIS WILSON: Have you put provisions in</p> <p>20 place with the DOT where if some vibration,</p> <p>21 degradation, whatever you call it, actually occurs,</p> <p>22 how does that affect the project? In other words,</p> <p>23 what will you do to rectify it? And what if you're</p>

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<p style="text-align: right;">Page 106</p> <p>1 building the pylon, it's going in that place, and the 2 kind of methodology you're using to build it creates 3 some destruction? What do you do? What provisions do 4 you put in place to rectify it, to change the 5 construction techniques? Enlighten us. 6 JOHN CLEARY: So for this particular 7 project -- I hope it's okay to elaborate -- we were 8 really just looking at the vibrations from doing this. 9 I can talk in general about what other places have 10 done and other general construction techniques. Is 11 that okay with you? 12 VINCE CALAMETTI: Sure. 13 JOHN CLEARY: So generally what other places 14 have done when they set up vibration monitoring is 15 they'll look at either -- they'll do one of two 16 things: look at a historic structure and set a 17 vibration monitor near that structure or they'll just 18 go at some distance from the project site, say that 19 250 feet if there's nothing within the 250 feet. And 20 so what they'll do is they'll monitor the vibration 21 levels that are occurring. The data collectors, the 22 monitor data collectors, you don't have to have 23 somebody sitting at it. You can have a cellular</p>	<p style="text-align: right;">Page 108</p> <p>1 standard technique that they use or their contractors 2 use is to first what we call either prebore or prejet 3 a hole. So basically, for lack of a better term, you 4 kind of dig a hole some amount of distance, you put 5 the pile in there, you start driving from there. 6 You're still able to get very good capacity out of it 7 and the capacity that you need, but what you do is you 8 reduce the vibrations because the deeper you go, the 9 smaller the vibrations become. And so preboring and 10 prejetting is one technique you can use to reduce 11 vibrations. 12 Another technique you can use is what's 13 called putting a pile cushion on top of the pile. So 14 particularly with concrete piles, you've got the pile. 15 There's a hammer sitting on top of it. Well, between 16 the hammer and the pile they'll put cushions of, I 17 don't know, two feet of plywood. And so what that 18 does is when the impact of the hammer comes down, it 19 basically acts as a cushion. Basically you get an 20 impulse load, you get a very quick load. What it 21 does, it reduces the load slightly but spreads it out, 22 and that's going to reduce your vibration. So that's 23 a technique that can be used.</p>
<p style="text-align: right;">Page 107</p> <p>1 connection or some other type of WiFi connection and 2 it can get immediately sent to your computer. And 3 then what it will do is it will send out an alert if 4 you get a vibration level that's higher than some 5 specific threshold. Now, you might not set that at .1 6 inches per second. You might set it at .15 inches per 7 second. I'm sorry. I should be going the other way. 8 You set it at like .08 inches per second. And if you 9 start to hit those levels that are going to cause a 10 concern to you, then you might contact -- it will send 11 out an alert, it will send it to whoever you want, to 12 email addresses, text messages. And then you'll alert 13 the contractor that they may need to stop work and 14 come up with a different plan for their construction 15 technique to help reduce those vibrations. 16 Now, as far as construction techniques are 17 concerned, there's lots of ways you can put in a 18 foundation system. There's lots of different 19 foundation systems. A driven pile foundation is very 20 common. These are used very often, particularly for 21 bridge structures. 22 These particular piles that we put in, they 23 used standard techniques that ALDOT uses. The</p>	<p style="text-align: right;">Page 109</p> <p>1 There's also different types of pile -- or 2 foundation systems that you can use. There's like a 3 large caisson that you see often used. The way they 4 do that is basically drill a hole and then you fill 5 that hole with concrete. So instead of driving a 6 pile, you'd be using a different technique. And so 7 some of those options are sometimes used when 8 vibration is a concern and for other reasons, other 9 reasons you may use those. 10 But those are some other techniques that you 11 can use to change your pile system or your foundation 12 system to actually be able to reduce your vibrations. 13 And so if a concern came up where they started to see 14 vibrations that were higher than they expected or 15 higher than the threshold levels, you could 16 re-evaluate the construction techniques you're using 17 or maybe even, if it's early enough in the project, 18 the foundation system that you're using. 19 HERNDON INGE: You know, if we're wed to 20 corridor B Prime, then we have to worry about all 21 this. If they're not wed or married to B Prime, we 22 won't necessarily have to worry about vibration damage 23 because we'll be further away, not 250 feet or 251</p>

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<p style="text-align: right;">Page 110</p> <p>1 feet, but a mile or a half a mile. And then you won't 2 have to worry about that. 3 In my limited experience, Doctor -- and I'll 4 just put it out here for those of us that don't do 5 this for a living -- you know, if there's one pylon -- 6 if the four lanes of this bridge are based on one 7 pylon, then there's one footprint on the ground with 8 one set of pilings under that. 9 JOHN CLEARY: Yes. 10 HERNDON INGE: But like the Dog River 11 Bridge, it's actually two bridges. It's a two lane 12 southbound and a two lane northbound. 13 JOHN CLEARY: Yes. 14 HERNDON INGE: So there are actually two 15 pylons. And that's just a 70-foot vertical clearance. 16 Now we're talking about a 215-foot vertical clearance, 17 so there's going to be a lot more piling. And 18 it's going to be twice as much if there are actually 19 two bridges as opposed to one. 20 If I remember correctly, those big pilings, 21 those 100-foot-long by three-feet-by-three-feet 22 precast concrete pilings, for that 70-foot clearance, 23 under each pylon, as I recall, there were 100 pilings.</p>	<p style="text-align: right;">Page 112</p> <p>1 the vibrations ruin Christ Church Cathedral's window, 2 then you can repair it. But it's still ruined the 3 window. 4 The old Goldstein's, when we were growing 5 up, the jewelry store, in building the Wallace Tunnels 6 in the '60s, the whole brick wall collapsed. 7 I mean Fort Conde -- Fort Conde Charlotte 8 House, how can we say -- if you have to have that 9 corridor that's almost directly over the Fort Conde 10 Charlotte House, how can you say that you've got to 11 keep that corridor, knowing that the building is, you 12 know, 200 years old, whatever it is? So we should be 13 considering relocating the corridor. 14 JOHN CLEARY: I'll comment -- I can't 15 comment on the corridor. Can I address this real 16 quick? Is that all right? 17 DAN WAGNER: Yeah. 18 JOHN CLEARY: I can address two things that 19 you mentioned, though, that I do know about. The 20 number of piles that are underneath there, that's 21 dependent on many factors: the soil type, the length 22 of them. You know, you can use a smaller diameter and 23 put longer lengths at times, or larger diameter and</p>
<p style="text-align: right;">Page 111</p> <p>1 Now, it was something like that. I don't think it was 2 10 and I don't think it was 40. I think it was 100. 3 So for every footprint of every piling on the two 4 bridges, there were a hundred 100-foot pilings. And 5 that was for a bridge with a vertical clearance 6 one-third as much as this bridge. So you have to 7 worry about all this. 8 Another little vignette is the engineer that 9 worked with us on the Dog River Bridge had also been 10 consulted in putting in the Wallace Tunnels. And they 11 were talking about vibrations in the stained-glass 12 windows that at that time that were 100 and something 13 years old, very valuable and couldn't be replaced. So 14 he had a plumb-bob -- so he said: I've got an idea. 15 I'll put a plumb-bob in the window and put a dot on 16 the windowsill and see if that plumb-bob moves as 17 you're building the Wallace Tunnel. Well, the plumb- 18 bob didn't move. The windows cracked, but the plumb- 19 bob didn't move. 20 And so I'm just saying if you're married to 21 this corridor, then you have to worry about 22 vibrations. If you move the corridor, then you don't 23 have to worry as much about vibrations. Because once</p>	<p style="text-align: right;">Page 113</p> <p>1 shorter length, more of them. It's affected by 2 obviously the load on the bridge. So the length of 3 the bridge is going to affect it. The height does 4 affect it some because when you have wind blowing at 5 it, you get what we call overturning, tries to push it 6 over. So there's a lot of factors that affect that. 7 But if you change the technique -- the type 8 of structure, you can change the number that go 9 underneath it. For example, some very large bridges 10 use a single pile as such. It's a drilled caisson. 11 But it might be 20 or 30 feet in diameter and go down 12 3 or 400 feet. So it's really highly dependent on 13 what's underneath it. 14 The other comment is -- you alluded a little 15 bit to construction technique. So when they built the 16 tunnel, it caused vibrations. I think it's really 17 important the construction technique that you're going 18 to use for putting the foundation system in can affect 19 vibrations a lot, too. 20 And then the third thing is the plumb-bob. 21 I would never recommend anything like that for 22 vibration monitoring. I'll go on record as saying 23 that. I think using things like geofoam, we can do</p>

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<p style="text-align: right;">Page 114</p> <p>1 crack monitoring. Some type of crack monitoring, if 2 you have an existing crack, you can put what's called 3 a crack gauge on there and visually see if the crack 4 changes. There's also electronic versions of those. 5 But I think the vibration monitoring using geofoam is 6 a much better technique to determine what the 7 vibration levels are outside the building. 8 DAN WAGNER: You pretty much answered my 9 question. I was just curious. With the soil 10 consistency, that change is not an option. 11 JOHN CLEARY: Yeah. It really depends on 12 what the soil is at the location of what type of 13 capacity that pile can have. You can put the same 14 exact type pile in two very different soil conditions 15 and have extremely different capacities of that pile. 16 So that's something that goes into the design when 17 they get to the actual design phase. They'll do an 18 analysis of the soil conditions at each location that 19 a piling would be put in to determine the best 20 foundation system. 21 Any other questions for me? Yes? 22 ELIZABETH MERRITT: I wanted to ask a couple 23 of questions. One is can you put the slide back that</p>	<p style="text-align: right;">Page 116</p> <p>1 actual brick facade of the building we didn't have any 2 measurable vibrations. 3 ELIZABETH MERRITT: Okay. Well, that was 4 actually going to be my next question, about where one 5 places the monitor devices. You know, do you place it 6 on the building? Do you place it on the foundation? 7 Do you place it in the ground next to the building? 8 JOHN CLEARY: All of the above at times. 9 The typical thing to do is to put it on the ground 10 near the building but on the side of the building 11 where you're going to have -- you don't want to put it 12 on the back side because the building is going to 13 shield it. So if, for example, we were monitoring 14 this building, you would put maybe a vibration monitor 15 here and then another one here. 16 Now, at times it is attached to the building 17 itself because if you're very worried about how the 18 vibrations are affecting the building, not just in the 19 soil but in the actual structure, at times vibration 20 studies will put it connected to the structure, 21 sometimes on the exterior, sometimes on the interior. 22 If you have a basement, maybe a basement wall or a 23 structural member. So that way you can know not only</p>
<p style="text-align: right;">Page 115</p> <p>1 has the actual vibration levels that you all 2 monitored? 3 JOHN CLEARY: This one? 4 ELIZABETH MERRITT: Before that. That one, 5 yeah. And so the structures -- let's see. Does that 6 say steel buildings? 7 JOHN CLEARY: Steel buildings, yes. 8 ELIZABETH MERRITT: Okay. So this chart 9 over here was based on what vibration levels were 10 experienced by those steel buildings? 11 JOHN CLEARY: No. We put vibration monitors 12 coming out linearly from each pile location. And then 13 actually it's not in the slide, but I did measure -- 14 so this is a large steel building here. The front 15 part of it was an office area. And on the front of 16 that building there's a brick facade. So we actually 17 put a data collector on the ground in front of the 18 brick facade and then one on the brick facade to see 19 what level vibrations were being transmitted into it. 20 And I can't remember the distances now, but I want to 21 say it was -- from that picture, it looks like it must 22 have been about 140 feet away. And we had measurable 23 vibrations at the ground, obviously. But on the</p>	<p style="text-align: right;">Page 117</p> <p>1 what are the vibration levels outside the structure 2 but how are those transmitting into the structure. 3 ELIZABETH MERRITT: And another question is: 4 Is there anything that you can do for sensitive 5 buildings to help protect them, any sort of 6 strengthening or something you can put around the 7 building to sort of help it resist vibrations? 8 JOHN CLEARY: There's structural reinforcing 9 you could do to a building. But then you're obviously 10 doing a lot of alterations to the building. So in 11 some cases there may be a building that maybe is in 12 very weak condition and you might be able to do some 13 structural reinforcing. 14 There has been some research into doing 15 things like digging ditches around a structure. Most 16 of the research shows very limited success in actually 17 reducing the vibrations with doing that because the 18 vibrations are propagating -- if you see this, this 19 could be 100 feet deep and the vibrations are actually 20 propagating outward from here. So if you're close to 21 the structure, you'd need a really deep ditch actually 22 to get across it. 23 There's been a few other techniques with</p>

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<p style="text-align: right;">Page 118</p> <p>1 some very limited success. Really the best thing to 2 do with a sensitive structure is to use a type of 3 construction technique that's going to reduce your 4 overall vibrations. 5 HERNDON INGE: Or to relocate the corridor. 6 JOHN CLEARY: Or relocate the corridor. 7 ELIZABETH MERRITT: In the context of 8 underground coal mining, I've seen some of these 9 techniques being used like digging a ditch, like 10 putting sort of a rubberband around the structure. 11 JOHN CLEARY: Yeah. I've read some 12 literature that talked about some techniques that have 13 had limited success on very specific projects. That's 14 very unique to the actual structure you're looking it. 15 You need to do a very in-depth structural evaluation 16 or analysis of it to see what type of techniques might 17 work for that particular structure. 18 ELIZABETH MERRITT: So I have a question 19 about glass in particular. I'm thinking of the 20 church. Are there situations where the monitor might 21 not register that the vibration level is above the 22 threshold but nonetheless the glass will crack? 23 JOHN CLEARY: Yeah. Well, what you could</p>	<p style="text-align: right;">Page 120</p> <p>1 vibrations, that you can effectively protect a 2 structure. Now, you can dig a pit and put it down in 3 the ground. When they do seismic monitoring, they 4 typically put them down in the ground for earthquakes. 5 But what you'd really be doing then is you'd also be 6 changing the level that you want to limit vibrations 7 to. Because the studies that have looked at it have 8 always looked at surface vibrations. 9 But that is something that I feel the .5 and 10 the .1 would take into account, the fact that you 11 could be getting transmission from the ground soil up 12 through your foundation system. 13 DEVEREAUX BEMIS: Yeah. To me that would be 14 the main worry, particularly with something like 15 Christ Church that has a very deep foundation. That 16 vibration would be down and you wouldn't feel it so 17 much at the top. 18 JOHN CLEARY: Yeah, yeah. 19 DEVEREAUX BEMIS: But it could still weaken 20 the foundation, particularly in these old buildings 21 they used a lime-based mortar and it's not 22 particularly secure. 23 JOHN CLEARY: Yeah. Very what we call</p>
<p style="text-align: right;">Page 119</p> <p>1 have is a situation where lower vibration levels than 2 expected could potentially cause damage. And so 3 that's something that -- what most people do to deal 4 with that is they do preconstruction surveys and they 5 go through and they make sure they document the exact 6 condition of the structure before you start and then 7 do close monitoring, not only with vibration 8 monitoring but also visual inspections and good 9 communication with the owners of that property and the 10 users of that property that if something is occurring, 11 that you can, you know, come up with mitigation 12 strategies. 13 ALFEDO ACOFF: Devereaux has a question. 14 JOHN CLEARY: Oh, I'm sorry. 15 DEVEREAUX BEMIS: It really has to do with 16 this slide, too. Because a lot of historical 17 buildings will have spread footings that go down 18 pretty far. So I don't think it's the surface that 19 you've got to worry about so much as down deep. Do 20 you put your monitors in the ground? 21 JOHN CLEARY: You can actually put the 22 monitors down. Most of the research has shown, 23 though, that if you measure and limit the surface</p>	<p style="text-align: right;">Page 121</p> <p>1 brittle structures. 2 HERNDON INGE: Let's assume that you were 3 selected to test within similar soil to the chosen 4 corridor. Okay? And you're very familiar with 5 piledriving techniques, but your focus is the 6 vibration. Explain to all of us -- we're interested 7 in a lot of -- we're interested in all of the impacts 8 to historic structures, not just vibration but noise. 9 It's been said that the construction will take between 10 five and ten years. Explain the noise pollution of 11 piledriving immediately adjacent to downtown over a 12 five- or ten-year construction schedule. 13 JOHN CLEARY: So we did not do a noise 14 analysis for this. But I will say piledriving is 15 noisy. Construction activity is noisy. There's 16 definitely no doubt about that. And there have been 17 cases where people have chosen not to do piledriving 18 because of the noise. I think one thing to keep in 19 mind, though, is -- I don't have a good picture of it. 20 But if we -- maybe we can talk here. 21 If you're in this area, even though it's a 22 five- to ten-year project, they won't be driving piles 23 for five to ten years in this area. The piledriving</p>

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<p style="text-align: right;">Page 122</p> <p>1 would start at some point and then continue outward. 2 So there would be other construction activity 3 occurring, but it wouldn't be piledriving necessarily, 4 that it wouldn't be five to ten years. Yeah, Vince 5 can definitely talk better on that. 6 VINCE CALAMETTI: I think that's right. 7 It's a step progression for the construction. IT 8 would move ahead. 9 But I wanted to go back. I think you 10 mentioned a little bit about a drilled shaft. Do you 11 know enough about the soil conditions to know would a 12 drilled shaft be possible in these locations? 13 JOHN CLEARY: I'm not a geotechnical expert. 14 But in my opinion, from the soils that I've seen here, 15 I think a drilled shaft could be a potential 16 foundation system that could be used here. 17 VINCE CALAMETTI: And that construction 18 method would actually -- would greatly reduce or 19 eliminate both the noise and the vibration issues? 20 JOHN CLEARY: Yeah. The vibration issues 21 are typically reduced pretty drastically with drilled 22 shaft. And the noise, you don't have the piledriving 23 occurring, so you don't have that constant noise</p>	<p style="text-align: right;">Page 124</p> <p>1 feet. And near the river -- this data is at the river 2 site -- near the river it was about 10 to 20 foot 3 deeper than it was at this location. So at least at 4 those two spots that we did that at, we had very 5 consistent soil properties. 6 Now, obviously -- I mean I come from Ohio. 7 And everywhere in Ohio there's all glacial fill. And 8 you could go two feet over and get something 9 different. So soil is something that's very important 10 to do a lot of analysis on. And typically on most 11 projects, right at the location of your pile, they're 12 going to be doing soil borings to characterize the 13 soil profile at that location. 14 MR. COVINGTON: If I could followup on the 15 noise? The EIS does address traffic noise as 16 predicted but for the 2010 condition, the future no 17 build and then the build condition. And there's also 18 a section in there on traffic noise -- excuse me -- 19 construction noise as well. And the construction 20 noise is almost an environmental commitment. It says 21 that construction noise is temporary and that the 22 contractor would be expected to abide or follow the 23 ALDOT construction methods that address specifically</p>
<p style="text-align: right;">Page 123</p> <p>1 that's occurring. 2 DEVEREAUX BEMIS: Vince, I would remind you 3 of what happened with GulfQuest where they were 4 surprised at what the soil was, that there was fill in 5 there. You know, keep your engineers apprised that 6 all of this is fill. You're going to get God knows 7 what in this stuff when you do it. 8 JOHN CLEARY: I can say at least in the 9 immediate vicinity of this project -- let me find the 10 right slide here. And obviously it could change 11 anywhere. There it is. We actually evaluated two 12 locations to do the piledriving study at. And the one 13 was the site we used. The other one, this is the 14 Bender administration office building or something 15 it's called. And so we actually had soil borings at 16 this location and this location where we have looked 17 at the soil, what was in the soils there. And we 18 actually found a very -- and it's 600 feet apart. So 19 obviously if we go in the other direction, we could 20 get something different, particularly along the river. 21 But the soils were very consistent actually 22 from this location to this location. And the only 23 thing that we saw is there's a clay layer at about 100</p>	<p style="text-align: right;">Page 125</p> <p>1 construction noise. I can't cite those chapter and 2 verse to you, but that is the commitment in the 3 document regarding construction noise. 4 MARK BARTLETT: Are there any other 5 questions? 6 ELIZABETH MERRITT: I have a followup 7 question perhaps for Mr. Calametti about the 8 construction technique called the drilled shaft 9 technique. Is that generally more or less expensive 10 or the same as the traditional piledriving 11 construction technique? 12 VINCE CALAMETTI: I think that depends on 13 the soil characteristics. Sometimes you can take -- 14 you can just auger it out and fill it with a reusable 15 grout and that you drill it out, you fill it up with 16 grout as you go down. Then as you pump the concrete 17 from the bottom up, you get the grout and you can 18 reuse it. So that's one method. In some areas that 19 the outside walls won't stand up on their own, even 20 with grout, you have to reinforce it with a metal 21 shell. So the prices vary depending on the soil 22 conditions. We've seen both down here. And I agree, 23 you have to have good geotech when you go out there,</p>

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<p style="text-align: right;">Page 126</p> <p>1 whether it's concrete pile or drill shaft. 2 MARK BARTLETT: But it's fair to say that 3 during our design process, we'll get a huge amount of 4 data based at every one of the locations of the piers; 5 right? 6 VINCE CALAMETTI: Yes. That is common 7 design practice. 8 DAN WAGNER: The duration, roughly the same 9 to do a piling? 10 VINCE CALAMETTI: Yes. 11 DAVID DAUGHENBAUGH: Quick question on the 12 study. Your study was one pile drive at a time. Is 13 it possible that during this project there will be 14 multiple pile drivers in the same proximity, every 15 other fifth hit at the same time, thus increasing -- 16 JOHN CLEARY: I suppose there could be a 17 potential, depending on the contractor's construction 18 technique, where they're driving multiple piles at the 19 same time. 20 DAVID DAUGHENBAUGH: And it will increase 21 the vibration, say every fifth one hit at the same 22 time? 23 JOHN CLEARY: I'm a professor. I want to</p>	<p style="text-align: right;">Page 128</p> <p>1 JOHN CLEARY: Yeah. 2 VINCE CALAMETTI: So there's various options 3 that the contractors have. We use it on projects. We 4 use it on projects now currently. 5 MARY ANN NABER: I just have one more 6 question about the recommended distance for placing 7 your monitors. Is that what you would say to be the 8 sort of -- you would want to place the monitoring on 9 the outside of the area that you would anticipate 10 there might be damage so you could -- 11 JOHN CLEARY: Yes. It really serves two 12 purposes. You would look at it and you would say if 13 I've got a structure that I would say is potentially 14 sensitive within 250 feet, I absolutely need to do 15 monitoring at that location. And, you know, it would 16 probably be recommended to do preconstruction and post 17 construction surveys to make sure you can limit damage 18 on it as well. 19 And then the other thing would be is if 20 there's structures beyond that 250 feet, you might 21 want to monitor either at 250 feet or whatever the 22 edge of your right-of-way is to make sure you don't 23 get vibrations that are larger than you expect. We'd</p>
<p style="text-align: right;">Page 127</p> <p>1 start writing on the board. 2 So when you have that impact occur, you have 3 a wave that generates outward. So if you had two 4 impacts occurring, you'd have two waves generating 5 from two different locations. There is always a 6 potential, some probability, that those waves could be 7 additive at the location of your historic structure. 8 And I think that's where monitoring is really 9 important so you can make sure that doesn't occur. 10 Now, statistically you could say if you only 11 had two, it's got a fairly low probability that they 12 would be additive, you know, because they could 13 actually cancel each other. In theory they could be 14 occurring at just the right frequency where you get 15 zero vibrations. Obviously we know that's not going 16 to occur. But you could get an increase. And I think 17 that's where the vibration monitoring is very 18 important. 19 VINCE CALAMETTI: And also setting the 20 maximum limits. That's very important. The 21 contractor, again, like John said, you can do the 22 cushion. If you reach those limits, the contractor 23 can reduce the hammer size.</p>	<p style="text-align: right;">Page 129</p> <p>1 expect that we would never reach over .1 inch per 2 second in 250 feet. We want to do monitoring to 3 ensure we don't do that. 4 MARY ANN NABER: So could we extrapolate 5 from this that structures that are greater than 250 6 feet from where the piledriving is being done would be 7 highly unlikely to be -- 8 JOHN CLEARY: Yes. They would have a very 9 low probability of damage at that distance. I don't 10 like to use probabilities because there's always a 11 probability. But it would be a low probability of 12 damage. 13 ELIZABETH MERRITT: Is there any chance that 14 we could get a copy of your presentation, an 15 electronic copy of it? 16 JOHN CLEARY: I think that's up to -- above 17 my head. 18 VINCE CALAMETTI: I'm sorry? 19 MARK BARTLETT: A copy of the presentation? 20 VINCE CALAMETTI: It's an approved project. 21 ALFEDO ACOFF: It's no problem. 22 ELIZABETH MERRITT: That would be very 23 helpful.</p>

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<p style="text-align: right;">Page 130</p> <p>1 ALFEDO ACOFF: It's the same information in 2 the -- 3 MR. CLEARY: Yes. This is the same 4 information from the final report on vibrations. And 5 that report, I believe, was just recently approved. I 6 don't think ALDOT likes to use the terminology 7 "approved." 8 ALFEDO ACOFF: Finalized. 9 JOHN CLEARY: Finalized. That's the word 10 they like to use. Finalized. So I don't know how 11 that is available. 12 ALFEDO ACOFF: When I get back to the 13 office, I will talk to Jeff Brown. But they have sent 14 an email saying -- 15 JOHN CLEARY: Yes. I saw the email that 16 said it was final. 17 ALFEDO ACOFF: There were a few comments and 18 stuff, but I think it's more grammar and stuff like 19 that, clarifying stuff. So that will be done and the 20 final report will be sent out. 21 MARK BARTLETT: But your question was really 22 about the presentation; right? 23 JOHN CLEARY: Yeah. And I think --</p>	<p style="text-align: right;">Page 132</p> <p>1 right-of-way are protected through the Mobile Tree 2 Commission. And any projects that impact those 3 particular trees need to be run through the Mobile 4 Tree Commission. 5 The City of Mobile with Urban Forestry, we 6 also look at tree plantings but also protection on 7 private property. And we do have some plantings and 8 some guidelines for the downtown district. But at 9 this point the plans that you have here are so 10 preliminary, and I haven't seen them. So I would 11 think that yes, there are some goals that we want to 12 look at in the area that we have high winds. We'd 13 like to have something that would visually buffer the 14 impact of the structure that's being built. But 15 coming up with selection locations, I think we're 16 really preliminary other than to say, hey, let's do 17 evergreen and try to look at wind-resistant 18 vegetation. 19 MARK BARTLETT: Is there a question about 20 tree plantings or canopy protection? 21 DEVEREAUX BEMIS: Just so you know, I think 22 what we were looking at particularly for the districts 23 were the corridors on Claiborne Street and Royal</p>
<p style="text-align: right;">Page 131</p> <p>1 MARK BARTLETT: That was the question. 2 VINCE CALAMETTI: The presentation can be 3 part of the minutes of the meeting. 4 MARK BARTLETT: Yeah, we can do that. We 5 can provide that as part of the minutes. 6 JOHN CLEARY: Any other questions for me on 7 vibrations? 8 (NO RESPONSE.) 9 JOHN CLEARY: Well, thank you for listening 10 to me talk. And I appreciate the comments. 11 MARK BARTLETT: I think we're getting a 12 little late in the hour. But I would just like to ask 13 that we stick to it a little bit longer. As we 14 advance the project, one of the issues that we talked 15 about is some of the canopy issues. And we've asked 16 the City to come, Urban Forestry, and talk a little 17 bit about some of the activities that the City has to 18 protect the canopy and things that they might be doing 19 to further encourage the canopy in the City. 20 David, can you kind of share a little bit 21 with us and maybe answer some questions? 22 DAVID DAUGHENBAUGH: Sure. The existing 23 trees within the City of Mobile on the City's</p>	<p style="text-align: right;">Page 133</p> <p>1 Street where we have that long view. And I frankly 2 love the palm trees, but on Canal Street those aren't 3 going to do much for visual. 4 DAVID DAUGHENBAUGH: I think, just to 5 followup as well, there has been some discussion on 6 tree plantings in the downtown area. And that is 7 ongoing within the City of Mobile with the new 8 administration. So right now we need to really follow 9 through that process and find out where we need to go 10 to implement that type of vegetation into this 11 program. 12 MARK BARTLETT: Sure. And I think the 13 department stands ready to talk about how we can 14 incorporate the plantings into the project, talk about 15 how we can actually incorporate some protection 16 working with the City with some type of agreement. 17 The next area I guess is the visualization. 18 J.D. is here. Does anybody have any questions about 19 the visual aspect? I know we've talked a lot about 20 it. I guess we're going to be presenting tonight at 21 the hearing, if you're going to be there, and we're 22 going to be showing the visualization of that; 23 correct?</p>

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<p style="text-align: right;">Page 134</p> <p>1 JOHN D'ARVILLE: It's rendering 2 examinations. We're also going to show it through 3 technology tonight called the Live Cube which will 4 actually circumvent the whole project. 5 ALFEDO ACOFF: Is that like from a certain 6 spot? 7 JOHN D'ARVILLE: Yeah. We can take them 8 where they want to go inside the project. 9 ALFEDO ACOFF: Sort of like a video game? I 10 mean I'm just saying. 11 JOHN D'ARVILLE: There's a lot that went 12 into it. 13 ALFEDO ACOFF: I'm talking about my 14 grandson. He loves video games. It's animated. 15 That's what I'm trying to say. 16 JOHN D'ARVILLE: We'll be there to show them 17 that. 18 MARK BARTLETT: That's good. Does anybody 19 have any questions, anything about the visual? 20 (NO RESPONSE.) 21 MARK BARTLETT: Well, I guess, you know, as 22 we look at the next steps -- and you'll notice the 23 next thing on the agenda is kind of a wrap-up. And we</p>	<p style="text-align: right;">Page 136</p> <p>1 strongly, based on our previous discussions, that they 2 were willing to step up and put those commitments out 3 there early. And this is their initial offering. 4 So if you have ideas about what types of 5 additional input you would like to have during the 6 process, input you would like to have from a 7 context-sensitive design, input into the aesthetics of 8 the bridge, input into the lighting, those type of 9 things, we would like to hear from you on evaluating 10 how a programmatic agreement might be implemented. 11 So with that, I would say we would like also 12 to engage in further communications and coordination. 13 You know, as we develop an initial programmatic 14 agreement in an outline form, very bullet oriented, 15 basic construct, being able to have the different 16 areas that we would like to fashion the agreement on 17 and develop some items under each one, come back 18 together either by phone or in person on the project 19 and talk about how we might be able to develop this 20 into a larger programmatic agreement. 21 So with that, I'll ask are there any other 22 questions? Yes? 23 CHRIS WILSON: I just want to reiterate, as</p>
<p style="text-align: right;">Page 135</p> <p>1 want to talk a little bit about the next steps that we 2 have in the process. 3 As we've reviewed some of the comments and 4 heard some of the comments here today, we would like 5 to propose that we do initiate at least the initial 6 part of developing a programmatic agreement. And what 7 we would like to do is kind of come up with an outline 8 form, working with ALDOT, including some of the 9 comments that we got from the Historic Commission as 10 well as the Alabama Historic Commission and talk about 11 how we can incorporate some of these ideas into an 12 agreement to develop a process base for a programmatic 13 agreement that will help us move forward. 14 So I guess what we would like to ask of you 15 is to provide us with some additional comments and 16 insight as to what you might like to see covered in 17 one of these types of agreements. And you'll notice 18 we've got a little block there on the chart there that 19 says input on mitigation and commitments. So you've 20 kind of got the opportunity to see what ALDOT has put 21 on the table in the way of commitments. I think this 22 is an early commitments statement. We put those in 23 later in our later documents. But ALDOT felt</p>	<p style="text-align: right;">Page 137</p> <p>1 you know, through the 106 process, you're supposed to 2 take into account what consulting parties say and what 3 their input is. And unless I'm in another meeting, in 4 this meeting it's been a very unified response from 5 many consulting parties that there are some adverse 6 effects. So I'd like you to consider that in your 7 next steps. I have worked for ACHP for several years 8 and I know the methodology. And I know there are 9 serious constraints, there are other environmental 10 issues outside of cultural. The NEPA portfolio is 11 huge. This certainly isn't the only thing that you 12 have to consider. 13 But I think it's been fairly unanimous 14 within the consulting parties and the preservation 15 constituency here, local, state and national level, 16 that there are adverse effects. So I'd like you to 17 consider that when you do your summary of this meeting 18 and when you conduct your next steps. 19 MARK BARTLETT: Okay. 20 ELIZABETH MERRITT: I would like to also 21 request, because you referred to the commitments that 22 ALDOT has already put on the table, if we could 23 receive some kind of followup clarification about what</p>

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<p style="text-align: right;">Page 138</p> <p>1 those commitments are exactly because of the fact that 2 the slide had references that didn't explain the 3 commitments or were erroneous. It's a little unclear 4 exactly what commitments are being offered at this 5 point, and we would certainly appreciate more 6 clarification. 7 MARK BARTLETT: One point is in the front of 8 the document, in the very front, there's the 9 transmittal letter of the actual commitment that was 10 signed by the chief engineer that actually outlines 11 each one of those commitments in the document. So 12 this is the commitments that they've transmitted with 13 the documents. This is what they would like to offer 14 as a commitment on the project. So that is the 15 very -- each individual item has a lot more 16 information than what was presented on the slide. 17 ELIZABETH MERRITT: So just because this is 18 an unconventional procedure sort of, is the proposal 19 that if you all could write the PA and be done with it 20 tomorrow, that that's what you put in this Section 106 21 programmatic agreement? Is that the suggestion? 22 MARK BARTLETT: Well, I think what -- if I 23 could characterize the department's commitment, over</p>	<p style="text-align: right;">Page 140</p> <p>1 MARK BARTLETT: Any other questions? 2 (NO RESPONSE.) 3 ALFEDO ACOFF: Like the Fish &amp; Wildlife 4 Service, if something has come up like the manatee, 5 that's from a different agency. So different things 6 will be changed a little bit. 7 MARK BARTLETT: And I guess I mentioned 8 earlier that we would like to see some comments back. 9 Currently the Draft EIS is out for public comment, and 10 that comment period ends on November 7th. So, if 11 possible, we would like to have the same time frame 12 apply to this. And that way everybody knows the dates 13 are about the same to get that information together. 14 And I guess we're going to develop the 15 minutes for this meeting. 16 ALFEDO ACOFF: We are. We have a court 17 reporter that's taking minutes. And several people in 18 the room were taking minutes at this time. We'll send 19 the minutes out to the group. 20 I didn't know if Mr. Cooper wanted to say 21 anything. 22 MARK BARTLETT: Did you want to say 23 anything?</p>
<p style="text-align: right;">Page 139</p> <p>1 time they've coordinated with a lot of different 2 individuals about a lot of different activities. And 3 what we've heard is that we would like some 4 consideration given to the following areas. And these 5 are the commitments that ALDOT has put toward in the 6 areas of lighting, aesthetics, vibration, and the 7 other areas that have the commitments listed. And so 8 that's kind of the way I would characterize them. 9 There's a few pages in the front of the document that 10 Ronnie Baldwin actually basically stated that this is 11 the department's commitment in the development of the 12 project. 13 MARY ANN NABER: But it doesn't preclude 14 consideration of additional methods. Of course, at 15 this stage, again, step 3 in the 106 process, we're 16 still looking to consider means of avoiding any 17 adverse effects or impacts. 18 ELIZABETH MERRITT: And minimizing. 19 MARY ANN NABER: And minimizing those if we 20 can. 21 ELIZABETH MERRITT: And clearly, some of 22 these -- like here's one on protective species. That 23 wouldn't go in a Section 106 agreement.</p>	<p style="text-align: right;">Page 141</p> <p>1 JOHN COOPER: No. 2 MARK BARTLETT: Very good. I appreciate 3 everybody coming out to this meeting and hope to see 4 you tonight. 5 (THE MEETING CONCLUDED AT 12:20 P.M.) 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23</p>

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**DISPOSITION OF COMMENTS RECEIVED FROM SEPTEMBER 23, 2014 SECTION 106 CONSULTING PARTY MEETING**

**Alabama Historical Commission**

	<b>Comment</b>	<b>Response</b>
1.	SHPO continues to agree with their original comments regarding visual impacts to some of the districts.	As discussed in Section 4.13.1 of the Supplemental DEIS, the determination of effects was changed from “no adverse effect” to “adverse effect.”
2.	SHPO asked about noise, where the bridge would begin, and the grade of the bridge.	Sections 4.10 and 5.5.3 of the Supplemental DEIS address potential noise impacts.  The bridge would begin elevating just east of the Virginia Street interchange. The maximum grade would be 4 percent.
3.	SHPO stated that they are not convinced that there will not be any impacts or adverse impacts and they will need more information to assess impacts on archaeological resources.	As discussed in Section 4.13.1 of the Supplemental DEIS, the determination of effects was changed from “no adverse effect” to “adverse effect.”  As discussed in Section 4.13.8 of the Supplemental DEIS and in the Draft Section 106 MOA in Appendix L, the approach to performing archaeological surveys and the findings of those surveys to date have been coordinated with the SHPO.

**Mobile Historic Development Commission**

	<b>Comment</b>	<b>Response</b>
1.	MHDC asked what the grade of the bridge would be and where the project would begin.	The maximum grade would be 4 percent. The bridge would begin elevating just east of the Virginia Street interchange. The project would begin at the Broad-Duval Street interchange.

2.	MHDC stated that they like the Vibrations Study and recommended that buildings such as St. Matthews, Council Traditional School, Prince of Peace, GulfQuest, and the cruise terminal be monitored.	As specified in the Draft Section 106 MOA in Appendix L, all modern structures within 150 feet of construction operations, and all historic structures within 250 feet of construction operations will be monitored for vibration impacts. Buildings to be monitored beyond those distances have been consulted on with the Consulting Parties and are identified in the Draft Section 106 MOA.
3.	MHDC would like something to help with noise mitigation.	Noise abatement measures have been reviewed and determined not to be feasible and reasonable in accordance with ALDOT's Noise Policy. Section 4.10 of the Supplemental DEIS contains more information on potential noise impacts.
4.	MHDC would like for final lighting design plans to wait until the project is closer to completion.	As discussed in Section 4.12 and the Draft Section 106 MOA in Appendix L, the selection of light fixtures and technology will be deferred until the final design process in order to incorporate the latest available technology to minimize light spill.
5.	MHDC is concerned about how the space under the bridge will be utilized. MHDC would like to see something happen under the bridge that would make it an amenity for the City.	As discussed in Section 6.8 and in the Draft Section 106 MOA in Appendix L, an Aesthetic Steering Committee has been developed to provide input on the aesthetics of the project, which includes treatment of the area beneath the bridge.
6.	MHDC is pleased with the plan and that direct impacts to historic districts and buildings are being avoided. MHDC wanted to know what would be done if damage from vibrations occur.	As discussed in the Draft Section 106 MOA in Appendix L, any damage to historic structures due to vibration levels above the maximum shall be repaired/restored in accordance with ALDOT Specification 107.12, 107.14, and 107.15 Protection and Restoration of Property, Landscape, and Utility Facilities, 36 CFR 800.12 Emergency Situations and 36 CFR 68 The Secretary of Interior's Standards for the Treatment of Historic Properties.
7.	MHDC requested time to review the archaeological surveys.	The results of the archaeological surveys will be sent to the Section 106 Consulting Parties for review after they have been coordinated with the SHPO.

8.	MHDC wants to see commitments related to lighting and vibrations included in the FEIS.	The Draft Section 106 MOA in Appendix L contains commitments related to lighting and vibrations.
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**BAE Systems – No Comments**

**Herndon Inge, Stop the Bridge Coalition**

	<b>Comment</b>	<b>Response</b>
1.	Mr. Inge respects the need for a bridge but voiced concerns about the effect the project would have on his quality of life, children, grandchildren, a cemetery, his house, and his office.	Potential impacts that may result from the proposed project and proposed mitigation measures are discussed in Chapter 4 of the Supplemental DEIS. The proposed project would not impact any cemeteries.
2.	Mr. Inge stated that there is confusion about how high the deck will be and noted that the design of the structure is not yet known.	The final design of the proposed project will not be complete until after FEIS/ROD. At the edge of the channel, the bridge deck height is expected to be around 230 to 240 feet.
3.	Mr. Inge believes the project will have an adverse visual effect on Church Street East, Lower Dauphin Street, Oakdale, Maysville, Union Hall, Old Southern Market, and Government Street Presbyterian Church.	By letter dated June 11, 2015, the SHPO concurred with the adverse visual effect determination on Church Street East Historic District and Lower Dauphin Street Historic District.
4.	Mr. Inge stated that Cooper Riverside Park would no longer exist because pylons and a suspension bridge would be above it.	The proposed project would not result in the destruction of Cooper Riverside Park. Cooper Riverside Park was designed to allow for a bridge pylon to be placed in the park area should it be necessary. The Preferred Alternative would not require a pylon in Cooper Riverside Park, and the bridge would not be located above the park.
5.	Mr. Inge stated that a low bascule bridge option could accommodate ship traffic.	A bridge type study was performed, and it was determined that a cable-stayed bridge would best meet the requirements for the span length required to cross the Mobile River and the navigation channel.



**National Trust for Historic Preservation**

	<b>Comment</b>	<b>Response</b>
1.	NTHP feels that the impacts will be adverse particularly in the area of visual impacts.	As discussed in Section 4.13.1 of the Supplemental DEIS, the determination of effects was changed from “no adverse effect” to “adverse effect” due to visual impacts.
2.	NTHP is concerned about vibration impacts.	Section 4.14.3 of the Supplemental DEIS and the Draft Section 106 MOA in Appendix L contain more detailed information on the Vibration Study performed and proposed mitigation measures.
3.	NTHP is concerned about access to the USS ALABAMA.	As discussed in Section 4.13.5 of the Supplemental DEIS and in the Draft Section 106 MOA in Appendix L, additional consultation with the Battleship Commission has occurred since this meeting, and concepts to provide direct access to the Park were evaluated and shared with the Commission but were found to not be design criteria.
4.	NTHP would like for other bridge types to be explored that are not so visually intrusive.	A bridge type study was performed, and it was determined that a cable-stayed bridge would best meet the requirements for the span length required to cross the Mobile River and the navigation channel.
5.	NTHP suggested the alignment be shifted further away from downtown.	A full range of reasonable alternatives, including alternatives to the north and south, has been evaluated, as described in the Alternatives Screening Evaluation contained in Appendix B of the DEIS. The Preferred Alternative would shift the bridge as far away from downtown Mobile as possible while minimizing impacts on the maritime industry.
6.	NTHP suggested that access to USS ALABAMA be included.	As discussed in Section 4.13.5 of the Supplemental DEIS and in the Draft Section 106 MOA in Appendix L, additional consultation with the Battleship Commission has occurred since this meeting, and concepts to provide direct access to the Park were evaluated and shared with the Commission but were found to not be design criteria.

	<b>Comment</b>	<b>Response</b>
7.	NTHP wants more data on assumptions used in noise analysis.	Sections 4.10 and 5.5 and Appendix J of the Supplemental DEIS contain more information on the noise analysis and traffic noise impacts.
8.	NTHP wants more information concerning why the height was changed and whether the businesses being relocated are historic.	Appendix C of the DEIS contains the report prepared to determine the appropriate air draft clearance for the proposed Mobile River Bridge. USCG concurrence with the proposed air draft clearance is contained in Appendix A of the Supplemental DEIS.
9.	NTHP requested a commitment be made to the level of acceptable vibration.	Section 4.14.3 of the Supplemental DEIS and the Draft Section 106 MOA in Appendix L contain commitments related to acceptable vibration levels.
10.	Visual impacts should be evaluated from being in the historic district and looking at the bridge and potential economic impacts on historic resources.	The Viewshed Impact Assessment in Appendix

**Advisory Council on Historic Preservation**

	<b>Comment</b>	<b>Response</b>
1.	Need to agree on adverse effect so that discussions about mitigation can be held.	As discussed in Section 4.13.1 of the Supplemental DEIS, the determination of effects was changed from “no adverse effect” to “adverse effect.”
2.	National Park Service needs to be involved if project has adverse effects.	National Park Service accepted FHWA’s invitation to serve as a Consulting Party by letter dated May 14, 2018.

**USS ALABAMA Battleship Commission**

	<b>Comment</b>	<b>Response</b>
1.	Requested additional access to Battleship Park be considered.	As discussed in Section 4.13.5 of the Supplemental DEIS and in the Draft Section 106 MOA in Appendix L, additional consultation with the Battleship Commission has occurred since this meeting, and concepts to provide direct access to the Park were

	<b>Comment</b>	<b>Response</b>
		evaluated and shared with the Commission but were found to not be design criteria.

**Christ Church Cathedral**

	<b>Comment</b>	<b>Response</b>
1.	Understand the need for the project but expressed concern about vibration impacts because they experienced vibration from the tunnel construction, construction of Government Plaza, and the demolition of the old County jail.	Section 4.14.3 of the Supplemental DEIS and the Draft Section 106 MOA in Appendix L contain information on the Vibration Study, including a construction monitoring system for vibration impacts and proposed mitigation measures. Christ Church Cathedral will be monitored for vibration impacts.
2.	Expressed concern about what will be beneath the bridge once it is constructed and the need to maintain continuity between neighborhoods.	As discussed in Section 6.8 and in the Draft Section 106 MOA in Appendix L, an Aesthetic Steering Committee has been developed to provide input on the aesthetics of the project, which includes treatment of the area beneath the bridge.

**City of Mobile Urban Forestry**

	<b>Comment</b>	<b>Response</b>
1.	Permits from the Mobile City Tree Commission may be necessary if trees are going to be impacted.	Comment noted. Additional coordination with the City of Mobile and its Right Tree Right Place program will be conducted as the design progresses, and ALDOT has committed to mitigation measures listed in the Draft Section 106 MOA in Appendix L.
2.	Impacts to landscaping at existing commercial properties need to be addressed.	As noted in the Draft Section 106 MOA in Appendix L, an Aesthetic and Landscape Plan will be required as part of the proposed project to minimize potential visual impacts on historic resources.

	<b>Comment</b>	<b>Response</b>
3.	Concerns about the treatment of the area beneath the bridge.	As discussed in Section 6.8 and in the Draft Section 106 MOA in Appendix L, an Aesthetic Steering Committee has been developed to provide input on the aesthetics of the project, which includes treatment of the area beneath the bridge.

## **APPENDIX L-3:**

**December 2, 2014**

**Meeting with ACHP and SHPO**

**December 2, 2014 Meeting with ACHP and SHPO**

**Volkert Contract No. 911602.12**  
**Project DPI-0030(005)**  
**I-10 Mobile River Bridge and Bayway Widening EIS**  
**Mobile and Baldwin Counties, Alabama**

**RESUME OF MEETING**

**DATE OF MEETING:** December 2, 2014 at 1:30pm  
**LOCATION:** ALDOT ETS Conference Room - Montgomery, Alabama  
**PURPOSE:** Agency Coordination Meeting

<b>ATTENDANCE</b>	<b>REPRESENTING</b>	<b>EMAIL</b>	<b>TELEPHONE</b>
Chris Wilson*	ACHP	<a href="mailto:cwilson@savingplaces.org">cwilson@savingplaces.org</a>	202-517-0229
Lee Anne Wofford	AHC/SHPO	<a href="mailto:leeanne.wofford@preserveal.org">leeanne.wofford@preserveal.org</a>	334-230-2659
Amanda McBride	AHC/SHPO	<a href="mailto:amanda.mcbride@preserveala.org">amanda.mcbride@preserveala.org</a>	334-242-3184
Mark Bartlett	FHWA	<a href="mailto:Mark.bartlett@dot.gov">Mark.bartlett@dot.gov</a>	334-274-6350
Lynne Urquhart	FHWA	<a href="mailto:Lynne.urquhart@dot.gov">Lynne.urquhart@dot.gov</a>	334-274-6371
Alfedo Acoff	ALDOT – ETS – Design Bureau	<a href="mailto:acoffa@dot.state.al.us">acoffa@dot.state.al.us</a>	334-242-6143
Natasha Clay	ALDOT – ETS – Design Bureau	<a href="mailto:clayn@dot.state.al.us">clayn@dot.state.al.us</a>	334-242-6315
Pat Patterson	ALDOT – ETS – Design Bureau	<a href="mailto:pattersonp@dot.state.al.us">pattersonp@dot.state.al.us</a>	334-242-6633
Don Powell*	ALDOT – SW Region	<a href="mailto:powelld@dot.state.al.us">powelld@dot.state.al.us</a>	251-470-5220
Edwin Perry, III*	ALDOT – SW Region	<a href="mailto:perrye@dot.state.al.us">perrye@dot.state.al.us</a>	251-470-8243
Matt Erickson*	ALDOT – SW Region	<a href="mailto:Ericksonm@dot.state.al.us">Ericksonm@dot.state.al.us</a>	251-470-8201
Tom Hand	Volkert, Inc.	<a href="mailto:Tom.hand@volkert.com">Tom.hand@volkert.com</a>	251-342-1070
David Webber	Volkert, Inc.	<a href="mailto:david.webber@volkert.com">david.webber@volkert.com</a>	251-342-1070
Brett Gaar	Volkert, Inc.	<a href="mailto:Brett.gaar@volkert.com">Brett.gaar@volkert.com</a>	251-971-3129
Jason Goffinet*	Volkert, Inc.	<a href="mailto:jason.goffinet@volkert.com">jason.goffinet@volkert.com</a>	770-298-9709

\* = Attended meeting via conference call.

**MEETING PURPOSE**

The purpose of the meeting was to discuss the approach to preparing the Programmatic Agreement (PA) between the ACHP/SHPO and the FHWA.

## **I. Response to Adverse Effects Comments**

- FHWA asked if the ACHP had received the FHWA's responses to questions regarding the No Adverse Effects statement.
- ACHP received the responses but did not reply because their questions had already been answered.

## **II. Discussion of Purpose of Meeting**

- FHWA explained that the purpose of this meeting was to discuss the content and format of the PA.
- ACHP stated that discussing the content and format before submittal is a great idea.
- Primary question is the assessment of effects determination. ACHP defers to the SHPO regarding eligibility of resources and the assessment of effects.

## **III. Discussion of the Assessment of Effects Determination**

- Purpose of MOA or PA is to resolve adverse effects through mitigation so a project can move forward.
- Section 106 does not guarantee preservation of resources and is not meant to stop projects. Rather the process can move forward through the completion of a PA if the assessment of effects and mitigation efforts are agreed upon. The project can still move forward while you resolve your adverse effects in your MOA or PA, which is then, signed then the process is concluded.
- It is not a show stopper to state there are adverse effects, it's very common.
- Because the historic resources in downtown Mobile are known, it would be in the best interest to agree to adverse effects and work through the PA and minimize them the adverse effects to the extent that they can be minimized. You can't minimize or mitigate something that's not an adverse effect.
- Project cannot move forward until the federal agencies and SHPO agree on determination of effects.

## **IV. SHPO Questions/Comments**

- Lighting: SHPO has heard from people who live in the district who are worried about the lights making the bridge more visible and lights that would shine into their houses. In the commitments made in the DEIS, the lighting will be shielded and would not encroach any further than it does now. The lighting can be dealt with during the design of the bridge.
- Vibrations: ALDOT has committed to monitoring for vibrations. SHPO asked what will be done if vibrations at a certain level are experienced at a site. FHWA stated that if vibration does occur then the construction will stop until they can decide how to proceed without causing any additional vibration. SHPO stated that they don't think that vibration is going to be an adverse effect, but it's going to be something that can be mitigated by having a plan in place in the event certain circumstances occur.
- Visual Effects: FHWA will work with SHPO on visual effects, planting trees, and maintain tree canopy.
- Aesthetics: FHWA would like SHPO's opinion on the design of the bridge when it comes time. There are different options and different style of bridges.
- Access to Battleship Park: Battleship Park would still be accessible by the Bayway and Causeway. Will not be accessible by the new bridge. Signs will be put up directing traffic to the Park.
- Union Hall: Won't be impacted by Alternative B' but will be impacted by Alternatives A, B, and C. The pylons won't affect the feeling or the setting.

## **V. Next Steps**

- FHWA will send letter to Consulting Parties on determination of effects and request input.



## **APPENDIX L-4:**

### **Consultation on Change to Adverse Effect Determination**

## **Consultation on Change to Adverse Visual Effect Determination**



STATE OF ALABAMA  
ALABAMA HISTORICAL COMMISSION  
468 SOUTH PERRY STREET  
MONTGOMERY, ALABAMA 36130-0900

FRANK W. WHITE  
EXECUTIVE DIRECTOR

November 4, 2014

TEL: 334-242-3184  
FAX: 334-240-3477

Mr. Mark Bartlett  
Federal Highway Administration  
Alabama Division  
9500 Wynlakes Place  
Montgomery, AL 36117

RE: AHC 00-1934  
I-10 Mobile River Bridge and Bayway Widening DEIS  
Mobile and Baldwin Counties, Alabama

Dear Mr. Bartlett:

We appreciate the opportunity to review the Draft Environmental Impact Statement for the above-referenced project. After attending the September 23, 2014, Consulting Parties meeting, and after touring the area with representatives of the Advisory Council on Historic Preservation, the Mobile Historic Development Commission, and the National Trust for Historic Preservation, the Alabama Historical Commission continues to express our concerns over potential visual, auditory, and vibratory adverse effects on historic properties.

While the DEIS addresses certain environmental commitments, we would prefer that FHWA and ALDOT identify and address all potential adverse effects to historic properties in either a Programmatic Agreement and/or a Memorandum of Agreement.

We also understand that Phase II Archaeology will be coordinated with this office and performed as part of the investigation of the Preferred Alternative in the FEIS. We agree that if sites cannot be avoided, FHWA will consult with our office.

Thank you for your continued work on this complicated project. We are committed to working with FHWA and ALDOT to move this project forward. If you have any questions, please feel free to contact Amanda McBride at 334.230.2692 or [Amanda.McBride@preserveala.org](mailto:Amanda.McBride@preserveala.org) or Lee Anne Wofford at 334.230.2659 or [LeeAnne.Wofford@preserveala.org](mailto:LeeAnne.Wofford@preserveala.org).

Sincerely,

Lee Anne Wofford  
Deputy State Historic Preservation Officer

LAW/ALM/RJG/law





U.S. Department  
of Transportation  
Federal Highway  
Administration

Alabama Division

May 18, 2015

9500 Wynlakes Place  
Montgomery, AL 36117  
334-274-6350  
334-274-6352  
[Alabama.FHWA@dot.gov](mailto:Alabama.FHWA@dot.gov)

In Reply Refer To:  
HDA-AL

Ms. Lee Anne Wofford  
Deputy State Historic Preservation Officer  
Alabama Historical Commission  
468 South Perry Street  
Montgomery, Alabama 36130

Subject: Project DPI-0030(005), Mobile & Baldwin Counties  
I-10 Mobile River Bridge & Bayway Widening  
Section 106 Determination of Effects Revisions

Dear Ms. Wofford:

This letter is in response to your June 30, 2014 and November 4, 2014 letters to the Alabama Department of Transportation (ALDOT) highlighting concerns pertaining to the referenced project. These concerns were discussed in our 2014 Section 106 Consulting Parties Meeting and further conversations with your office and the Advisory Council on Historic Preservation (ACHP). Based on these discussions and follow-up meetings, we have made some revisions to our Determination of Effects for the referenced project.

Specifically, your June letter expressed concerns over visual impacts to historic properties within the Church Street East Historic District and the Lower Dauphin Street Historic District. We have determined that there may be adverse visual effects to these historic districts by the Build Alternatives. The proposed project will be clearly visible from the edges of these historic districts. Due to the extensive modern structures (infill) to the southeast of the historic districts, the visual effects of the project will be limited. However, when the project is combined with the visual effects of the numerous other modern structures within view, a cumulative impact will occur. The cumulative impact on the historic districts' viewshed may further diminish the settings of these historic districts, which would be an adverse effect. We are hereby seeking your concurrence with our finding of additional impacts due to visual changes to the settings of these two historic districts.

Once we receive your concurrence with this determination, we intend to notify all consulting parties and begin discussing appropriate mitigation to minimize any adverse impacts. Ultimately, agreed upon mitigation measures would be included in a Memorandum of Agreement that would address the following areas of concern: adverse visual effects; potential effects to archaeological sites; documentation of the Union Hall; bridge aesthetics; access to the Battleship Park; and measures to avoid impacts from lighting and vibration.

If you have any questions or comments regarding this matter, please contact Ms. Lynne Urquhart of this office at (334) 274-6371 or [lynne.urquhart@dot.gov](mailto:lynne.urquhart@dot.gov).

Sincerely,

Mark D. Bartlett, P. E.  
Division Administrator

cc:

Ms. Alfredo Acoff, ALDOT  
Ms. Pat Patterson, ALDOT



STATE OF ALABAMA  
ALABAMA HISTORICAL COMMISSION  
468 SOUTH PERRY STREET  
MONTGOMERY, ALABAMA 36130-0900

FRANK W. WHITE  
EXECUTIVE DIRECTOR

June 11, 2015

TEL: 334-242-3184  
FAX: 334-240-3477

Mark D. Bartlett  
U.S. Department of Transportation  
9500 Wynlakes Place  
Montgomery, AL 36117

Re: AHC 00-0352  
Widen I-10 from Broad Street in Mobile to East of US 98 in Baldwin County  
Baldwin and Mobile Counties

Dear Mr. Bartlett:

Upon review of the above referenced project, we concur with the adverse affect determination. We look forward to developing the MOA.

We appreciate your commitment to helping us preserve Alabama's historic archaeological and architectural resources. Should you have any questions, please contact Amanda McBride at 334.230.2692 or [Amanda.McBride@preserveala.org](mailto:Amanda.McBride@preserveala.org). Have the AHC tracking number referenced above available and include it with any future correspondence.

Sincerely,

A handwritten signature in cursive script that reads "Lee Anne Wofford".

Lee Anne Wofford  
Deputy State Historic Preservation Officer

LAW/AMH/amh



U.S. Department  
of Transportation  
**Federal Highway  
Administration**

Alabama Division

June 25, 2015

9500 Wynlakes Place  
Montgomery, AL 36117  
334-274-6350  
334-274-6352  
[Alabama.FHWA@dot.gov](mailto:Alabama.FHWA@dot.gov)

In Reply Refer To:  
HDA-AL

To: All Mobile River Bridge  
Section 106 Consulting Parties

Re: Project DPI-0030(005), Mobile & Baldwin Counties  
I-10 Mobile River Bridge & Bayway Widening  
Section 106 Determination of Effects Revisions  
AHC Reference Numbers: AHC 00-1934; AHC 00-0352

Dear Sir/Madam:

Since the September 23, 2014 Section 106 Consulting Party Meeting, we have been in consultation with the Alabama Historical Commission (ACH) and the Advisory Council on Historic Preservation (ACHP). Based on these discussions and follow-up meetings, we have made some revisions to our Determination of Effects for the referenced project. Attached is our recent correspondence with the AHC.

The primary areas of concern were visual impacts to historic properties within the Church Street East Historic District and the Lower Dauphin Street Historic District. We have determined that there may be an adverse visual effect to these historic districts by the Build Alternatives. The proposed project will be clearly visible from the edges of these historic districts. Due to the extensive modern structures (infill) to the southeast of the historic districts, the visual effects of the project will be limited. However, when the project is combined with the visual effects of the numerous other modern structures within view, a cumulative impact will occur. This cumulative impact may further diminish the settings of these historic districts, which would be an adverse effect. We are hereby notifying you of our finding of additional impacts due to visual changes to the settings of these two historic districts.

We now propose to begin discussing appropriate mitigation measures to resolve and/or minimize any adverse impacts. Ultimately, agreed upon mitigation measures would be included in a Memorandum of Agreement that would not only address adverse visual effects but potential effects to archaeological sites, documentation of the Union Hall, bridge aesthetics, access to the Battleship Park, and measures to avoid impacts from lighting and vibration, as well.

If you have any questions or comments regarding this matter, please contact Ms. Lynne Urquhart of this office at (334) 274-6371 or [lynne.urquhart@dot.gov](mailto:lynne.urquhart@dot.gov).

Sincerely,

Mark D. Bartlett, P. E.  
Division Administrator

MB/lu/af

Attachments:

1. Section 106 Consulting Parties List
2. FHWA Determination Letter May 2015
3. AHC Response June 2015

# **APPENDIX L-5:**

**Consultation on Draft  
Section 106 MOA**

## **Consultation on Draft Section 106 MOA**





# ALABAMA DEPARTMENT OF TRANSPORTATION

Design Bureau

1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050  
Phone: 334-242-6178 FAX: 334-269-0826



Robert Bentley  
Governor

John R. Cooper  
Transportation Director

February 24, 2016

Title, First Name, Last Name  
Agency/Organization  
Address Line 1  
Address Line 2  
City, State, Zip

RE: Draft *Memorandum of Agreement* for ALDOT Project DPI-0030(005) I-10 Mobile River Bridge and Bayway Widening EIS Mobile and Baldwin Counties, Alabama

Dear Sir or Madam:

Per 36 CFR 800.6(b)(2), ALDOT and FHWA have consulted and shall continue to consult with the State Historic Preservation Office (SHPO), the Advisory Council on Historic Preservation (Council), and other Consulting Parties to seek ways to avoid, minimize or mitigate the adverse effects of our project on historic properties.

Previous consultations on potential mitigation, have led to the attached *Memorandum of Agreement (MOA)*. The MOA also stipulates future opportunities for the SHPO, the Council, and other Consulting Parties to provide input regarding the Section 106 Process. Please take this opportunity to provide feedback on both the proposed mitigation and future opportunities for input.

Please provide your written comments to us by March 25, 2016. Your comments should be mailed to:

Ms. Natasha Clay  
Alabama Department of Transportation  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, Alabama 36110.

Thank you for your continued cooperation and interest in this project.

Sincerely,

William F. Adams, P.E.  
State Design Engineer

By: \_\_\_\_\_  
Alfredo Acoff,  
State Environmental Administrator

Enclosure: Draft Copy of the *Memorandum of Agreement* Section 106 Consultation

cc: FHWA  
ALDOT Southwest Region  
Thompson Engineering  
Natasha Clay, ETS Project Manager  
Cultural Resources File



# ALABAMA DEPARTMENT OF TRANSPORTATION

1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050



**Robert Bentley**  
Governor

**John R. Cooper**  
Transportation Director

July 27, 2016

«Title» «First\_Name» «Last\_Name»  
«Company\_Name»  
«Address\_Line\_1»  
«Address\_Line\_2»  
«City», «State» «ZIP\_Code»

**RE:** *Comments to the Memorandum of Agreement* for ALDOT Project DPI-0030(005) I-10 Mobile River Bridge and Bayway Widening EIS Mobile and Baldwin Counties, Alabama

Dear Sir or Madam:

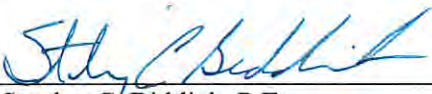
As per our February 24, 2016 letter, Consulting Parties were afforded the opportunity to comment on the Draft MOA by March 25, 2016. Attached please find a copy of all the comments received. Comments will be taken into consideration and incorporated into the next Draft and as the project progresses throughout preliminary design. We anticipate another Draft MOA to be circulated prior to the next Consulting Parties Meeting tentatively planned for this fall.

Per 36 CFR 800.6(b)(2), ALDOT and FHWA shall continue to consult with the State Historic Preservation Office (SHPO), the Advisory Council on Historic Preservation (Council), and other Consulting Parties to seek ways to avoid, minimize or mitigate the possible adverse effects of our project on historic properties.

Thank you for your continued cooperation and interest in this project.

Sincerely,

William F. Adams, P.E.  
State Design Engineer

By:   
Stanley C. Biddick, P.E.  
Asst. State Design Engineer  
Environmental Technical Section

Enclosure: Copy of the Comments to the *Memorandum of Agreement*

cc: FHWA  
ALDOT Southwest Region  
Thompson Engineering  
Natasha Clay, ETS Project Manager  
Cultural Resources File

Mr. Cart Blackwell  
Mobile Historic  
Development Commission  
PO Box 1827  
Mobile, AL 36633-1827

Ms. Lee Anne Wofford  
Alabama Historical Commission  
468 S Perry St  
Montgomery, AL 36130

Commissioner Jerry Carl  
President  
Mobile County Commission  
205 Government St  
Mobile, AL 36644-1001

Commissioner Charles Gruber  
Chairman  
Baldwin County Commission  
201 E Section Ave  
Foley, AL 36535

Ms. Mary L. Cousar  
6 St Joseph St  
Mobile, AL 36602

Mr. Douglas Burtu Kearley  
Ten Wisteria Ave  
Mobile, AL 36607

Reverend Canon Beverly F. Gibson  
Christ Church Cathedral  
115 S Conception St  
Mobile, AL 36602

Col. Patrick Downing  
USS Alabama Commission  
Battleship Memorial Park  
PO Box 65  
Mobile, AL 36601-0065

Ms. Elizabeth Merritt  
National Trust for Historic Preservation  
2600 Virginia Ave NW  
STE 1100  
Washington, DC 20037

Mr. Chris Wilson  
Advisory Council on  
Historic Preservation  
401 F St NW, STE 308  
Washington, DC 20001-2631

The Honorable Dane Haygood  
Mayor of Daphne  
PO Box 400  
Daphne, AL 36526

Mr. Walter Meigs  
BAE Systems/Southeast  
Shipyards Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

Ms. Elizabeth S. Sanders  
Downtown Mobile Alliance  
PO Box 112  
Mobile, AL 36601

Mr. Herdon Inge  
PO Box 40188  
Mobile, AL 36640

Ms. Melanie Thornton  
Director  
Historic Mobile Preservation Society  
300 Oakleigh Place  
Mobile, AL 36604

Mr. Tilmon Brown  
President  
Restore Mobile, Inc.  
PO Box 40037  
Mobile, AL 36640

Mr. John Hildreth  
National Trust for Historic Preservation  
William Aiken House  
456 King St - 3rd Floor  
Charleston, SC 29403

The Honorable Sandy Stimpson  
Mayor of Mobile  
PO Box 1827  
Mobile, AL 36633-1827

The Honorable Michael McMillan  
Mayor of Spanish Fort  
PO Box 7226  
Spanish Fort, AL 36527

Mr. Ray Harris  
Signal Shipyard/Bender Shipbuilding  
& Repair Co.  
601 S Royal St  
Mobile, AL 36602

Ms. Caroline Etherton  
Colonial Dames and Conde-Charlotte  
Museum House  
PO Box 1968  
Mobile, AL 36633

Ms. Ann Bedsole  
6 St Joseph St  
Mobile, AL 36602

Ms. Katherine Frangos  
Friends of the Museum  
PO Box 2068  
Mobile, AL 36602

Ms. Wendy Crocker  
BAE Systems/Southeast  
Shipyards Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

**From:** [Betsy Merritt](#)  
**To:** [Bartlett, Mark \(FHWA\)](#); [Urquhart, Lynne \(FHWA\)](#); [acoffa@dot.state.al.us](mailto:acoffa@dot.state.al.us); [clayn@dot.state.al.us](mailto:clayn@dot.state.al.us); [adamsw@dot.state.al.us](mailto:adamsw@dot.state.al.us)  
**Cc:** [Chris Wilson; mnaber@achp.gov](mailto:mnaber@achp.gov); [Reid Nelson](#); [Charlene Vaughn](#); [Javier Marques](#); [Kelly Yasaitis Fanizzo](#); [Lindauer, Owen \(FHWA\)](#); [LeeAnne.Wofford@preserveala.org](mailto:LeeAnne.Wofford@preserveala.org); [Herndon Inge III](#)  
**Subject:** I-10 Mobile River Bridge and Bayway Widening project  
**Date:** Wednesday, March 02, 2016 3:27:38 PM  
**Attachments:** [DRAFT MOA for I-10 Mobile River Bridge 2016-03-01.pdf](#)

---

Yesterday we received, via US Postal Service, the attached letter from the Alabama DOT regarding the proposed I-10 Mobile River Bridge project.

The letter encloses a Draft MOA and asks that we provide written comments on the draft agreement within 24 days (by March 25).

The statements in the cover letter suggest that the Consulting Parties under Section 106 have somehow been involved in the development of this draft MOA. Nothing could be further from the truth.

Indeed, this is the first communication of any kind that we have received from any of the transportation agencies in TEN MONTHS.

The most recent communication was a letter from the FHWA, which we received on July 6, 2015 via the US Postal Service, acknowledging the adverse effects of the proposed project. That letter stated, “We now propose to begin discussing appropriate mitigation measures to resolve and/or minimize any adverse impacts.” If that discussion has been underway at all during the past ten months, the consulting parties have not been notified or invited to participate.

We strongly object to the development of this MOA without consultation, and we object to the request that the consulting parties submit written comments in well under 30 days, again without any consultation. Consultation is supposed to involve “the process of seeking, discussing, and considering the views of other participants, and, where feasible, seeking agreement with them regarding matters arising in the section 106 process.” 36 C.F.R. 800.16(f). That is clearly not occurring here.

We ask that the FHWA initiate actual “consultation” regarding this proposed project, and that comments on the Draft MOA be deferred until after that consultation is underway.

Sincerely,  
Elizabeth Merritt

**Elizabeth Sherrill Merritt | DEPUTY GENERAL COUNSEL**  
(202) 297-4133 (Mobile)  
(202) 588-6035 (Law Dep't)

**NATIONAL TRUST FOR HISTORIC PRESERVATION**  
The Watergate Office Building  
2600 Virginia Avenue NW Suite 1100  
Washington, DC 20037  
**Note my new email address:**

**From:** [Herndon Inge III](#)  
**To:** [Bartlett, Mark \(FHWA\)](#); [Urquhart, Lynne \(FHWA\)](#); [acoffa@dot.state.al.us](mailto:acoffa@dot.state.al.us); [clayn@dot.state.al.us](mailto:clayn@dot.state.al.us); [adamsw@dot.state.al.us](mailto:adamsw@dot.state.al.us)  
**Cc:** ["Powell, Don"](#)  
**Subject:** Mobile River Crossing  
**Date:** Thursday, March 03, 2016 3:11:40 PM

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Sir,

Yesterday I received in my post office box, via US Postal Service, a letter from the Alabama DOT dated February 24, 2016, regarding the proposed I-10 Mobile River Bridge project. The letter encloses a Draft Memorandum of Agreement and asks that I provide written comments on the draft agreement within 24 days (by March 25).

The statements in the cover letter suggest that the Consulting Parties under Section 106 have somehow been involved in the development of this draft MOA. Nothing could be further from the truth. Indeed, this is the first communication of any kind that I have received from any of the transportation agencies since your letter of June 25, 2015, acknowledging the adverse effects of the proposed project. That letter stated, "We now propose to begin discussing appropriate mitigation measures to resolve and/or minimize any adverse impacts." If that discussion has been underway at all during the past ten months, I, as a consulting party, have not been notified, consulted or invited to participate, to "discuss".

I strongly object to the development of this MOA without consultation, and I object to the request that the consulting parties submit written comments in well under 30 days, again without any consultation. Consultation is supposed to involve "the process of seeking, discussing, and considering the views of other participants, and, where feasible, seeking agreement with them regarding matters arising in the section 106 process." 36 C.F.R. 800.16(f). That is clearly not occurring here.

I ask that the FHWA initiate actual "consultation" regarding this proposed project, and that comments on the Draft MOA be deferred until after that consultation is underway, according to your letter of June 25, 2015.

I had been available to "consult" and to "discuss" since June 25, 2015, but have not been contacted.

Herndon Inge  
Stop the bridge

**From:** [Bartlett, Mark \(FHWA\)](#)  
**To:** [Betsy Merritt](#)  
**Cc:** [Chris Wilson](#); [mnaber@achp.gov](mailto:mnaber@achp.gov); [Reid Nelson](#); [Charlene Vaughn](#); [Javier Marques](#); [Kelly Yasaitis Fanizzo](#); [Lindauer, Owen \(FHWA\)](#); [LeeAnne.Wofford@preserveala.org](mailto:LeeAnne.Wofford@preserveala.org); [Herndon Inge III](#); [Powell, Don \(powelldo@dot.state.al.us\)](mailto:Powell, Don (powelldo@dot.state.al.us)); [Calametti, Vince \(calamettiv@dot.state.al.us\)](mailto:Calametti, Vince (calamettiv@dot.state.al.us)); [Urquhart, Lynne \(FHWA\)](#); [clayn@dot.state.al.us](mailto:clayn@dot.state.al.us); [adamsw@dot.state.al.us](mailto:adamsw@dot.state.al.us)  
**Subject:** RE: I-10 Mobile River Bridge and Bayway Widening project  
**Date:** Tuesday, March 15, 2016 10:01:19 AM  
**Importance:** High

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Betsy,

As a follow up to our phone call today, let me thank you for your participation in this project. As I noted during our discussion, ALDOT has hired Thompson Engineering to conduct additional engineering analysis and preliminary design work on the interchanges and assist us in completing the 106 process. Our intention in sending out this draft MOA is to reinitiate the consultation process. The language of the MOA is based on the discussions of our meeting held with the consulting parties in Mobile in September of 2014. A follow up phone call or meeting will be held prior to finalizing the MOA. Your comments may be provided by letter or email. Should you need additional time to review, please let us know.

Thanks,

Mark Bartlett  
FHWA Alabama Division  
(334)-274-6350

---

**From:** Betsy Merritt [mailto:[emerritt@savingplaces.org](mailto:emerritt@savingplaces.org)]  
**Sent:** Wednesday, March 02, 2016 3:27 PM  
**To:** Bartlett, Mark (FHWA); Urquhart, Lynne (FHWA); [acoffa@dot.state.al.us](mailto:acoffa@dot.state.al.us); [clayn@dot.state.al.us](mailto:clayn@dot.state.al.us); [adamsw@dot.state.al.us](mailto:adamsw@dot.state.al.us)  
**Cc:** Chris Wilson; [mnaber@achp.gov](mailto:mnaber@achp.gov); Reid Nelson; Charlene Vaughn; Javier Marques; Kelly Yasaitis Fanizzo; Lindauer, Owen (FHWA); [LeeAnne.Wofford@preserveala.org](mailto:LeeAnne.Wofford@preserveala.org); Herndon Inge III  
**Subject:** I-10 Mobile River Bridge and Bayway Widening project

Yesterday we received, via US Postal Service, the attached letter from the Alabama DOT regarding the proposed I-10 Mobile River Bridge project. The letter encloses a Draft MOA and asks that we provide written comments on the draft agreement within 24 days (by March 25).

The statements in the cover letter suggest that the Consulting Parties under Section 106 have somehow been involved in the development of this draft MOA. Nothing could be further from the truth.

Indeed, this is the first communication of any kind that we have received from any of the transportation agencies in TEN MONTHS.

The most recent communication was a letter from the FHWA, which we received on July 6, 2015 via the US Postal Service, acknowledging the adverse effects of the proposed project. That letter stated,

“We now propose to begin discussing appropriate mitigation measures to resolve and/or minimize any adverse impacts.” If that discussion has been underway at all during the past ten months, the consulting parties have not been notified or invited to participate.

We strongly object to the development of this MOA without consultation, and we object to the request that the consulting parties submit written comments in well under 30 days, again without any consultation. Consultation is supposed to involve “the process of seeking, discussing, and considering the views of other participants, and, where feasible, seeking agreement with them regarding matters arising in the section 106 process.” 36 C.F.R. 800.16(f). That is clearly not occurring here.

We ask that the FHWA initiate actual “consultation” regarding this proposed project, and that comments on the Draft MOA be deferred until after that consultation is underway.

Sincerely,  
Elizabeth Merritt

**Elizabeth Sherrill Merritt | DEPUTY GENERAL COUNSEL**

(202) 297-4133 (Mobile)  
(202) 588-6035 (Law Dep't)

**NATIONAL TRUST FOR HISTORIC PRESERVATION**

The Watergate Office Building  
2600 Virginia Avenue NW Suite 1100  
Washington, DC 20037

Note my new email address:  
[emerritt@savingplaces.org](mailto:emerritt@savingplaces.org)

[www.PreservationNation.org](http://www.PreservationNation.org)







Mobile Historic Development Commission

P.O. Box 1827  
Mobile, AL 36633-1827



21 March 2016

Natasha Clay  
Alabama Department of Transportation  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, Alabama 36110

Dear Ms. Clay:

I contact you with regard to the Draft Memorandum of Agreement for ALDOT Project DPI-0030(005). A letter dated 24 February 2016 contained the aforementioned document therein. The letter of 24 February 2016 requested receipt of comments by 25 March 2016.

The Draft Memorandum of Agreement for ALDOT Project DPI-0030(005) concerns the construction of an Interstate 10 Bridge that will span the Mobile River. This important project will impact Mobile for generations to come. The location, construction, and design of the bridge impact multiple National Register listed and locally designated historic districts. The buildings therein are a testament to Mobile's past and a vehicle for her future. The Draft Memorandum of Agreement demonstrates an awareness of the archaeological, architectural, historical, and visual impacts for Mobile's historic districts

The Staff of the Mobile Historic Development Commission thanks you for the provision of Draft Memorandum of Agreement. We look forward to working with you to achieve a bridge that successfully enhances the appearance and experience of the historical landscape. If questions should arise or assistance is required, do not hesitate to contact me.

Sincerely & Respectfully,

*Cartledge W. Blackwell, III*

Cartledge W. Blackwell, III  
City of Mobile/Mobile Historic Development Commission  
Assistant Director  
[cartledge.blackwell@cityofmobile.org](mailto:cartledge.blackwell@cityofmobile.org)  
251.208.7998



## Restore Mobile

P.O. Box 40037  
Mobile, AL 36640

March 21, 2016

Ms. Natasha Clay  
Alabama Department of Transportation  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, AL 36110


Dear Ms. Clay:

The Executive Committee met and discussed the letter from ALDOT dated February 24, 2016 concerning the draft Memorandum of Agreement. The Committee noted several things listed below.

1. **Lighting:** "to meet current design criteria". In the public meeting it was discussed that the lighting design would be undertaken as late as possible to incorporate the newest technology available. This included discussion that the fixtures would not be specified until just prior to needing them for installation.
2. **Visual:** "reasonable planting plan will be developed in an effort to *maintain* the tree canopy." In the public meetings, the request was to maintain and improve the current tree canopy to soften the visual effect of the bridge on the historic resources with a sustainable and implemented landscape plan. Major concern was expressed about what occurs beneath the bridge. We request that a commitment be made to insure that the land be utilized for the benefit of Mobilians. Ideas to consider are a park, riverfront access, walking trail or other amenities to mitigate the quality of life impact for those living and working near the structure.
3. **Vibrations:** Unfortunately, we do not have the expertise to comment on this. The concern is that construction vibrations will be monitored and mitigated to protect historic resources.
4. **Bridge Aesthetics:** There does not appear to be any public input into the overall design of the bridge. The letter reads as though the first comment period available from the public will be at the 30% review. At that point it would seem the design would have been settled and the general concept decided. During the public meetings it was stated that the design of the bridge would not necessarily be that shown in the artist renderings. If that was correct, when and how will the concept be determined? It should also be noted that according to the letter the consulting process is to "insure an attractive yet functional and economical bridge". As a historic organization we believe the design must also be "appropriate".

There were several areas in the letter that we do not have the expertise to comment on such as archaeology, vibrations and the implementation and oversight of the plan. We trust that others reviewing this document will have appropriate input. Thank you.

Sincerely,

  
Devereaux Bemis  
Secretary







P.O. Box 65 • Mobile, Alabama 36601  
(251) 433-2703 • FAX: (251) 433-2777  
www.ussalabama.com



March 21, 2016

Ms. Natasha Clay  
Alabama Department of Transportation  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, Alabama 36110

Re: Draft *Memorandum of Agreement* for ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening EIS  
Mobile and Baldwin Counties, Alabama

Dear Ms. Clay:

Thank you for your letter and request for comments in reference to the proposed construction of a new 6-lane bridge across the Mobile River and Widening the existing I-10 bridges across Mobile Bay from 4-lanes to 8-lanes in Mobile and Baldwin Counties.

The USS Alabama Battleship Commission first addressed the proposed I-10 Mobile River Bridge with your office on April 11, 2003. In that letter, and since that time, the Commission has voiced its opposition to the proposed construction as designed to a variety of state and federal officials. The design is somewhat different in 2016, of course; however, our concerns and objections remain constant.

We are a self-sustaining Memorial Park which opened to the public on January 9, 1965. Under §41-9-348, Code of Alabama (1975), the USS Alabama Battleship Commission is a state agency and has exclusive control over the Battleship USS Alabama, the memorial park, as well as improvements, exhibits and additions. However, we have never received any public funding for daily operations. Our fiscal responsibility is to maintain and display our two National Historic Landmarks (USS Alabama, submarine USS Drum) as efficiently as possible.

The USS Alabama Battleship Commission believes the I-10 Mobile River Bridge in its present configuration would have adverse consequences on our operations. The high-speed bypass would possible take much-needed Park income and tax revenues currently generated out of Mobile, and seriously hampers any future operational revenue enhancement for our self-

Ms. Natasha Clay  
ALDOT Environmental Technical Section  
March 21, 2016  
Page 2

supporting park. Mitigation to protect Battleship Memorial Park from the resulting economic harm from would certainly include an exit at or close to the Park's entrance.

Mr. Vincent Calametti, our Regional Engineer for the Alabama Department of Transportation, and Jimmy Shumock, President and CEO of Thompson Engineering here in Mobile, provided an informative update to the Commission at its March 11, 2016 meeting. Mr. Calametti answered questions about the project and mentioned items that dealt with specific signage on all routes that would enhance travelers' awareness of Battleship Memorial Park, which was positive.

The draft *Memorandum of Agreement* Stipulation Section I.C. addresses Historic Battleship Park and a proposed signage plan. Input will be provided by SHPO, the USS Alabama Battleship Commission (Memorial Park), and other §106 Consulting Parties. This is also seen as a positive development; however, the Commission remains in opposition to the proposed construction.

The Battleship Commission appreciates being part of the planning process, and for the opportunity to make our concerns known reference Project DPI-0030(005). We look forward to continuing dialog on this topic.

Sincerely,



Patrick H. Downing (USA-Ret.)  
Chairman  
USS Alabama Battleship Commission

## Patterson, Pat M

---

**From:** McBride, Amanda <Amanda.McBride@ahc.alabama.gov>  
**Sent:** Tuesday, May 03, 2016 3:08 PM  
**To:** Patterson, Pat M  
**Cc:** Hathorn, Stacye; Wofford, Lee Anne  
**Subject:** comments on I-10 Bridge MOA

Pat,

We have finished our review of the MOA for the I-10 bridge and offer the following comments:

1. There is no mention of archaeology to be completed at the project terminus {REDACTED}
2. Attachment 5, Under "Post-Review Discovery Plan, Section B: "When notified by an **applicant**..." You may want to replace the word "applicant" with something else. The word "applicant" also appears in the same attachment under Section E "...ALDOT and the **applicant** may participate..." Also in Appendix 5, Section B.2: "The applicant shall inform FHWA." Finally, you may want to remove the wording regarding tree root balls in Appendix 5, Section B: "...including archaeological deposits found in tree root balls...". It is assumed that these instances are simply copy and paste errors.
3. The final draft should include the ACHP's Human Remains Policy as well as the Alabama Burial Act as appendices.

That's all we have!

Amanda

Amanda McBride  
Environmental Review Coordinator  
Historic Preservation Division  
Alabama Historical Commission  
468 South Perry Street  
Montgomery, AL  
36130-0900 (US Post)  
36104 (Courier)  
334.230.2692  
Amanda.McBride@preserveala.org

Celebrate 50 Years of Keeping Our Heritage Our Future



Alabama Historical Commission

**DISPOSITION OF COMMENTS RECEIVED ON DRAFT SECTION 106 MEMORANDUM OF AGREEMENT**

**Mobile Historic Development Commission**

	<b>Comment</b>	<b>Response</b>
1.	The Draft Memorandum of Agreement demonstrates an awareness of the archaeological, architectural, historical, and visual impacts for Mobile's historic districts.	Comment noted. No revisions to MOA required.

**Restore Mobile**

	<b>Comment</b>	<b>Response</b>
1.	Lighting: "to meet current design criteria." In the public meeting, it was discussed that the lighting design would be undertaken as late as possible to incorporate the newest technology available. This included discussion that the fixtures would not be specified until just prior to needing them for installation.	<p>ALDOT is committed to designing roadway and bridge lighting necessary to meet current design criteria, while minimizing light pollution to the extent that is practical for the traveling public and its safety.</p> <p>Measures to minimize light pollution on historic resources will be developed with input from the SHPO and Section 106 Consulting Parties. Through the Section 106 process, review and coordination meetings will be held as necessary to gather input at appropriate times during the design process and/or during the design review period. In order to incorporate the newest technology available at the time of construction, lighting fixtures will not be specified until later in the design process.</p>
2.	Visual: "reasonable planting plan will be developed in an effort to <i>maintain</i> tree canopy." In the public meetings, the request was to maintain and improve the current tree canopy to soften the visual effect of the bridge on the historic resources with a sustainable and implemented landscape plan. Major concern was expressed about what occurs beneath the bridge. We request that a	<p>A landscape and management plan for areas within ALDOT's right-of-way, including areas beneath the bridge, will be developed and implemented. The Aesthetic Steering Committee that has been developed by ALDOT will provide input into this plan.</p>

<p>commitment be made to insure that the land be utilized for the benefit of Mobilians. Ideas to consider are a park, riverfront access, walking trail or other amenities to mitigate the quality of life impact for those living and working near the structure.</p>	<p>Eight of the nine members comprising the Aesthetic Steering Committee are Section 106 Consulting Parties. The Aesthetic Steering Committee has developed draft Aesthetic Guidelines for the project to direct the aesthetic design of the project. As the design process develops, the Aesthetic Steering Committee will be asked to provide input on design components which will include, but not be limited to, lighting technology, lighting fixtures, landscaping, and the overall Aesthetic Guidelines for the project.</p> <p>In addition, ALDOT understands the importance of maintaining and improving the tree canopy within and around downtown Mobile to minimize visual impacts of the new I-10 Mobile River Bridge. ALDOT has partnered with the City of Mobile in the <i>Right Tree, Right Place</i> program. This program places appropriate trees and landscaping throughout the City of Mobile. The <i>Right Tree, Right Place</i> Committee will work with ALDOT to comply with any municipal ordinances or regulations related to landscaping requirements. ALDOT has committed to contribute \$50,000 to the <i>Right Tree, Right Place</i> program as mitigation for adverse visual effects resulting from the I-10 Mobile River Bridge and Bayway Project. The City of Mobile will be responsible for administering this money in accordance with the principles and objectives of the <i>Right Tree, Right Place</i> program to maintain and improve the City's tree canopy.</p>
<p>3. Vibrations: We do not have the expertise to comment on this. The concern is that construction vibrations will be monitored and mitigated to protect historic resources.</p>	<p>The alignment of Alternative B' has been shifted further away from downtown historic resources, thus further reducing potential vibrations. Additional studies related to vibrations have been conducted and will be discussed during the upcoming Section</p>



		<p>106 consultation meeting. ALDOT has committed to monitoring vibrations at historic resources during construction and mitigating impacts that may occur. More details are included in the Draft MOA.</p> <p>An Aesthetic Steering Committee has been developed by ALDOT. Eight of the nine members comprising the Aesthetic Steering Committee are Section 106 Consulting Parties. The Aesthetic Steering Committee has developed draft Aesthetic Guidelines for the project to guide the aesthetic design of the project. As the design process develops, the Aesthetic Steering Committee will be asked to provide input on design components which will include, but not be limited to, lighting technology, lighting fixtures, landscaping, and the overall Aesthetic Guidelines for the project.</p>
4.	<p>Bridge Aesthetics: There does not appear to be any public input into the overall design of the bridge. The letter reads as though the first comment period available from the public will be at the 30% review. At that point, it would seem the design would have been settled and the general concept decided. During the public meetings it was stated that the design of the bridge would not necessarily be that shown in the artist renderings. If that was correct, when and how will the concept be determined? It should also be noted that according to the letter the consulting process is to insure an attractive yet functional and economical bridge.” As a historic organization we believe the design must also be “appropriate.”</p>	

**USS Alabama Battleship Commission**

	<b>Comment</b>	<b>Response</b>
1.	<p>The Commission has voiced its opposition to the proposed construction as designed to a variety of state and federal officials.</p>	<p>Comment noted.</p>
2.	<p>The USS ALABAMA Battleship Commission believes the I-10 Mobile River Bridge in its present configuration would have adverse consequences on our operations. The high-speed bypass would possibly take much-needed Park income and tax revenues currently generated out of Mobile, and seriously hampers any future operational revenue enhancement for our self-supporting park. Mitigation to protect Battleship Memorial Park from the resulting economic harm would certainly include an exit at or close to the Park’s entrance.</p>	<p>A coordination meeting was held with the Commission on April 21, 2017 to discuss the Commission’s concerns about access to the Park and potential impacts that could occur as part of this project. ALDOT evaluated several options to provide more direct access to the Park. Concepts providing direct access to the Park via a new ramp or relocating the Park’s entrance could not meet design criteria for safe roadway conditions; therefore, they were not advanced for further consideration. An exit at or near the Park’s entrance has been evaluated and determined not to be feasible due to safety concerns.</p>



Comment	Response
<p>3. Mr. Vincent Calametti, our Regional Engineer for the Alabama Department of Transportation, and Jimmy Shumock, President and CEO of Thompson Engineering here in Mobile, provided an informative update to the Commission at its March 11, 2016 meeting. Mr. Calametti answered questions about the project and mentioned items that dealt with specific signage on all routes that would enhance travelers' awareness of Battleship Memorial park, which was positive.</p>	<p>Existing access to the Park from I-10 will not be altered in the final condition of this project.</p> <p>In order to improve signage directing travelers to the Park, ALDOT has developed a preliminary signage plan for the Park including proposed locations and types of signs. The plan was developed with input from the SHPO and the Commission. New signs are proposed to supplement the existing signs along the I-10 corridor. The signs will direct travelers from I-10 to the Park. ALDOT will coordinate with the Commission to finalize the signage plan prior to construction. The draft signage plan was shared with the Commission during the April 21, 2017 meeting.</p>
<p>4. The Draft Memorandum of Agreement Stipulation Section I.C. address Historic Battleship Park and a proposed signage plan. Input will be provided by SHPO, the USS Alabama Battleship Commission (Memorial Park), and other 106 Consulting Parties. This is also seen as a positive development; however, the Commission remains in opposition to the proposed construction.</p>	<p>Comment noted.</p>

**Alabama Historical Commission**

Comment	Response
<p>1. There is no mention of archaeology to be completed at the project terminus in Baldwin County (the D'Olive site, a shell midden).</p>	<p>The proposed project would not impact the shell midden site, and impacts to the site will continue to be avoided; therefore, no further archaeological investigations are required. Language has been updated in the Draft MOA.</p>
<p>2. Attachment 5, Under "Post-Review Discovery Plan, Section B: "When notified by an applicant..." You may want to replace the word "applicant" with something else. The word "applicant" also appears in the same attachment under Section E " ...ALDOT and the applicant may participate..." Also in Appendix 5, Section B.2: "The applicant shall inform FHWA." Finally, you may want to remove the wording regarding tree root balls in Appendix 5, Section B: " ...including</p>	<p>Draft MOA has been revised per comments.</p>

	Comment	Response
	archaeological deposits found in tree root balls..." It is assumed that these instances are simply copy and paste errors.	
3.	The final draft should include the ACHP's Human Remains Policy as well as the Alabama Burial Act as appendices.	Draft MOA has been revised per comments.

**National Trust for Historic Preservation**

	Comment	Response
1.	<p>Yesterday we received, via US Postal Service, the attached letter from the Alabama DOT regarding the proposed I-10 Mobile River Bridge project. The letter encloses a Draft MOA and asks that we provide written comments on the draft agreement within 24 days (by March 25). The statements in the cover letter suggest that the Consulting Parties under Section 106 have somehow been involved in the development of this draft MOA. Nothing could be further from the truth. Indeed, this is the first communication of any kind that we have received from any of the transportation agencies in TEN MONTHS.</p> <p>The most recent communication was a letter from the FHWA, which we received on July 6, 2015 via the US Postal Service, acknowledging the adverse effects of the proposed project. That letter stated, "We now propose to begin discussing appropriate mitigation measures to resolve and/or minimize any adverse impacts." If that discussion has been underway at all during the past ten months, the consulting parties have not been notified or invited to participate.</p>	<p>ALDOT has hired Thompson Engineering to conduct additional engineering analysis and preliminary design work on the interchanges and assist us in completing the 106 process. Our intention in sending out this draft MOA is to reinitiate the consultation process. The language of the MOA is based on the discussions of our meeting held with the consulting parties in Mobile in September of 2014. A follow up phone call or meeting will be held prior to finalizing the MOA. Your comments may be provided by letter or email. Should you need additional time to review, please let us know.</p> <p>FHWA extended the comment period, and no further comments were received.</p>
2.	<p>We strongly object to the development of this MOA without consultation, and we object to the request that the consulting parties submit written comments in well under 30 days, again without any consultation. Consultation is supposed to involve "the process of seeking, discussing, and considering the views of other participants, and, where feasible, seeking agreement with them regarding matters arising in the section 106 process." 36 C.F.R. 800.16(f). That is clearly not occurring here.</p>	

	Comment	Response
3.	We ask that the FHWA initiate actual “consultation” regarding this proposed project, and that comments on the Draft MOA be deferred until after that consultation is underway.	

**Herndon Inge**

	Comment	Response
1.	Yesterday I received in my post office box, via US Postal Service, a letter from the Alabama DOT dated February 24, 2016, regarding the proposed I-10 Mobile River Bridge project. The letter encloses a Draft Memorandum of Agreement and asks that I provide written comments on the draft agreement within 24 days (by March 25). The statements in the cover letter suggest that the Consulting Parties under Section 106 have somehow been involved in the development of this draft MOA. Nothing could be further from the truth. Indeed, this is the first communication of any kind that I have received from any of the transportation agencies since your letter of June 25, 2015, acknowledging the adverse effects of the proposed project. That letter stated, “We now propose to begin discussing appropriate mitigation measures to resolve and/or minimize any adverse impacts.” If that discussion has been underway at all during the past ten months, I, as a consulting party, have not been notified, consulted or invited to participate, to “discuss.”	ALDOT has hired Thompson Engineering to conduct additional engineering analysis and preliminary design work on the interchanges and assist us in completing the 106 process. Our intention in sending out this draft MOA is to reinitiate the consultation process. The language of the MOA is based on the discussions of our meeting held with the consulting parties in Mobile in September of 2014. A follow up phone call or meeting will be held prior to finalizing the MOA. Your comments may be provided by letter or email. Should you need additional time to review, please let us know.
2.	I strongly object to the development of this MOA without consultation, and I object to the request that the consulting parties submit written comments in well under 30 days, again without any consultation. Consultation is supposed to involve “the process of seeking, discussing, and considering the views of other participants, and, where feasible, seeking agreement with them regarding matters arising in the section 106 process.” 36 C.F.R. 800.16(f). That is clearly not occurring here.	FHWA extended the comment period, and no further comments were received.
3.	I ask that the FHWA initiate actual “consultation” regarding this proposed project, and that comments on the Draft MOA be deferred until after that consultation is underway, according to your letter of June 25, 2015. I had been available to “consult” and to “discuss” since June 25, 2015, but have not been contacted.	

# **APPENDIX L-6:**

**May 8, 2018**

**Section 106 Consulting Party Meeting**

**May 8, 2018 Section 106 Consulting Party Meeting**



**ALABAMA DEPARTMENT OF TRANSPORTATION**

1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050



**Kay Ivey**  
Governor

**John R. Cooper**  
Transportation Director

April 6, 2018

«Title» «First\_Name» «Last\_Name»  
«Company\_Name»  
«Address\_Line\_1»  
«Address\_Line\_2»  
«City», «State» «ZIP\_Code»

**RE:** Section 106 Consulting Parties Consultation Meeting  
ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening EIS  
Mobile and Baldwin Counties

Dear Sir or Madam:

As you are aware, the Draft Environmental Impact Statement (DEIS) for the I-10 Mobile River Bridge and Bayway Widening was approved by the Federal Highway Administration (FHWA) on July 22, 2014. As a Consulting Party, you were provided a distribution copy of the approved DEIS. By letter dated February 24, 2016, Consulting Parties were provided a copy of the Draft Memorandum of Agreement (MOA) for the above-referenced project for review and comment. Comments received from the Consulting Parties were transmitted to the Consulting Parties by letter dated July 27, 2016.

Per 36 CFR 800.6(b)(2), ALDOT and FHWA shall continue to consult with the State Historic Preservation Office (SHPO), the Advisory Council on Historic Preservation (Council), and other Consulting Parties to seek ways to avoid, minimize, or mitigate the possible adverse effects of out project on historic properties.

The purpose of this letter is to invite you to participate in a Section 106 Consulting Parties Consultation Meeting. The meeting will be held on May 8, 2018 from 9:00 a.m. – 12:00 p.m. The location is:

Alabama Department of Transportation  
Southwest Region  
1701 I-65 West Service Road North  
Building T  
Mobile, AL 36618

Page 2  
April 6, 2018

The consultation meeting will focus on three areas. First, we will begin by giving an overview of the project and a discussion of changes that have occurred in the project and present additional information that has been developed since the DEIS was signed. Second, we will discuss the topics that are included as stipulations in the Draft MOA. Third, we will conclude the meeting by identifying the next steps in the consultation process.

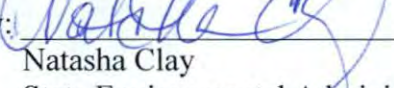
A CD containing the following information is enclosed:

1. Cultural Resources reports and SHPO correspondence since the 2014 DEIS,
2. Disposition of comments received on the Draft MOA from Section 106 Consulting Parties,
3. Latest version of the Draft MOA, and
4. Archaeological Fieldwork Status Update as of April 4, 2018

Please feel free to contact Ms. Natasha Clay at (334) 242-6315 if you should have any questions or comments regarding this meeting. We look forward to your participation in the Section 106 Consulting Parties consultation meeting on May 8, 2018. Thank you for your continued cooperation and interest in this project.

Sincerely,

Steven E. Walker, P.E.  
State Design Engineer

By:   
Natasha Clay  
State Environmental Administrator

NC/PP/mem  
Attachments  
cc: FHWA  
Andrew Wood, ALDOT SW Region  
Missi Shumer  
Greg Lowe  
ETS Cultural Resources File



Mr. John Sledge  
Mobile-Historic Development  
Commission  
PO Box 1827  
Mobile, AL 36633-1827

Ms. Lee Anne Wofford  
Alabama Historical Commission  
468 S Perry St  
Montgomery, AL 36130

The Honorable Dane Haygood  
Mayor of Daphne  
PO Box 400  
Daphne, AL 36526

Mr. Walter Meigs  
BAE Systems/Southeast Shipyards  
Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

Ms. Elizabeth Stevens  
Downtown Mobile Alliance  
PO Box 112  
Mobile, AL 36601

Mr. Douglas Burtu Kearley  
Ten Wisteria Ave  
Mobile, AL 36607

Ms. Carolyn Jeffers  
Christ Church Cathedral  
115 S Conception St  
Mobile, AL 36602

Major General Janet Cobb  
USS ALABAMA Battleship Memorial  
Park  
PO Box 65  
Mobile, AL 36601-0065

Ms. Amanda McBride  
Alabama Historical Commission  
468 S Perry St  
Montgomery, AL 36130

Ms. Elizabeth Merritt  
National Trust for Historic Preservation  
2600 Virginia Ave NW  
Suite 1100  
Washington, DC 20037

The Honorable Sandy Stimpson  
Mayor of Mobile  
PO Box 1827  
Mobile, AL 36633-1827

The Honorable Michael McMillan  
Mayor of Spanish Fort  
PO Box 7226  
Spanish Fort, AL 36527

Ms. Wendy Crocker  
BAE Systems/Southeast Shipyards  
Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

Ms. Elizabeth Harris  
Colonial Dames and Conde-Charlotte  
Museum House  
57 Oakland Ave  
Mobile, AL 36608

Mr. Herndon Inge  
PO Box 40188  
Mobile, AL 36640

Historic Mobile Preservation Society  
300 Oakleigh Place  
Mobile, AL 36604

Mr. Tilmon Brown  
Restore Mobile  
PO Box 40037  
Mobile, AL 36640

Mr. David S. Clarke  
US Department of Transportation  
HEPE, Rm. E.76  
1200 New Jersey Avenue, SE  
Washington, DC 20590

Mr. John Hildreth  
National Trust for Historic Preservation  
William Aiken House  
456 King St - 3rd Floor  
Charleston, SC 29403

Commissioner Connie Hudson  
President  
Mobile County Commission  
205 Government St  
Mobile, AL 36644-1001

Commissioner Chris Elliot  
Baldwin County Commission  
1100 Fairhope Ave  
Fairhope, AL 36532

Ms. Mary Cousar  
6 St Joseph St  
Mobile, AL 36602

Mr. Ray Harris  
Signal Shipyard/Bender Shipbuilding &  
Repair Co  
601 S Royal St  
Mobile, AL 36602

Ms. Ann Bedsole  
6 St Joseph St  
Mobile, AL 36602

Ms. Katherine Frangos  
Friends of the Museum  
PO Box 2068  
Mobile, AL 36602

Ms. Sarah Stokely  
Advisory Council on Historic Preservation  
401 F Street NW  
Suite 308  
Washington, DC 20001-2637



# Mobile River Bridge and Bayway Project

## Section 106 Consulting Party Meeting

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Date: May 8, 2018  
Time: 9:00 a.m. – 12:00 p.m.  
Location: ALDOT, Southwest Region, Building T, 1701 I-65 West Service Road North, Mobile, Alabama 36618

### **MEETING SUMMARY**

#### **I. Welcome/Introductions**

ALDOT opened the meeting by welcoming everyone and thanking the Consulting Parties for participating in the meeting. Each attendee stated their name and who they represented. ALDOT introduced the new project team that had been put in place since the last Section 106 Consultation meeting.

Mayor Sandy Stimpson spoke for several minutes about the importance of the project and the need to provide additional capacity on I-10 in Mobile and Baldwin Counties to prevent Mobile from being one of the worst bottlenecks on the I-10 corridor. He stated that the project has the City's full support.

#### **II. Project Presentation**

The purpose of the meeting was to provide an update on the project and re-initiate Section 106 consultation. The project team gave a presentation, a copy of which is attached to this meeting summary. The presentation covered the following items:

- Purpose and need
- Project scope and limits
- Project history
- Status of NEPA documentation
- Summary of previous Section 106 consultation activities
- Activities since the July 2014 Draft Environmental Impact Statement was signed
  - Additional studies (geotechnical, storm surge, environmental, etc.)
  - Refinements to Alternative B' (including shift in the alignment further to the east away from downtown Mobile) and more detailed interchange concepts
- Change from traditional design-bid-build process to an alternative delivery method

Questions were taken throughout the presentation, as noted in the attached meeting transcript.

After the first part of the presentation, a video animation showing what the proposed project may look like was shown.

The second part of the meeting centered around the Section 106 Draft Memorandum of Agreement (MOA). The presentation covered specific topics of discussion and updates to the Draft MOA that had been made to address comments received from the Consulting Parties.

These topics of discussion included:

- Vibration study updates
- Aesthetics
- Lighting
- Landscaping
- Tree Canopy
- USS ALABAMA Battleship Memorial Park
- Historic Structures
- Archaeology

Questions and discussion occurred throughout the presentation and are included in the attached transcript.

The next steps in the project process, including Section 106 Consultation activities, were discussed. Section 106 Consulting Parties were asked to submit comments on the Draft MOA by June 8, 2018. ALDOT noted that the Draft MOA would be revised to address comments received, as appropriate, and would be circulated to the Consulting Parties. The Draft MOA would also be included in the Supplemental Draft Environmental Impact Statement for the project, with a Final MOA required for the Final Environmental Impact Statement/Record of Decision.

### **III. Section 106 Consulting Party Discussion**

Consulting Parties were asked to sign up to speak so that their comments could be recorded by the court reporter. Herndon Inge (Stop the Bridge) and Devereaux Bemis (Restore Mobile) were the only individuals who signed up to speak. A copy of the transcript is included as an attachment to this meeting summary.

### **IV. Closing/Adjourn**

ALDOT stated that additional comments and questions should be sent to Natasha Clay at ALDOT ([clayn@dot.state.al.us](mailto:clayn@dot.state.al.us)). ALDOT thanked everyone for attending the meeting.

### **Attachments:**

- 1) List of Attendees
- 2) PowerPoint Presentation (omitted from Supplemental DEIS - available upon request to ALDOT)
- 3) Meeting Transcript (omitted from Supplemental DEIS - available upon request to ALDOT)

**List of Attendees (in alphabetical order by first name)**

The following individuals attended the meeting on May 8, 2018:

<b>Name</b>	<b>Organization</b>
Aaron Moses	ALDOT
Allison Gregg	Consultant – ALDOT Procurement Advisory Team
Andrew Wood	ALDOT
Bill Turner	ALDOT
Beverly Gibson	Christ Church Cathedral
Bonnie Gums	University of South Alabama
Brian Ingram	ALDOT
Brian Underwood	City of Mobile
Commissioner Connie Hudson	Mobile County Commission
Devereaux Bemis	Restore Mobile
Dolha Kayisavera	ALDOT
Edwin Perry	ALDOT
Elizabeth Harris	Colonial Dames and Condé-Charlotte Museum House
Elizabeth Merritt (by phone)	National Trust for Historic Preservation
Elizabeth Stevens	Downtown Mobile Alliance
Geri Robinson	U.S. Coast Guard
Greg Lowe	Consultant – ALDOT Procurement Advisory Team
Herndon Inge	Stop the Bridge
The Honorable Sandy Stimpson	Mayor, City of Mobile
Jimmy Shumock	Consultant – ALDOT Procurement Advisory Team
John Sledge	Mobile Historic Development Commission
Julia Wilcox	Condé-Charlotte Museum House
Leah Stimpson	Condé-Charlotte Museum House
Louise Rayford	Condé-Charlotte Museum House
Lynne Urquhart	Federal Highway Administration
Major General Janet Cobb	USS ALABAMA Battleship Memorial Park
Mark Bartlett	Federal Highway Administration
Mary Lee Montgomery	Condé-Charlotte Museum House
Matt Ericksen	ALDOT
Missi Shumer	Consultant – ALDOT Procurement Advisory Team
Natasha Clay	ALDOT
Nick Amberger	City of Mobile
Pat Hickox	Consultant – ALDOT Procurement Advisory Team
Pat Patterson	ALDOT
Sarah Stokley (by phone)	Advisory Council on Historic Preservation
Stan Biddick	ALDOT
Stephanie Dragotta	ALDOT
Steve Walker	ALDOT
Tim Heisler	Federal Highway Administration
Vince Calametti	ALDOT

# I-10 Mobile River Bridge and Bayway Project Section 106 Consulting Party Meeting

## Meeting Agenda

**Date** May 8, 2018  
**Time** 9:00 a.m. – 12:00 p.m.  
**Location** ALDOT, Southwest Region  
 Building T  
 1701 I-65 West Service Road North  
 Mobile, Alabama 36618

TIME	AGENDA ITEM	LEAD
9:00 a.m.	<b>Welcome</b> <ul style="list-style-type: none"> <li>- New Project Team</li> <li>- Meeting Format/Purpose</li> <li>- Introductions</li> </ul>	Mark Bartlett Matt Ericksen Allison Gregg Allison Gregg
	<b>Project Overview</b> <ul style="list-style-type: none"> <li>- Purpose and Need</li> <li>- Project Scope &amp; Limits</li> <li>- Project History</li> <li>- Status of NEPA Documentation</li> <li>- Previous Section 106 Consultation Activities</li> </ul>	Missi Shumer
	<b>Activities since July 2014 DEIS</b> <ul style="list-style-type: none"> <li>- Additional Studies</li> <li>- Refinements to Alternative B'</li> <li>- Alternative Delivery Method</li> </ul>	Pat Hickox Pat Hickox Allison Gregg
	<b>Video</b>	
10:15-10:30 a.m.	<b>Break</b>	
	<b>Topics of Discussion</b> <ul style="list-style-type: none"> <li>- Vibration Study Updates</li> <li>- Aesthetics</li> <li>- Lighting</li> <li>- Landscaping</li> <li>- Tree Canopy</li> <li>- USS ALABAMA Battleship Park</li> <li>- Historic Structures</li> <li>- Archaeology</li> </ul>	Pat Hickox Pat Hickox Pat Hickox Pat Hickox Brian Underwood Missi Shumer Missi Shumer Missi Shumer
	<b>Next Steps</b> <ul style="list-style-type: none"> <li>- Anticipated Project Milestones</li> <li>- Future Section 106 Activities</li> </ul>	Matt Ericksen
	<b>Section 106 Consulting Party Discussion</b>	Allison Gregg
	<b>Closing/Adjourn</b>	Allison Gregg

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**SECTION 106**  
**CONSULTING PARTIES CONSULTATION MEETING**

**Project DPI-0030(005)**  
**I-10 Mobile River Bridge and Bayway**  
**Mobile and Baldwin Counties, Alabama**

**Held at the**  
**Alabama Department of Transportation**  
**1701 West I-65 Service Road North, Building T,**  
**Mobile, Alabama**  
**on May 8, 2018**  
**commencing at 9:01 a.m.**

	I N D E X	
1		
2	MARK BARTLETT - welcome	3
3	MARK ERICKSEN - Introductions	3
4	MAYOR SANDY STIMPSON	5
5	ALLISON GREGG - Introductions	7
6	MISSI SHUMER - Purpose and need	13
7	PAT HICKOX - Project updates	20
8	ALLISON GREGG - Alternative Delivery	42
9	(VIDEO PLAYED)	47
10	PAT HICKOX - Vibration Study Updates	50
11	PAT HICKOX - Aesthetics	59
12	PAT HICKOX - Landscaping	64
13	BRIAN UNDERWOOD - Tree canopy	70
14	MISSI SHUMER - USS Alabama Memorial Battleship Park	81
15	MISSI SHUMER - Historic structures	83
16	MISSI SHUMER - Archeology	84
17	MATT ERICKSEN - Future Section 106 Activities	95
18		
19	HERNDON INGE - Comments	99
20	DEVEREAUX BEMIS - Comments	109
21	ELIZABETH MERRITT - Comments	111
22		
23		

## P R O C E E D I N G S

1  
2 MARK BARTLETT: Good morning everyone. I'm  
3 Mark Bartlett, the division administrator for the  
4 Federal Highway Administration. And I would like to  
5 be the first to welcome you to our consulting party  
6 meeting. We appreciate all of the consulting parties  
7 for coming out today and visiting with us. The last  
8 meeting we had was back in 2014, and I know a number  
9 of you participated in that meeting. Since then we've  
10 had a number of activities, including sending out a  
11 Draft MOA that was based on some of the findings of  
12 that meeting, some of your comments. And we have also  
13 conducted a number of studies.

14 So today what we would like to do is walk  
15 you through some of the activities that have occurred  
16 since that last meeting, share with you some of the  
17 activities that ALDOT has made on the project,  
18 introduce the new team that's working on the project  
19 for ALDOT, and just get some more input from you.

20 So with that, I'll turn it over to Matt.  
21 Matt?

22 MATT ERICKSEN: Thank you, Mark. And good  
23 morning. And we do appreciate everybody attending

1 today and those that have joined us via Skype and by  
2 phone. As Mark said, today we'll be giving you some  
3 updates on the progress that ALDOT has made on this  
4 project, which has been substantial since our last  
5 meeting on September 23rd of 2014.

6 I wanted to first start off by introducing  
7 you to our new team of advisors. We have made a  
8 change since that September 2014 meeting, and we have  
9 a new owner's rep. Our new project team is being led  
10 by Thompson Engineering and also the consulting firms  
11 of HDR and Mott MacDonald from the technical side.  
12 PRR is our new public relations firm. And Shumer  
13 Consulting is leading our environmental process. And  
14 you'll hear from Missi today. And she has a good  
15 history with the project as she's worked on it for  
16 several years.

17 So before I bring Allison Gregg, who is our  
18 public information officer, up to lead us through the  
19 agenda, we'd like to thank Mayor Stimpson for being  
20 here today and carving some time out of his busy  
21 schedule. He's going to come up and give us a few  
22 words about the project from the City of Mobile's  
23 perspective. So Mayor?



1           MAYOR STIMPSON: Thank you, Matt. And good  
2 morning to everybody.

3           My involvement with the I-10 bridge project  
4 began 10 years ago as a private citizen as I attended  
5 a Chamber of Commerce function. My involvement and  
6 knowledge of the project somewhat escalated when I  
7 became the chairman of the Chamber in 2009. And after  
8 being elected the mayor in 2013, my involvement  
9 tremendously escalated as I learned more, listened  
10 more, and networked more with other stakeholders.

11           There's a little feedback. Let me see if  
12 it's my phone. Is that better? No.

13           Anyway, as we inch closer and closer to the  
14 reality of the bridge being built, advocacy groups  
15 continue to expand and strengthen in their resolve to  
16 support the project. From my perspective, this is due  
17 to how ALDOT has handled this process. The project  
18 would not be where it is without their leadership and  
19 their willingness to embrace public input, coalition  
20 building, and transparency. More importantly, they  
21 have responded to public input by seeking solutions to  
22 meet the needs of all Mobilians.

23           Additionally, the engagement of constituent

1 groups in both Mobile and Baldwin County have ushered  
2 the project along. At one point 16 mayors and three  
3 county commissioners from the two counties  
4 participated in a Washington, DC fly-in in support of  
5 the project. This is unprecedented and reflective of  
6 the project's broad support.

7 10 years ago there was skepticism about the  
8 bridge. Today it's almost nonexistent. As a matter  
9 of fact, on a recent factfinding tour of Mobile,  
10 Governor Ivey met with approximately 20 elected  
11 officials and community leaders. As she stood poised  
12 at the whiteboard ready to hear their remarks about  
13 their number one priority, what she heard was, with  
14 unanimity: It's the bridge.

15 She then heard a whole list of reasons,  
16 among them including improved public safety and  
17 commerce. Further, she heard that the daily gridlock  
18 along the I-10 corridor is now gridlocking traffic  
19 every day in downtown Mobile, subsequently having a  
20 negative impact on business and commerce.

21 In the face of the booming opportunities  
22 arising from the growth of the port, the growth of  
23 Airbus, the Walmart Import Distribution Center coming

1 on line, plus many other growing businesses combined  
2 with increased tourism travel, the time for the bridge  
3 is now.

4 In closing, I want to thank ALDOT for  
5 embracing innovation and for handling this project  
6 with the utmost integrity and professionalism. The  
7 project has my 100 percent support. Thank you very  
8 much.

9 MATT ERICKSEN: Thank you, Mayor. We  
10 appreciate your comments.

11 So with that I'm going to introduce Allison  
12 Gregg. As I mentioned, she is our project public  
13 information officer that has come to us from PRR. And  
14 she'll be leading us through the agenda today.

15 ALLISON GREGG: Thank you, Matt. Good  
16 morning everybody. And good morning to those who I  
17 have not met yet today.

18 Speaking of meeting people, we have a court  
19 reporter in the room, and so we do want to go around  
20 the room and have introductions. And then also as  
21 you're making comments, we ask that you throughout the  
22 presentation say your name so that the court reporter  
23 can capture you.

1           So I am Allison Gregg, and I'm the public  
2 information officer.

3           PAT HICKOX: Pat Hickox, the project manager  
4 for the consulting team.

5           MISSI SHUMER: Missi Shumer, consulting  
6 team.

7           MATT ERICKSEN: Matt Ericksen, Alabama  
8 project director.

9           MARK BARTLETT: Mark Bartlett, Federal  
10 Highways division administrator.

11          JANET COBB: Janet Cobb, Battleship Memorial  
12 Park.

13          BRIAN HOGGE: Brian Hogge, Federal Highway  
14 assistant division administrator.

15          TIM HEISLER: Tim Heisler, Federal Highway  
16 Administration, area engineer for Mobile.

17          LYNNE URQUHART: Lynne Urquhart, Federal  
18 Highway, environmental.

19          AARON MOSES: Aaron Moses, ALDOT, design  
20 location.

21          BRIAN INGRAM: Brian Ingram, design  
22 location, ALDOT.

23          STEPHANIE DRAGOTTA: Stephanie Dragotta,

1 ALDOT, Mobile Bridge project manager.

2           ANDREW WOOD: Andrew Wood, project manager,  
3 ALDOT.

4           EDWIN PERRY: Edwin Perry, ALDOT, southwest  
5 region.

6           NICK AMBERGER: Nick Amberger, City of  
7 Mobile, City engineer.

8           ELIZABETH STEVENS: Elizabeth Stevens,  
9 Downtown Mobile Alliance.

10           LEAH STIMPSON: Leah Stimpson,  
11 Condé-Charlotte Museum.

12           ELIZABETH HARRIS: Elizabeth Harris,  
13 Condé-Charlotte Museum.

14           LOUISE RAYFORD: Louise Rayford,  
15 Condé-Charlotte Museum.

16           JULIA WILLCOX: Julia willcox,  
17 Condé-Charlotte.

18           MARY LEE MONTGOMERY: Mary Lee Montgomery,  
19 Condé-Charlotte Museum.

20           WADE HENRY: Wade Henry, ALDOT Design  
21 Bureau, preliminary engineering.

22           STEVE WALKER: Steve Walker, ALDOT, state  
23 design engineer.

1 DOLHA KAYISAVERA: Dolha Kayisavera, ALDOT  
2 environmental.

3 NATASHA CLAY: Natasha Clay, ALDOT  
4 environmental.

5 HERNDON INGE: I'm Herndon Inge. I am  
6 representing Stop the Bridge.

7 DEVEREAUX BEMIS: Devereaux Bemis with  
8 Restore Mobile.

9 BONNIE GUMS: Bonnie Gums with the  
10 University of South Alabama.

11 PAT PATTERSON: Pat Patterson, ALDOT  
12 environmental division.

13 BILL TURNER: I'm Bill Turner. I'm with  
14 ALDOT environmental section.

15 CONNIE HUDSON: Connie Hudson, president of  
16 the Mobile County Commission.

17 GREG LOWE: Greg Lowe with the consulting  
18 team.

19 STAN BIDDICK: Stan Biddick, ALDOT Design  
20 Bureau.

21 BRIAN UNDERWOOD: Brian Underwood, City of  
22 Mobile, long-range planning.

23 VINCE CALAMETTI: Vince Calametti, ALDOT,

1 region engineer.

2 JIMMY SHUMOCK: Jimmy Shumock, consulting  
3 team.

4 JOHN SLEDGE: John Sledge, Mobile Historic  
5 Development Commission.

6 ALLISON GREGG: Great. And as mentioned  
7 earlier, we actually do have some folks on the phone.  
8 And I'm worried about the logistics of them  
9 introducing themselves, because you know how everybody  
10 starts at once. But on the phone -- Missi, can you  
11 help me with the names?

12 MISSI SHUMER: So far we have Chris from the  
13 Alabama Historical Commission, William from the  
14 Alabama Historical Commission, Amanda McBride from the  
15 Alabama Historical Commission, and Karen White.

16 ALLISON GREGG: Did we miss anybody on the  
17 phone?

18 BETSY MERRITT: Betsy Merritt, National  
19 Trust for Historic Preservation. I tried to get there  
20 in person, but my flight was delayed in Tampa. So I'm  
21 sorry I can't be there in person.

22 ALLISON GREGG: That's okay. Thank you for  
23 joining us on the phone.

1           So the way that we're going to work today is  
2 -- we have a lot of information to go over. It's been  
3 a while --

4           SARA STOKELY: There's one more person.  
5 Sara Stokley from the Advisory Council on Historic  
6 Preservation.

7           ALLISON GREGG: Sorry about that, Sara.

8           SARA STOKELY: That's okay.

9           ALLISON GREGG: So the way we want today to  
10 operate is to be a conversation. So what we're going  
11 to do is go through a lot of the information about the  
12 project and the work that's been done in the last four  
13 years. And then also we want to make sure that you  
14 have time to talk about the things that are not -- and  
15 I'm sorry about this; I can't figure out how to make  
16 it stop. (Referring to microphone.) And I'm also a  
17 very loud person, which is why I went into public  
18 relations because I like to talk.

19           So we will have a time at the end of the  
20 presentation to go through and do general questions.  
21 And there's an opportunity for you to -- it's doing it  
22 again. (Referring to microphone.)

23           GERI ROBINSON: Geri Robinson for the United



1 States Coast Guard. I just came in.

2 ALLISON GREGG: Great. Nice to see you.

3 So you'll have an opportunity to provide  
4 comments generally about the project at the end. And  
5 there was an opportunity to sign up at the beginning  
6 as you came in.

7 And I want to talk about safety. We do have  
8 these outlets around the floor. So please be mindful  
9 as you're walking around. The restrooms are located  
10 straight out this door to your right. And then we  
11 will have a break in the middle of our presentation as  
12 well.

13 So thank you. If you have any questions,  
14 just let us know. I'll turn it over to Missi.

15 MISSI SHUMER: So I know that a lot of you  
16 are familiar with this project, but we have some new  
17 faces. So we thought we'd start with an overview of  
18 the project beginning with the purpose and need.

19 You've heard a lot of this from the Mayor.  
20 But the purpose and need of the project is to increase  
21 capacity of the I-10 corridor to meet existing and  
22 predicted future traffic volumes, to provide a more  
23 direct route for vehicles transporting hazardous

1 materials, all while minimizing impacts to Mobile's  
2 maritime industry.

3 So the project is approximately 10 miles  
4 long, and it begins just on the left, southwestern  
5 side of the Virginia Street interchange. And it  
6 includes a six-lane cable-stayed bridge over the  
7 Mobile River as well as an eight-lane Bayway. And it  
8 includes -- these orange circles here are the five  
9 interchanges that were modified as part of the  
10 project.

11 So a little bit about the project history.  
12 As part of the project development, we conducted an  
13 alternatives analysis that began with 14 build  
14 alternatives as well as the no-build alternative.  
15 Those alternatives were evaluated, and we reduced them  
16 for various reasons down to four build alternatives in  
17 the Draft Environmental Impact Statement. We also  
18 kept the no-build alternative in the Draft  
19 Environmental Impact Statement and will continue to  
20 carry it through the project as we move forward.

21 The Federal Highway Administration approved  
22 the Draft Environmental Impact Statement in July of  
23 2014, and the Draft Environmental Impact Statement

1 included a variety of environmental commitments  
2 related to things such as cultural resources, wetland  
3 mitigation, lighting, bike/ped, and others.

4 So following approval of the Draft  
5 Environmental Impact Statement in July, we held a  
6 series of public hearings in September and got  
7 comments and feedback from the public and from the  
8 agencies on the project.

9 Since that time, as Matt introduced, a new  
10 project team has come onboard. And there have been a  
11 lot of changes to the project. And that's what we're  
12 here to share with you today. And to evaluate those  
13 changes, the Federal Highway Administration published  
14 a Notice of Intent to prepare a Supplemental Draft EIS  
15 and a combined Final EIS and Record of Decision. That  
16 was published in the Federal Register in June of 2017.

17 We are currently preparing the Supplemental  
18 Draft Environmental Impact Statement, and it will  
19 cover major changes in the project, such as refinement  
20 to Alternative B Prime, updates to traffic analyses,  
21 the addition of tolling as a funding source,  
22 alternative delivery method, and updated environmental  
23 studies.

1           Once this Supplemental Draft EIS is  
2 approved, we will hold another series of public  
3 hearings, and the EIS will be available for public and  
4 agency review.

5           Ultimately the project will have a Final EIS  
6 and Record of Decision after the public hearings, and  
7 that is when we will have a decision on which  
8 alternative will be carried forward.

9           So this is actually the seventh section 106  
10 Consulting Party meeting since this project began.  
11 And just to remind everybody what each of those  
12 meetings were, the very first one was in December of  
13 2003. And it was really a scoping meeting. That  
14 meeting was the first consulting party meeting after  
15 the project was elevated from an Environmental  
16 Assessment to an Environmental Impact Statement. And  
17 at that meeting we discussed the purpose and need,  
18 alternatives, the NEPA process, National Environmental  
19 Policy Act process, and the section 106 process.

20           In July of 2006 we had identified some  
21 potential build alternatives, and we got back together  
22 to discuss and identify the area of potential effects.  
23 And the area of potential effects is the limits that

1 confined our cultural resources surveys for Section  
2 106 consultation and the environmental process.

3 So in August of 2010 we got back together  
4 and discussed the alternatives once again. And at  
5 that point we had some potential effects that we could  
6 discuss, especially centered around our initial  
7 Viewshed Impact Assessment. So that was the meeting  
8 where we started sharing renderings of what the bridge  
9 may look like from a variety of sites and locations.

10 The following day we conducted a field  
11 review with the consulting parties where we actually  
12 went out into the field and looked at where the  
13 alternatives would be located and discussed potential  
14 effects.

15 July of 2012 we met again and talked more  
16 about potential effects. And we had some updated  
17 cultural resources surveys and shared the results of  
18 those.

19 In September of 2014 was the most recent  
20 meeting where we provided a project update and  
21 discussed potential effects.

22 So following these meetings, ALDOT and FHWA,  
23 as part of the Section 106 consultation, they put

1 together a Draft Memorandum of Agreement, which  
2 hopefully everyone in this room has seen. And really  
3 the intent of that Draft MOA was to be a starting  
4 point to further this discussion. So the comments  
5 that we have gotten back from the consulting parties  
6 on potential effects, on resources of concern, on  
7 potential mitigation measures, those are all things  
8 that were put into this document to further the  
9 conversation. And as you've seen, it is a work in  
10 progress. And so as we send it to you, we take your  
11 comments, we revise it, and we continue to gather  
12 information, which is really the point of today,  
13 taking into consideration the updates that we're going  
14 to share.

15 So in May of 2014 we requested written  
16 comments on the determination of effects.

17 On February 24th, 2016, we submitted a Draft  
18 Memorandum of Agreement for review and comment by the  
19 consulting parties.

20 July of 2016 the comments that we received  
21 from the consulting parties along with a revised MOA  
22 were submitted and circulated to the consulting  
23 parties.

1           And then the most recent written  
2           correspondence was April of this year where we sent  
3           you all the comments that we received as well as a  
4           disposition of how those comments either were  
5           addressed or will be addressed in the MOA as well as  
6           the latest version of the Draft Memorandum of  
7           Agreement.

8           So I know that's a lot of information. But  
9           it's important to understand how this process has  
10          worked and hopefully set the stage for how it will  
11          continue.

12          Subsequent to the last meeting we received a  
13          lot of comments on the effects determination. And  
14          previously we had a no-adverse-effect determination.  
15          But on May 18th, 2015, the Federal Highway  
16          Administration issued a letter to the Alabama  
17          Historical Commission stating that the project may  
18          have adverse visual effects on the Church Street East  
19          Historic District and the Lower Dauphin Historic  
20          District.

21          The Alabama Historical Commission concurred  
22          with the adverse visual effect determination on June  
23          11th, 2015, and that determination has been included

1 in the most recent version of the Draft MOA. So  
2 that's an important change since the last time we met.  
3 We're not -- we're not saying there's no adverse  
4 effect. So we are moving ahead with that  
5 determination.

6 with that, I'm going to turn it over to.  
7 Pat, and he's going to share with you the updates to  
8 the project.

9 PAT HICKOX: Good morning, everybody. I'd  
10 first like to start with the additional studies that  
11 we've accomplished since the last consulting party  
12 meeting. These have been completed in support of  
13 advancing the EIS portion of this project.

14 Geotechnical studies have been progressing.  
15 There have been 180 borings on the site right now.

16 Project-wide survey.

17 Storm surge analysis. And I'll cover storm  
18 surge analysis later in the presentation.

19 Tolling and traffic. We have ongoing  
20 traffic and tolling modeling that we're doing in  
21 support of advancing the financial support for the  
22 project.

23 Bike and pedestrian alternatives. We



1 completed a study and held a public involvement  
2 meeting in October of 2016.

3 Hazardous materials; basically completed a  
4 preliminary report of initial investigations.

5 Noise and air. Noise and air studies are  
6 underway, and those will be finalized as we complete  
7 our traffic studies.

8 Draft mitigation plan. We've held agency  
9 coordination meetings and issued a draft mitigation  
10 plan, including review and comments from those  
11 agencies.

12 We've also performed refinements to the  
13 geometrics to the alignment of B Prime that we'll go  
14 over shortly as well.

15 So those are the basic studies that we've  
16 completed on the project so far.

17 So first let's address the refinements of  
18 B Prime. The first one, I'll just call out what these  
19 colors mean on the chart that you can see there or the  
20 map. Basically in blue, what you see in light blue on  
21 each side of the red line was the original B Prime  
22 alignment as it existed in the Draft EIS. As I stated  
23 previously, we've advanced the geometrics of the

1 project with more information, and we've been able to  
2 relocate, or move, slightly refine the alignment of  
3 B Prime as shown in red further away from downtown and  
4 the historic districts, which is a great improvement  
5 over the previous alignment.

6 Basically along the alignment as it crosses  
7 Mobile River, we actually maintained the same B Prime  
8 alignment. And then going towards the east, very  
9 little changes were made with regard to geometrics of  
10 the B Prime as it crosses Mobile River and ties back  
11 into the Bayway on the east side of the river.

12 So let's talk about the main span, kind of  
13 starting at the top and working our way down. This is  
14 a basic layout of the main span and high-level units.  
15 The main span is the section in green which you can  
16 see on the screen there with the high-level approaches  
17 being in light blue on each side of the main span  
18 unit.

19 So we'll take each one of these separately  
20 and talk about them and how we've advanced their  
21 portions of the project.

22 So a little bit about the main span. The  
23 structure type is a high-performance, concrete,

1 cable-stayed bridge. And I'll talk a little bit more  
2 about that bridge type shortly. We also did a number  
3 of cross-section studies looking at various  
4 cable-stayed bridge types to really optimize a number  
5 of things; one, the visual effects, visual impacts, as  
6 well as economics for that structure type. We did a  
7 number of span arrangement studies. Obviously, as we  
8 cross the river, as you can see here, the B Prime  
9 alignment, again the main span unit is the lime green  
10 color. We have to maintain vertical clearance and  
11 horizontal clearance of the center channel or the  
12 center span of the main span unit, 215 feet vertical,  
13 over 600 feet horizontal as we cross the channel. As  
14 you notice from the screen --

15 HERNDON INGE: Excuse me, Pat. What is the  
16 vertical clearance now?

17 PAT HICKOX: 215 feet, the same as it was in  
18 the Draft EIS. Yes. Thank you.

19 One thing to notice, because of the  
20 alignment of B Prime, we cross the river at a skew,  
21 which means that that would require a longer span to  
22 accommodate that same horizontal clearance. So this  
23 span arrangement was chosen for a number of reasons.

1 One is to obviously accommodate the vertical and  
2 horizontal clearance requirements, and two is to  
3 really optimize and minimize the impacts to the  
4 potential users of Mobile River. So the placement of  
5 the tower foundations that you can see here are very  
6 close to the edges of the banks of the river. So that  
7 allows, again, more open area underneath the river for  
8 the users and the businesses that do a lot on the  
9 Mobile River.

10 We have been courting closely with a number  
11 of agencies, including the Port, Corps of Engineers,  
12 U.S. Coast Guard, and the FAA in the selection and  
13 determination of this bridge type.

14 One thing to note in case -- I meant to add  
15 this in case I miss it later on is the main span will  
16 have an open barrier on the outside, traffic barrier.  
17 That's for a couple of reasons. One, it offers  
18 advantages from an aerodynamic performance standpoint  
19 for the bridge itself. But two, it also allows  
20 drivers to be able to see as they cross the river. So  
21 again, the barriers on the inside as you're driving  
22 across will be solid; the barriers on the outside will  
23 be an open railing type.

1           So a little bit about why a cable-stayed  
2 bridge. This chart indicates the various structure  
3 types that potentially could be considered at this  
4 location. We looked at all of these. And really,  
5 based -- what this chart shows really is a span range  
6 of economy to make sure we really selected the most  
7 appropriate structure type for the crossing of the  
8 river. As you can tell for the span that we need and  
9 the clearances required, it puts us in a very good --  
10 I would say the sweet spot of cable-stayed bridge  
11 lengths. Obviously other structure types potentially  
12 could be stretched, but obviously this would be at  
13 expense to the project.

14           We also kind of wanted to place the project  
15 in scale so that everyone kind of understands a little  
16 bit about how it fits with other similar structures  
17 that many of you may know. This is, as depicted on  
18 the screen, the Mobile River Bridge with its 215 feet  
19 of vertical clearance.

20           The Cochrane Bridge with its 155 feet of  
21 vertical clearance.

22           And then Sunshine Skyway in Tampa, Florida,  
23 with 175 feet of vertical clearance.

1           And the Ravenel Cooper River Bridge in  
2 Charleston, South Carolina, with 185 feet vertical  
3 clearance. It's very similar to that structure, as  
4 you can tell by the screen.

5           And then again, just to compare it to a much  
6 longer type of bridge, the Golden Gate Bridge in  
7 San Francisco with 220 feet of vertical clearance.

8           Moving on to what we did with the  
9 cable-stayed bridge moving forward is we took that  
10 structure type and we basically focused on a number of  
11 similar arrangements using a cable-stayed bridge. The  
12 images on the left side of the screen indicate three  
13 basic configurations of cable-stayed bridge type that  
14 could be considered. And these are the tower shapes.  
15 So just to give you an idea of where you are on this  
16 image, this is basically a cross-section. So the cars  
17 would be basically traveling in and out of the screen.  
18 So vehicles would be in this location here and here  
19 going in and out of the screen. So it gives you an  
20 idea as you're driving across what the towers could  
21 look like.

22           So we looked at each of these: A single,  
23 what we call a single tower, a double diamond that's

1 very similar to the Fred Hartman Bridge if you've ever  
2 traveled through that area in Texas, and double  
3 inverted Y, which is very similar in shape to the  
4 Charles River Bridge in Boston, Massachusetts. And we  
5 looked at each of these, measuring their ability to  
6 serve us well in this project from an economic  
7 standpoint as well as a serviceability standpoint.

8           And the result was the double inverted Y,  
9 the one on the far right, really had the best  
10 advantage, the best structural response during extreme  
11 wind events. It offered up a very shallow opportunity  
12 for a very shallow superstructure. And that  
13 superstructure you can see in this location here. And  
14 again, this would be -- the vehicles would be  
15 traveling on each side of that roadway that you can  
16 see. But again, it offered us an opportunity to  
17 really shallow up -- minimize the visual impact as it  
18 crosses the river. And again, we think it brings a  
19 very high-stake value opportunity to the area.

20           We've created a number of conceptual  
21 renderings. And I'm going to talk more about  
22 aesthetics later in the presentation. But we did want  
23 to give you a view of the previous rendering from the

1 B Prime alternative and where we are today with the  
2 double inverted Y cable-stayed bridge.

3 Okay. Now moving on to the high-level  
4 approaches. If you recall, those are the light blue  
5 sections of the bridge approaching the main span unit.  
6 Obviously, to obtain the 215 feet of vertical  
7 clearance for the river crossing, we have to start a  
8 long way back to achieve that elevation. So I'm going  
9 to talk to you briefly about these.

10 On the west the high-level approach starts  
11 just east of Virginia Street connecting with the main  
12 span unit, starting here and ending at this location  
13 here. Longer spans were used at various locations to  
14 minimize the impacts to local businesses and roadways  
15 underneath, including the Mobile County Metro Jail.

16 On the east side the high-level approach  
17 continues from the end of the main span unit at this  
18 location, across the Austal parking lot, and ends in  
19 between the existing Bayway structures.

20 We also wanted to bring up the basic  
21 cross-sections that we looked at with regard to what  
22 the structure could look like. And this is the basic  
23 cross-section for the typical high-level approach



1 bridges as they approach the main span unit with the  
2 superstructure being a closed-cell concrete box. And  
3 that's shown here.

4 And these were chosen for several reasons.  
5 Obviously economics. But they also had benefits from  
6 an aesthetics standpoint, a nice smooth underneath  
7 surface as well as the opportunity for longer spans  
8 and minimizing the impacts to features underneath the  
9 bridge.

10 We also looked at a number of -- you'll see  
11 the various pier shapes that support the  
12 superstructure down below that. A number of these  
13 will be used to accommodate the alignment in the  
14 roadways underneath. So there will be a variety of  
15 these types of structures.

16 So let's get into the interchanges. This is  
17 the overview map that Missi went through earlier. So  
18 you can see the five interchanges highlighted there in  
19 the orange circles. So we'll take each one of those,  
20 starting to the west and working our way east.

21 So we'll start with the Virginia Street  
22 interchange. This interchange is a new diverging  
23 diamond with improved free-flow connections in and out

1 of the port. It also includes replacement of the  
2 overpasses at Tennessee, Pillans, Virginia and Texas  
3 Streets shown in purple and placing the double  
4 diverging diamond at grade. This alternative was  
5 developed in close coordination with the port as well  
6 as the truckers association.

7 Now to the Canal Street or Water Street,  
8 sometimes called the west tunnel interchange project.  
9 As many of you may recall, this design was actually  
10 completed, the design was finished and permitted and  
11 was close to going to construction a couple of years  
12 ago. And it was decided at that time, because of the  
13 fact that it's integral to the Mobile River Bridge  
14 project, that this interchange would be incorporated  
15 into this project.

16 what this enables us to do is convert the  
17 existing I-10 into business I-10. So this will become  
18 a new business I-10 through the Wallace Tunnel.  
19 Similar to Virginia Street, it is a diverging diamond  
20 at grade level, which is important to know at this  
21 point, with an overpass bridge at Canal Street shown  
22 in purple.

23 It offers up a lot of advantages, this

1 alternative does. Improved connection to Wallace  
2 Tunnel is one of them. Also it offers up the  
3 opportunity to place all local traffic at grade level.

4 The demolition and removal of all existing  
5 ramps that currently serve the Wallace Tunnel, the  
6 removal of those aerial structures will be part of  
7 this project. So those ramps will no longer exist.  
8 And all of the traffic will be replaced by local  
9 traffic at ground level.

10 This open area -- basically it will create a  
11 very large, as you can imagine, open area -- will be  
12 developed in close consult with the City of Mobile.  
13 So we're very excited about this alternative and what  
14 it can mean to Mobile.

15 Now, the east tunnel interchange looks very  
16 similar to the existing today with some geometric  
17 improvements, especially with the ramps, on and off  
18 ramps to I-10. You can also see how the high-level  
19 approaches in light blue will tie in and comprise the  
20 beginning of the Bayway.

21 Midbay interchange, basically reconstruction  
22 of the existing as it exists today. The only  
23 difference being it's potentially more likely to

1 become a signalized intersection. But that will  
2 depend upon the warrant at the time.

3 Eastern Shore interchange, very similar  
4 reconstruction of the existing to maintain those  
5 connections, but we believe an improvement over  
6 existing with longer decel lanes for the I-10  
7 eastbound off ramps as well as improved laneage and  
8 connectivity along U.S. 90 and 98.

9 Okay. So that takes us through the main  
10 span, the high-level approaches, as well as the  
11 interchanges.

12 So storm surge. The reason why this is  
13 being brought up as a topic is because as a result of  
14 the hurricanes that we've had over the last 10 or 15  
15 years in this area as well as requirements from FHWA  
16 and AASHTO, we had to perform a storm surge as part of  
17 this project, storm surge study. Here's some examples  
18 of some of the damage that occurred from Ivan in  
19 2004 -- these are all local bridges, local to us --  
20 and Katrina in 2005. It really changed our landscape,  
21 as many of you can appreciate. The approach -- really  
22 as an engineer, approach is vital to structures away  
23 from areas. As a result, AASHTO and FHWA have

1 requirements of how to design for bridges that are in  
2 these types of areas or zones.

3 I'm not trying to baffle anyone here by this  
4 slide. But the point of this slide is AASHTO -- this  
5 is straight out of AASHTO. They have specifics on how  
6 they address and avoid these types of impacts and  
7 forces to superstructures. Basically it's just,  
8 wherever practical, the vertical clearance of highway  
9 and bridge should be sufficient to provide at least  
10 one foot of vertical clearance over the 100-year  
11 design crest elevation. And then it also goes on to  
12 say in areas where that can't be avoided, other steps  
13 should be taken into account. And that basically  
14 means that we have to design the bridge to withstand  
15 those wave impact forces.

16 So what does that mean to the Mobile River  
17 Bridge project and specifically the Bayway? This  
18 plot -- and I'll walk you through it very carefully so  
19 it's easier to understand -- is a plot from our level  
20 3 storm surge analysis that we did as part of the  
21 project to help us determine the potential elevations  
22 for potential wave crest heights and storm events and  
23 forces that come from the 50- and 100-year storms.

1           From this plot you can see -- I'll show it  
2           real quickly. If you look at the very top, that's the  
3           profile of the Bayway if you drove across it today.  
4           So if you drove across it in your car and were able to  
5           measure the elevation of the Bayway, that's what you  
6           would see, that top black line. The 100-year wave  
7           crest height, maximum wave crest height, that you  
8           would see at these locations is shown in red. So you  
9           can see in nearly all portions of the Bayway for a  
10          100-year storm event, the Bayway is overwhelmed and  
11          more than likely destroyed.

12           So because of the results of this study and  
13          requirements from AASHTO and FHWA, the decision has  
14          been made to replace rather than widen the Bayway for  
15          this project.

16           So in cross-section, what does that mean for  
17          us? This is a comparison of the old shown in kind of  
18          the light gray off to the left and right of the center  
19          of the screen and the new Bayway section, four lanes  
20          in each direction with four shoulders in between the  
21          existing Bayway structures. In general, you're  
22          looking at an elevation change of about eight feet,  
23          depending on where you are. If you remember from the

1 plot, it somewhat varies as you go across because  
2 that's how waves work. But in general, we're looking  
3 at about an eight-foot increase in height over  
4 existing today.

5 Okay. On to bicycle and pedestrian  
6 facilities. We did a number of studies associated  
7 with this. It really started with the commitments in  
8 the Draft EIS. And here is a summary of those  
9 commitments. Basically ALDOT committed to providing a  
10 crossing facility for bikes and peds across the Mobile  
11 River via the Cochrane Bridge or the Bankhead Tunnel.  
12 Then it goes on to make sure that we address the  
13 location of pier supports or bridge supports in those  
14 areas around some of the local trails, including the  
15 Crepe Myrtle Trail, the Eastern Shore National  
16 Recreation Trail/I-10 Scenic Underpass Trail on the  
17 Eastern Shore. So that was a real important part of  
18 the project that we needed to move forward.

19 So we did a number of studies. We looked at  
20 a number of alternatives. We also had a bike and ped  
21 workshop on October 27, 2016, where we went through  
22 some of the preliminary results of these studies,  
23 being the Bankhead Tunnel alternative, the Cochrane

1 Bridge alternative. And ALDOT chose also, based on  
2 some community response, to include the new Mobile  
3 River Bridge as an alternative for the study as well.  
4 So they actually added an alternative for  
5 consideration.

6 I'll walk you through this. This looks  
7 confusing as well. I'll try to walk you through this  
8 one. This is a map of the potential crossings for a  
9 bike and ped facility for the Mobile River Bridge. As  
10 I stated earlier, there are three primary crossings,  
11 so you can see them there. Coming out of the  
12 workshops and the studies, ALDOT has selected the  
13 primary or base crossing for the bike and pedestrian  
14 facility to be the Cochrane Bridge shown in green. It  
15 says base requirement B, but don't worry about that.  
16 Basically this is one of the base requirements for the  
17 pedestrian crossing of the Mobile River.

18 Based on some community information in the  
19 public outreach that we did, one of the major comments  
20 was that we need to address or offer up an opportunity  
21 for views of Mobile Bay and the surrounding community.  
22 As a result of that, ALDOT decided to add another base  
23 requirement, and this is the construction of a ramp



1 leading from one of the anchor piers here in the main  
2 span tower leading up to a belvedere, which basically  
3 is an overlook allowing views of downtown and Mobile  
4 Bay from the edge of the river.

5 So those are the two base requirements that  
6 we're going to place on our contractor, proposed  
7 contractors, for this project.

8 Now, we also heard the community with regard  
9 to connectivity. And so we felt like it was  
10 important, if the project could afford it, to offer up  
11 other alternatives in addition to the base  
12 requirements. So again, the base requirement,  
13 Cochrane Bridge crossing in green; belvedere at the  
14 main span tower on the west side of the river. We  
15 also added, again, other alternatives. The  
16 alternative that we're going to offer up and basically  
17 receive pricing for is a full path that goes basically  
18 from the Virginia Street area, along the main span  
19 unit, down the high-level approach on the other side,  
20 connecting to a trail all the way to an area just in  
21 front of the Battleship Park. So that's an  
22 alternative we're going to request pricing for. And  
23 based on economics, we'll make a decision if it's

1 affordable or not at that point in time.

2 We're also going to ask for pricing for  
3 extension of the Cochrane Bridge path from Bay Bridge  
4 Road and 165. You'll actually travel along 165.  
5 There will be a dedicated bike path along 165 and  
6 connect at a location just around Beauregard Street.

7 And the last value-added option that we're  
8 looking at to request is the other side where we  
9 connect to the end of the Cochrane Bridge path all the  
10 way to Battleship Park along U.S. 90.

11 So the base requirement, again, the Cochrane  
12 Bridge. This gives you an idea. This is a  
13 cross-section of the bridge. I'm getting everybody  
14 used to my cross-sections now. That's good. The top  
15 being as it exists today, and then below being the  
16 conversion of that section. Because of how the  
17 bridge -- its orientation, we're required to place a  
18 bike path on each side of the bridge. So there will  
19 be one going eastbound and westbound and ends  
20 basically -- the location ends at the east service  
21 road of 98 and at 165 on Bay Bridge Road on the other  
22 side of the project.

23 ELIZABETH STEVENS: what is the width? I

1 can't read it.

2 PAT HICKOX: Those are eight-foot lanes. So  
3 for directional pedestrians, eight-foot lanes are  
4 allowed.

5 ELIZABETH STEVENS: Is there some separation  
6 for pedestrians? Or how is that going to work?

7 PAT HICKOX: No. They both follow the same  
8 paths. But again, you don't have to deal with  
9 pedestrians or cyclists coming from the other  
10 direction. It's basically one way across and one way  
11 back across.

12 The other base project requirement is the  
13 belvedere. Again, ALDOT heard the community and is  
14 offering this up as a base project feature. The  
15 access for the belvedere will be at the anchor pier.  
16 Basically it's the end of the cable-stayed bridge  
17 unit. If you remember the end of the lime green and  
18 the beginning of the light blue, it will be basically  
19 at that location. So it will be accessed either by  
20 stair tower or elevator up to the bridge deck, which  
21 will be over 215 feet because of the depth of the  
22 superstructure. At this location there will be a  
23 pathway, a 10-foot path, that will allow people to get

1 all the way up to the belvedere or a nice pedestrian  
2 overlook at the edge of the river on the west side of  
3 the project. And it's a pretty good size belvedere.  
4 It's about 700 square feet, the requirements in the  
5 technical provisions as we have it today. And then  
6 you can see the basic renderings of what potentially  
7 this could like.

8 ELIZABETH STEVENS: How long did you say it  
9 is?

10 PAT HICKOX: It will be 585 feet from the  
11 anchor pier to the first tower, or the belvedere.  
12 About 600 feet, a pretty easy walk for most folks.

13 ALLISON GREGG: So the question for those on  
14 the phone is how long is the belvedere, and the answer  
15 is 585 feet.

16 PAT HICKOX: So just to recap, the basic  
17 requirements of the Cochrane Bridge and the belvedere  
18 and the value-added options: a full bike path, which  
19 would be value-added A, all the way across the Mobile  
20 River Bridge. That will be a 12-foot path. Cochrane  
21 to downtown, extend the Cochrane Bridge route along  
22 I-165 from Bay Bridge Road to Beauregard Street. And  
23 the Cochrane to Battleship, extend the Cochrane Bridge

1 route along U.S. 98 from East Service Road to  
2 Battleship Park.

3 ELIZABETH STEVENS: Are you saying somebody  
4 could pick one of these three or all?

5 PAT HICKOX: We could pick one or all,  
6 depending on the affordability. Obviously I think  
7 everyone here would love to have all of these features  
8 as part of this project. But we'll see how the  
9 project economics work out.

10 ELIZABETH STEVENS: Are you thinking that  
11 each proposal -- they would propose an option in each  
12 one or they might just say I'm going to propose an  
13 option on A or whatever?

14 PAT HICKOX: They will provide pricing for  
15 each of these options. So the question was --

16 ALLISON GREGG: So the question was how does  
17 the value-added options work within the RFP.

18 PAT HICKOX: And the answer is we'll price  
19 each one of these.

20 Thank you for repeating the questions.

21 Any other comments there?

22 (NO RESPONSE.)

23 PAT HICKOX: Great. Allison is going to

1 cover alternative delivery.

2 ALLISON GREGG: So as you can see, a lot has  
3 happened in the last four years. One of the things  
4 that's also happened in the last four years is that  
5 ALDOT will be pursuing a public-private partnership  
6 for the funding of this project. And a public-private  
7 partnership is a contract with a private entity, so an  
8 organization, and then a public authority, such as  
9 ALDOT, to develop and deliver an infrastructure  
10 project. These are becoming more and more common  
11 across the United States. We are looking at a 55-year  
12 concession agreement, which is five years for  
13 construction and then 50 years for operation and  
14 maintenance. So we're pursuing what we in the  
15 industry call a DBFOM: design, build, finance,  
16 operate and maintain.

17 And so why we're doing this is because the  
18 price -- the estimated price tag of this project is  
19 2 billion dollars. And given the other needs across  
20 the state, obviously ALDOT needs to pursue some  
21 alternative funding. And at the federal and national  
22 level, the U.S. Department of Transportation is really  
23 encouraging this type of delivery method and

1 partnership. It really allows ALDOT to accelerate  
2 delivery, and it really gives ALDOT access to industry  
3 experts who have worked on projects similar to this  
4 across the country. And also it really transfers risk  
5 from ALDOT as part of the proposal process. And I'll  
6 talk about that in a minute. The concessionaire has  
7 to demonstrate financial and technical capabilities to  
8 ensure that ALDOT and the taxpayers don't incur a  
9 debt.

10 One of the things that a procurement team is  
11 working on with ALDOT is developing the technical  
12 provisions. And these are contractual requirements  
13 that will go into the requests for proposal. And this  
14 is also where the environmental commitments will be  
15 incorporated into the technical provisions.

16 As I mentioned, we are estimating the cost  
17 to be 2 billion dollars. And ALDOT is pursuing an  
18 array of funding for the project. We are looking at  
19 private activity bonds. We have applied for an INFRA  
20 grant and anticipate hearing the results of that later  
21 this summer. And we will also be doing a TIFIA loan  
22 as well. The red area that is highlighted indicates  
23 the area that will be paid back through toll revenue.

1           DEVEREAUX BEMIS: I'm sorry. What is the  
2 INFRA grant and TIFIA loan?

3           ALLISON GREGG: Sure. The INFRA grant is  
4 through the U.S. Department of Transportation, and it  
5 helps fund larger projects. So we are pursuing 250  
6 million dollars from the INFRA grant. And then the  
7 TIFIA is a loan program.

8           DEVEREAUX BEMIS: From?

9           ALLISON GREGG: From USDOT.

10          Matt, do you have anything to add to that?

11          MATT ERICKSEN: No. That's right. The  
12 INFRA grant is a discretionary grant that we've  
13 applied to for the last three years. And TIFIA is a  
14 loan through the USDOT that's a fixed and the lowest  
15 interest rates that are available. So it's about the  
16 cheapest cost of capital that is available.

17          ALLISON GREGG: So in order to make sure  
18 that we select the right concessionaire for the  
19 project, we are doing a two-step procurement process.  
20 In August of last year we had an industry forum where  
21 we had 400 industry representatives from all over the  
22 globe come to Mobile to meet with ALDOT to learn about  
23 the project. And then from there we sent out a



1 request for qualifications. We had 14 submit their  
2 qualifications. And in February of this year we  
3 short-listed that to three teams.

4 We are in the request-for-proposal phase  
5 right now, and we're doing an industry review. And  
6 that's really different for the P3 process. So the  
7 three teams are actually coming to Mobile. They are  
8 going to be here about four times individually, and  
9 they're meeting with ALDOT to learn and to help them  
10 further the development of our RFP.

11 And as we talked earlier, the RFP and the  
12 environmental process, procurement process, are really  
13 running concurrently. And so we will finalize the  
14 RFP, and that will go out at the end of this year.  
15 And then from there we'll select the best value  
16 proposal. And then we will go into our concession  
17 55-year agreement, which is the build and finance and  
18 the operations and maintenance of the project.

19 Tolling overview. One of the most common  
20 questions I get asked is what is the tolling rate  
21 going to be. And we actually do not know that yet.  
22 We are still working with our traffic and revenue team  
23 to make sure that we determine the tolling rate. We

1 do know that we will have all electronic tolling, so  
2 you will either have a transponder in your car or  
3 you'll pay by plate. And we'll have walk-in centers  
4 and customer service centers here in Mobile and in  
5 Baldwin County as well.

6 We will have a free route to assure that  
7 folks are able to travel between Mobile and Baldwin  
8 Counties. It will be the Causeway, Bankhead Tunnel,  
9 and Cochrane Bridge. And then the I-10 corridor will  
10 be tolled. That's the Mobile River Bridge, Wallace  
11 Tunnel, and the Bayway.

12 That is all of the first part of our day.  
13 We have a video now that we're going to show, and then  
14 we'll take a break. I did want to make an  
15 announcement. We do have copies of the aesthetic  
16 guidelines up here at the table. If you would like to  
17 come and take a look at those during the break, it  
18 might help provide a little context as we go into the  
19 next segment. And then we'll start the video. And  
20 we'll go ahead and plan to meet back here about 10:20.

21 ELIZABETH STEVENS: Can I just ask a  
22 question?

23 ALLISON GREGG: Yes.

1 ELIZABETH STEVENS: So the wallace will be  
2 after the toll?

3 ALLISON GREGG: wallace will be part of the  
4 toll, yes.

5 And then also if you did not sign up to  
6 speak earlier, the sign-up sheet is still out front.

7 (Video played. )

8 MISSI SHUMER: For those of you on the  
9 phone, can you see the animation that's on the screen?

10 ALLISON GREGG: Yes, it's there.

11 ELIZABETH STEVENS: The belvedere is not on  
12 there?

13 PAT HICKOX: No.

14 ELIZABETH STEVENS: Could you kind of point  
15 out where it would be?

16 PAT HICKOX: Sure. I'll be glad to narrate  
17 this a bit. Obviously this is a view from the City  
18 looking south across the main span unit.

19 Can we stop it? So the question was --  
20 that's perfect -- where would the belvedere be on this  
21 image? Access through the stair tower or elevator  
22 would be at the base of this location up to the top  
23 deck, top of the bridge deck, out 585 feet to the

1 tower. And that's where the belvedere would be.

2 ELIZABETH STEVENS: On the downtown side?

3 PAT HICKOX: On the downtown side.

4 ANDREW WOOD: Our rendering shows the  
5 downtown side.

6 PAT HICKOX: Yeah. I apologize. This is  
7 the downtown side. But yes. It could be on either  
8 the south or the north side of the bridge. Thank you.  
9 That's something we'll be looking at with the  
10 proposals that are submitted.

11 So this is just obviously a view  
12 crossing the -- it's about 1,340 feet, that center  
13 span, three lanes of traffic in each direction across  
14 the bridge.

15 So this is the approach as you're going  
16 towards the Eastern Shore from Mobile, three lanes in  
17 each direction at the high-level approaches with full  
18 shoulder. You can see the open barrier to your right,  
19 as I mentioned earlier, and the solid barrier to the  
20 left.

21 So we'll need to update the model to depict  
22 the base options.

23 So this is the beginning of the main span

1 unit as you go between the cable stays. This is the  
2 inverted Y. If you remember the cross-section, we're  
3 going through the legs of the inverted Y.

4 We're nearing the center of the river at  
5 this location. This location is where the stays end  
6 and the other stays for the other towers begin. So we  
7 pass through the other tower. It really creates a  
8 beautiful kind of portal in and out of the area and  
9 sweeping views on both sides as you exit the main span  
10 unit.

11 All right. So we'll just let this loop. So  
12 take your time. We'll come off break and answer any  
13 more questions about the rendering. And again, the  
14 aesthetic guidelines -- and I'll go over what those  
15 are after the break. But they're available for you to  
16 take a look at and get a sense of those. Thank you.

17 (A RECESS WAS TAKEN FROM 10:01 A.M.  
18 TO 10:23 A.M.)

19 ALLISON GREGG: Before we get started for  
20 our second session of the day, I want to take a moment  
21 and those who were not here at the beginning when we  
22 did introductions, if you could introduce yourselves  
23 if you were not in the room at the beginning.

1 BEVERLY GIBSON: I'm Beverly Gibson with  
2 Christ Church Cathedral.

3 ALLISON GREGG: And Beth?

4 BETH SCHIAVONI: Beth Schiavoni, Thompson  
5 Engineering.

6 ALLISON GREGG: Thank you everybody.

7 PAT HICKOX: All right. So I want to make  
8 sure everybody can hear me okay. We're going to turn  
9 off the mic. Is everybody good? Okay. Good.

10 ELIZABETH STEVENS: Maybe just a little  
11 louder.

12 ALLISON GREGG: Can you on the phone hear  
13 our presenter?

14 A WOMAN: So far, yes.

15 ALLISON GREGG: Thank you.

16 PAT HICKOX: Thank you everybody.

17 One of the topics that was obviously brought  
18 up as a level of importance in the last consulting  
19 party meeting was vibrations. And if you recall,  
20 Dr. Cleary was here at the last meeting providing an  
21 update of the vibration study that was on the way.  
22 And that was on September 23rd when he was here, 2014.  
23 The draft study that was available at that time was

1 included in the Draft EIS. And the final report,  
2 which is out now, will be included in the Supplemental  
3 Draft EIS. And that was issued on June the 12th of  
4 2015.

5 So what are some of the findings and  
6 recommendations? Very little, if any, has changed  
7 from his update in the last meeting. But we felt like  
8 it was important to bring you to the point of what  
9 we've done with the findings and the final reports to  
10 date.

11 This is a graph from his report that  
12 indicates basically the vibrations measured in peak  
13 particle velocity, and that's the vertical axis and  
14 distance away. So what he's plotted here are two  
15 things. One is a very large displacement pile, as you  
16 can see, a 36-inch concrete pile. That's the top blue  
17 graph. So basically it gives you vibrations over a  
18 distance for a very large pile, a nondisplacement  
19 pile.

20 So what it shows is that over a distance of  
21 150 feet, this location along this axis here, we're  
22 expecting -- what he measured was a .15 PPV, which  
23 means that we'll have little to no risk of damage to

1 any adjacent structures at that distance. So any  
2 structures beyond that will experience even less  
3 potential risk. So that was very good news. We're  
4 fortunate that our geotechnical soil types really  
5 helped us mitigate any potential vibrations associated  
6 with concrete foundation piling for this project.

7 So how are we moving that information  
8 forward in the current contract documents? Well,  
9 really building on the previous meetings that you've  
10 had on this topic before and conversations, that's  
11 what we're recommending. First we're going to survey  
12 and monitor for any potential damages for all modern  
13 structures 150 feet or less, 250 feet or less for  
14 sensitive structures. In addition, specific  
15 structures, including the Christ Church Cathedral and  
16 Old City Hall, as well as the Wallace and Bankhead  
17 Tunnels.

18 DEVEREAUX BEMIS: May I ask a question about  
19 that?

20 PAT HICKOX: Yes, sir.

21 DEVEREAUX BEMIS: I don't have the map in  
22 front of me. But what about the effects on the  
23 buildings in Fort Condé village? Are they outside of



1 that?

2 ALLISON GREGG: So for those on the phone,  
3 the question in the room is what about the effects on  
4 the Fort Condé buildings.

5 DEVEREAUX BEMIS: Since we have most of the  
6 Fort Condé-Charlotte House sitting in front of me.

7 PAT HICKOX: Let me just fast-forward one  
8 slide. You teed it up nicely. Thank you.

9 I want to remind everybody that we're  
10 located in the realigned B project. And there's  
11 advantages to the refinement of this alignment,  
12 especially with regard to the Fort Condé Village.  
13 This shows you the previous B Prime versus the  
14 realigned B Prime on this map.

15 Now, what we've shown here is the closest  
16 potential pile for foundation that could occur on this  
17 project. So let's just take, for example -- if you  
18 remember, this is the bridge overpass coming out of  
19 the Wallace Tunnel. Obviously, as we get further  
20 along in the project, these foundations get further  
21 and further away from Fort Condé as well as the church  
22 and Old City Hall.

23 So what we did is we selected the location

1 of what potentially could be the closest potential  
2 pile or the closest foundation. So this is the  
3 B Prime alignment and the foundations associated with  
4 them shown in green. So what you're seeing in the  
5 circle is the 150-foot radius on distance. So if you  
6 recall in this graph, at 150 feet we anticipate very  
7 little if no risk to damage to any structures.

8 And at 250 feet we've recommended  
9 monitoring. So what I've shown on this chart or this  
10 aerial are two rings. One is the 250-foot distance  
11 shown in yellow and the 150-foot distance shown in  
12 red.

13 So from this you can see we're very far from  
14 nearly all structures associated with the project.  
15 The closest home as measured on this map was 260 feet  
16 away from that foundation there. The church was over  
17 600 feet away. And Old City Hall was over 1,000 feet  
18 away. And that's from this location. Obviously, from  
19 any of the foundations built along B Prime, they're  
20 even further away than that.

21 DEVEREAUX BEMIS: So that top one is not  
22 actually going to be built? Is that what you're  
23 saying?

1 PAT HICKOX: No. It will be built.

2 ALLISON GREGG: So the question from the  
3 room is with regards to the top bull's-eye, so to say,  
4 that is directly below the study, asking if that will  
5 be built.

6 PAT HICKOX: would it be helpful for me to  
7 show the interchange?

8 DEVEREAUX BEMIS: No. I think that's fine.  
9 When you said B Prime down there, I wasn't sure what  
10 that was at the top.

11 PAT HICKOX: Right. So this structure is  
12 part of the interchange reconstruction that will  
13 eliminate these ramps. But there's an overpass here.  
14 Much like there is today, there's an existing overpass  
15 here. It actually goes a little further back so we  
16 can get over onto the local roads. But this location  
17 is identified as the closest potential foundation we  
18 could have to any of the impacted properties.

19 DEVEREAUX BEMIS: It's hard for me to tell,  
20 but on that north one are we there at the fire museum?  
21 Nick, can you tell?

22 PAT HICKOX: That would be here?

23 DEVEREAUX BEMIS: Right in there somewhere.

1 PAT HICKOX: Obviously the fire museum would  
2 be one of those structures we would need to monitor.

3 EDWIN PERRY: Isn't the fire museum at the V  
4 next to the Civic Center?

5 PAT HICKOX: I thought it was right here.  
6 Nick, do I have that right?

7 NICK AMBERGER: No. It's up to the -- put  
8 your arrow -- up, left, left. Right there. Up to the  
9 right. Right there.

10 PAT HICKOX: So it looks like it's 250 feet.  
11 Now, again, that's comparing it to the  
12 largest potential displacement pile that a contractor  
13 could use. He may not choose to use a 36-inch  
14 concrete pile. He may use a much smaller pile or a  
15 nondisplacement file, which means the opportunity for  
16 vibrations would even be less.

17 DEVEREAUX BEMIS: Thank you.

18 PAT HICKOX: Yes, sir.

19 Any other questions about this?

20 (NO RESPONSE.)

21 PAT HICKOX: So how are we going to enforce  
22 this? We have written into the project technical  
23 provisions that we will address how the vibration

1 monitoring and mitigation will be handled during  
2 construction as well as design. Prior to construction  
3 they'll have to submit a vibration monitoring plan  
4 that will be reviewed and approved by ALDOT. There  
5 will be a condition assessment of all potentially  
6 impacted structures, including pre- and post-  
7 construction surveys. And more importantly, there  
8 will be monitoring in place that will measure all  
9 vibrations and allow us to immediately stop monitoring  
10 and immediately stop work and mitigate if thresholds  
11 are exceeded. So we'll know when these vibrations  
12 exceed the thresholds that we have placed within the  
13 technical provisions.

14 ELIZABETH STEVENS: And that will be on the  
15 two tunnels, too?

16 PAT HICKOX: That will be on the two tunnels  
17 as well. So we'll basically have a system such that  
18 it will alarm us if these vibrations and thresholds  
19 have been exceeded, the contractor will be required to  
20 stop work immediately, and then offer up mitigation  
21 methods. And those mitigation methods could be  
22 changing the foundation insulation procedures. It  
23 could be changing the pile types. But he'll have to

1 accommodate all those requirements. And so they're  
2 very aware of how to handle this for the construction  
3 phase of this project.

4 BEVERLY GIBSON: Obviously this hasn't been  
5 planned yet. But, for instance, on Christ Church  
6 Cathedral, there will be something on our campus  
7 measuring the vibration level? Is that part of the  
8 plan?

9 PAT HICKOX: That is correct.

10 ALLISON GREGG: So the question from the  
11 room is assuring that there will be vibration  
12 monitoring done on buildings and structures near the  
13 construction site.

14 PAT HICKOX: ALDOT as well as the contractor  
15 team will need to work closely with you in identifying  
16 those locations. Obviously we don't want to be  
17 obtrusive with the placement of these devices. But  
18 they're fairly small. Basically they're used --  
19 they're seismic devices used to measure earthquakes.  
20 They're very small devices to be placed to alarm us if  
21 any issues occur.

22 BETSY MERRITT: This is Betsy Merritt. Can  
23 I ask a question?

1 ALLISON GREGG: Yes.

2 ELIZABETH MERRITT: What is the threshold at  
3 which the alarm would go off and the vibration monitor  
4 would tell you to stop work, the PPV?

5 PAT HICKOX: Yes. Those thresholds -- and  
6 I'll pull those back up. It's .15, which we  
7 anticipate to be 150 feet.

8 ELIZABETH MERRITT: So you would set the  
9 monitors to alarm you if the vibration levels reach  
10 .15?

11 PAT HICKOX: That's correct.

12 ELIZABETH MERRITT: We would want that  
13 number to be specifically included in the Section 106  
14 agreement, just to be specifically mentioned. Thanks.

15 PAT HICKOX: Okay. Anything else?

16 (NO RESPONSE.)

17 PAT HICKOX: All right. I think we've  
18 addressed the questions. Were there any other ones?

19 (NO RESPONSE.)

20 PAT HICKOX: Now on to aesthetics.  
21 Hopefully everyone here had an opportunity to look  
22 through the aesthetic guideline that we have over  
23 here. If not, feel free to hang around afterwards or

1 during the break -- I guess we won't have another  
2 break -- but afterwards to look at that.

3 So aesthetics. Aesthetics were an  
4 amendment. And to ensure that the project aesthetic  
5 amendments were met, ALDOT and FHWA created an  
6 Aesthetic Steering Committee. And that's comprised of  
7 many of the individuals here today. And this is a  
8 picture of some of the committee members that you can  
9 see there. And this committee, their goal is really  
10 to assist ALDOT and FHWA in the development of  
11 aesthetic guidelines to be used by the proposing  
12 contracting teams. They're also there to ensure that  
13 the aesthetic amendments for the project are going to  
14 be met. They will evaluate a set of packages as we  
15 receive them from the submitting teams to ensure that  
16 they adhere to the guidelines that have been  
17 established for the project and will participate in  
18 meetings with ALDOT and FHWA to provide input on  
19 project components.

20 And you can see some of the components there  
21 that are included in the guidelines as well as the  
22 technical provisions, and they address topics such as  
23 architectural themes, land use, streetscape,



1 landscape, materials and finishes, structures -- and  
2 when I say structures, that's all structure types:  
3 walls, signs, overhead gantries, bridges -- as well as  
4 bike and ped amenities. So they'll be offering up  
5 amenities to the base requirements that we have as  
6 part of the project that I discussed earlier. As well  
7 as lighting. So that's roadway lighting as well as  
8 aesthetic lighting. And I'll be touching on aesthetic  
9 lighting in just a moment.

10 The Aesthetic Steering Committee had three  
11 meetings to establish these guidelines that we've  
12 talked about, with the next meeting being scheduled to  
13 review the proposing team's draft aesthetic plans, to  
14 get their input, to provide them input, the proposing  
15 teams' input, prior to them submitting the final bids  
16 on the project. So the goal being if the committee  
17 felt like the proposing team had not understood the  
18 intent of some of the requirements for the aesthetic  
19 guidelines, they'd have that ability to provide that  
20 input to the proposing teams, allowing them to make  
21 any changes that were necessary.

22 On the lighting, obviously there's lighting  
23 requirements to ensure minimum roadway and bridge

1 lighting criteria for safety. Those will be included.  
2 One of the requirements coming out of this -- or the  
3 needs or the desires coming out of this Consulting  
4 Parties Meeting is the desire to defer selection of  
5 light fixtures until late in design to use the latest  
6 technologies available. We certainly embrace that.  
7 Those light fixtures will not be selected until final  
8 RFP. So obviously we're meeting with the teams.  
9 They're using the latest fixtures and technologies  
10 available at that time.

11 We are making sure that they include  
12 measures to minimize and reduce light spill and to  
13 achieve the commitments from the Draft EIS. And  
14 again, the aesthetic lighting will be included in the  
15 packages from the teams and reviewed by the Steering  
16 Committee.

17 So taking the requirements from the  
18 Aesthetic Steering Guidelines, aesthetic guidelines,  
19 and from the input from the Steering Committee, we've  
20 created a number of renderings that depict what  
21 potentially the bridge could look like from various  
22 locations. This is kind of the end of Broad Street  
23 looking south across the bridge towards the bay. One

1 of the things that I wanted to make sure to note is  
2 that the aesthetic lighting we'll have full ability to  
3 control. In other words, we'll be able to control the  
4 light levels, we'll be able to control the colors, as  
5 well as when they go on and off throughout the  
6 evening. So we'll have full capability to control the  
7 lighting for the project.

8 A view as you enter the Mobile River looking  
9 north towards the City.

10 Okay. Any questions about aesthetics or  
11 lighting?

12 ELIZABETH MERRITT: This is Betsy Merritt.  
13 I have one. Can you tell us about the members of the  
14 Aesthetic Steering Committee? What is their  
15 background and their role? Who are they?

16 PAT HICKOX: The committee was selected  
17 through ALDOT and FHWA. Eight of the nine members, I  
18 think it is, are members of the consulting parties.  
19 So mostly consulting-party involvement.

20 MISSI SHUMER: Betsy, this is Mitzi Shumer.  
21 So we wanted a diverse group in terms of background.  
22 We had a landscape architect, we had an architect, we  
23 had an engineer --

1 PAT HICKOX: Planners.

2 MISSI SHUMER: -- we had planners, and a  
3 couple of local officials. So it was a very diverse  
4 group with a very diverse background in terms of  
5 training and profession.

6 ELIZABETH MERRITT: Okay.

7 PAT HICKOX: Okay. Anything else?

8 (NO RESPONSE.)

9 PAT HICKOX: Okay. A little bit about  
10 landscaping. Obviously that's real important to all  
11 of us. Most of us live in the area. We want to make  
12 sure that the local land use plans are really examined  
13 when the teams develop their proposed solutions. So  
14 those were included as well in the recommendations to  
15 the proposing teams.

16 And for the landscape and management plan,  
17 for those areas within the project limits or ALDOT's  
18 right-of-way, those would be controlled and developed  
19 by the concessionaire, who will also be maintaining  
20 those facilities for the length of the concession.  
21 And those requirements are --

22 ELIZABETH STEVENS: Can you say that again?

23 PAT HICKOX: So because this is a

1 concession, potential concession project -- like  
2 Allison mentioned earlier, this potentially could be a  
3 50- or 55-year concession. So part of that concession  
4 is that the concessionaire has to operate and maintain  
5 the facility during that duration. And that would  
6 include everything within --

7 ELIZABETH STEVENS: Even the earth  
8 underneath it?

9 PAT HICKOX: That's correct. Including the  
10 landscape areas established as part of the project.  
11 Now, there may be areas that are not part of the  
12 project that potentially could be developed later on  
13 by other entities such as the City or the County or  
14 someone else.

15 ELIZABETH STEVENS: Is there a map of that,  
16 what those boundaries are?

17 PAT HICKOX: Yes. In fact, at the end of  
18 the Aesthetic Steering Guide, there's character area  
19 maps. So what those mean, we basically have outlined  
20 those areas for landscaping that we've identified. So  
21 the Steering Committee actually worked with our  
22 architect in defining those -- our landscape architect  
23 in defining those areas. So at the end, if you would

1 like, or at any time you can go look at those. And so  
2 for each part of the project we show character areas  
3 and what the expectations were for the landscaping  
4 within those areas.

5 ELIZABETH STEVENS: So it will be worked  
6 out -- the public's right to use it and all that in  
7 advance for those 50 years, all that will be in the  
8 agreement, I guess? And how it will be managed?

9 MATT ERICKSEN: Yeah, that's correct. It  
10 will be leased.

11 ALLISON GREGG: Can you repeat the question?

12 PAT HICKOX: So the question was the  
13 authorities or the public could use those features on  
14 the project that are on the ALDOT right-of-way.

15 ELIZABETH STEVENS: It will be pre-agreed  
16 to?

17 PAT HICKOX: Right.

18 ELIZABETH STEVENS: Thank you.

19 PAT HICKOX: And the response was yes, those  
20 will be identified, those locations or opportunities.

21 ELIZABETH STEVENS: How would that be  
22 modified if 20 years in the community wants to do  
23 something different?

1           PAT HICKOX: I would assume that ALDOT would  
2 have sole regulatory -- in their agreement with the  
3 concessionaire -- some ability to go to them and offer  
4 up an agreement for whatever facilities that we're  
5 looking for. I'd have to go look. It's a really good  
6 legal question.

7           MATT ERICKSEN: That would really depend on  
8 what the usage was initially and how it was developed.

9           PAT HICKOX: One thing I did want to bring  
10 up, we certainly identified this as a very diverse  
11 project. As you cross this corridor, it has a lot of  
12 characteristics. And we felt like that was important.  
13 So within the aesthetic guidelines, we identified  
14 those character areas that we thought best identified  
15 the character of the project area that it's within.  
16 Obviously the Eastern Shore and the Causeway area and  
17 downtown Mobile, they all have different  
18 characteristics. So we really -- the Steering  
19 Committee worked really hard in helping us identify  
20 what those areas should have and what they should  
21 depict. So I'd invite you to take a look at those.

22           And so the proposing teams will submit their  
23 own plans based on what we're recommending. And then

1 the Steering Committee as well as the landscape  
2 architect will have an opportunity to review and  
3 approve those. And those requirements are outlined in  
4 the technical provisions that are basically the  
5 requirements for the contractors bidding the project.

6 Any more questions about landscaping?

7 DEVEREAUX BEMIS: Devereaux Bemis with  
8 Restore Mobile. At one point they were talking about  
9 landscaping in downtown Mobile to help mitigate the  
10 visual effects. Is that part of this landscaping?

11 ALLISON GREGG: So the question is what  
12 downtown landscaping could be incorporated into the  
13 project.

14 PAT HICKOX: well, I think there's two  
15 answers to that. I think we're going to talk about  
16 tree canopy. First of all, in the character area map,  
17 we have a special area for downtown that has to do  
18 with the creation of that gateway. And there's also a  
19 gateway on the Eastern Shore because the project  
20 starts and ends at two different locations. So we do  
21 have that. I think, Missi, did you --

22 MISSI SHUMER: I was going to say that  
23 segues nicely into the next discussion, which is the



1 tree canopy.

2 So one of the things that became very  
3 apparent through the Viewshed Impact Assessment and  
4 our previous discussions with the consulting parties  
5 was the importance of our existing tree canopy in  
6 shielding and minimizing the visual effects of this  
7 project when you're looking at it from various  
8 locations within historic districts that are really --  
9 in terms of physical proximity, some of them are  
10 further removed than what you may typically think of  
11 in terms of effect.

12 So rather than duplicating the City of  
13 Mobile's efforts in tree canopy and maintaining a tree  
14 canopy and enhancing a tree canopy, ALDOT felt like it  
15 was prudent to partner with the City. And in doing  
16 so, they committed to funding, providing contributions  
17 financially to the City's Right Tree, Right Place  
18 program. And we are fortunate to have Brian  
19 Underwood. He's the deputy director of long-range  
20 planning for the City of Mobile. And he's going to  
21 give us some more insight into how this program, which  
22 is a pretty new program for the City, will help  
23 maintain, improve, and enhance our tree canopy in

1 downtown Mobile.

2 BRIAN UNDERWOOD: Thank you. Brian  
3 Underwood. I'm glad to be here with you this morning.  
4 I want to talk to you about our new landscaping  
5 initiative in the City of Mobile. It's called the  
6 Right Tree, Right Place program.

7 So the Right Tree, Right Place task force  
8 was an initiative of Mayor Stimpson. The task force  
9 was convened several years ago in response to problems  
10 with some of the infrastructure in the City. It is  
11 kind of tied into the City's capital improvement  
12 program. You may be aware the City has allocated 63  
13 million dollars over a three-year window initially to  
14 address needs in the capital infrastructure: streets,  
15 sidewalks, drainage, and other infrastructure.

16 During the planning that has led to the  
17 capital improvements program, the City became aware  
18 that this wonderful live oak canopy has done some  
19 damage to much of the infrastructure. So there are  
20 problems with the built environment based on our  
21 beautiful canopy. The Right Tree, Right Place  
22 committee was convened to address some of these  
23 problems. A primary outcome has been the development

1 of a plant list of appropriate canopy, subcanopy, and  
2 shrub varieties that the City can use to implement  
3 within the City for redevelopment and new development  
4 projects in the local landscape. Currently the City's  
5 codes and ordinances, they primarily recommend only  
6 two varieties, that being the live oak and southern  
7 magnolia. So that kind of results in a limited pallet  
8 of plant opportunities that landscapers can use.

9 The Right Tree, Right Place committee was  
10 composed of members -- local landscapers, landscape  
11 architects, and representatives from the commercial  
12 real estate development community. They met and  
13 helped develop this new list. And currently it  
14 contains over 100 species of trees and shrubs and  
15 palms, et cetera, vines, which can be used to enhance  
16 our local landscape and aesthetics.

17 So what are the problems within the built  
18 environment? Here we see a typical cross-section of  
19 an urban streetscape. Starting on the right you see  
20 the street, there's a curb, there's utility poles, a  
21 sidewalk, and then you transition up into a private  
22 property with homes, et cetera. So we have concerns  
23 currently with trees. Maybe they're too close to the

1 overhead utilities. Maybe the roots are growing down  
2 and destroying underground storm drains, underground  
3 utilities, cracking the curbs, cracking the sidewalks.  
4 So those are some of the considerations that went into  
5 deriving this new Right Tree, Right Place tree list  
6 essentially.

7           The result is -- it's in table form. Again,  
8 it's categorized by the size of the trees. It begins  
9 with your canopy varieties, transitions down to the  
10 medium-sized trees for your subcanopy, and then on  
11 down to your shrubs, and it includes palms. This  
12 table also includes a legend which identifies trees  
13 that are more compatible with powerlines. It  
14 identifies trees and shrubs that are shade tolerant.  
15 It identifies plants which may be flowering, which may  
16 be very desirable to have in your landscape to enhance  
17 the aesthetics.

18           So again, we're currently very limited with  
19 respect to the City's code. It specifies live oaks  
20 and southern magnolias. Very limited. We want to  
21 promote a vibrant, diverse landscape which avoids  
22 negative effects of what you call a monoculture. If  
23 you have too many of the same tree variety, that has

1 been proven to promote disease and other pathogens  
2 which can come in and cause blight and actually do  
3 damage to the varieties. And I believe in Texas there  
4 have been some situations where live oaks have been  
5 attacked by these blight pathogens, and it has  
6 resulted in damage to the tree canopy in some cities  
7 in Texas. So that was considered in the big picture,  
8 to try to promote a more diverse plant pallet for the  
9 City. Also we wanted to encourage production of  
10 additional plant varieties which the local nursery  
11 industry could produce that would again help overall  
12 with our local aesthetics.

13 So the new provisions. The plant list  
14 provides minimum planting area requirements as well  
15 based on the growth form of specific varieties of the  
16 trees and plants. If you want to plant a live oak,  
17 you're going to need a much bigger planting area than  
18 if you want to plant a crepe myrtle, for instance,  
19 just based on the root mass that's associated with the  
20 different varieties.

21 So those parameters are included in the  
22 table, the plant list. There are minimum spacing  
23 requirements stated for -- if you want to plant

1 multiple trees in a row, there's a spacing distance  
2 that must be met to make sure the roots don't  
3 intertwine underground and to make sure the canopies  
4 don't compete with each other at the top. So that was  
5 included in the new table plant list.

6           Upon adoption, the Right Tree, Right Place  
7 plant list will be administered through the zoning  
8 code. As you may be aware, the City is in the middle  
9 of a new zoning code overhaul to replace the old code  
10 which was developed in the 1960s. So this is going to  
11 be incorporated into the Unified Development Code  
12 Article 4, which is the development standards section  
13 of the new code, and will ultimately be administered  
14 through the Planning and Zoning Department.

15           Here's an example. You have your southern  
16 live oak, *quercus virginiana*. Upon reaching its  
17 maximum height, it may have a crown spread or canopy  
18 spread of 120 feet and the canopy height can be 80  
19 feet. So imagine we have these in our City along the  
20 right-of-way. You see the little exhibit on the  
21 bottom. You can see the root structure is almost as  
22 great in a typical tree as the canopy spread.

23           So again, over the years, our live oaks

1 within the City have done great damage based on the  
2 encroachment of the roots within our infrastructure.

3 So the Right Tree, Right Place list  
4 stipulates -- if you want to plant a live oak, that's  
5 perfectly fine. But you've got to incorporate an area  
6 with a minimum of 204 square feet of open ground  
7 horizontally to be able to plant a live oak in your  
8 development plan, on your landscape plan. And also  
9 the minimum dimension from, say, a sidewalk and a curb  
10 has to be a minimum of 15 feet -- so that would be  
11 seven and a half feet either side of the trunk -- in  
12 order to prevent damage to the hardscape that would be  
13 adjacent to the live oak. And a minimum spacing, if  
14 you place multiple live oaks in a row, that minimum  
15 spacing would be 48 feet.

16 So for every plant on this list it contains  
17 these same parameters to help guide the landscapers  
18 and the landscape architects as they're devising new  
19 plans for project sites.

20 Okay. One interesting component of the  
21 list, it contemplates a plant unit. These provisions  
22 are also found in Article 4 of our new Unified  
23 Development Code. When we adopt the code this fall,

1 the plant unit will allow groupings of different types  
2 of plants. If you wish, you could have a cluster of  
3 palms or you could have a single tree. And, for  
4 instance, our live oak tree has a point value of 1.  
5 So if the code requires one plant unit, you can plant  
6 one palm -- excuse me -- live oak. Where a palm has a  
7 unit count of .3, you would have to plant a cluster of  
8 three palm trees to equate to one plant unit. And  
9 again, these provisions are going to be reflected in  
10 our new Unified Development Code.

11 The crepe myrtle is another tree, it has a  
12 point value of .3. Camellias, which everyone likes,  
13 they're beautiful flowering, that has a point value of  
14 .5. So it would take two camellias to be planted to  
15 equal a value of one.

16 Again, this is coming to our Tree Bank  
17 provisions. This is currently in the code. It's  
18 enabled through the zoning ordinance section 64-4. It  
19 allows for developers to contribute to a -- provide a  
20 payment to compensate for trees which may be destroyed  
21 or removed through development. It is managed through  
22 the Mobile Tree Commission. And again, it governs  
23 tree removals on private property during development.



1 And it establishes the Tree Planting & Preservation  
2 Trust Fund, also known as the Tree Bank. And  
3 currently the Mobile Tree Commission's standing rules  
4 provide protection for trees within streets  
5 rights-of-way.

6 ELIZABETH STEVENS: what does that mean?

7 BRIAN UNDERWOOD: Ma'am?

8 ELIZABETH STEVENS: what does that mean,  
9 quote, unquote, "standing rules"? what does that  
10 mean?

11 BRIAN UNDERWOOD: That's the policy document  
12 that's adopted for the Mobile Tree Commission, kind of  
13 their policies that they implement during their  
14 meetings.

15 ELIZABETH STEVENS: So it's not in the --

16 BRIAN UNDERWOOD: It's not in the City  
17 zoning code, no, ma'am. It's kind of their policies  
18 that they developed to help regulate the protection  
19 for the City.

20 So through the work of the Mobile Tree  
21 Commission, trees which may be approved for removal  
22 can be mitigated, as I mentioned earlier, through  
23 replanting in a different location, say like in a park

1 or within a right-of-way, or through donation, payment  
2 of a donation into this trust fund or the Tree Bank.  
3 And the standing rules provide mitigation at a rate of  
4 \$200 per tree if an individual or a business entity is  
5 requesting the removal of the tree or it can be \$90  
6 per tree if a local or state or county agency is  
7 enacting the removal of the tree which requires  
8 mitigation.

9 And I wanted to mention the Tree Commission  
10 is represented by a City employee. That's the urban  
11 forester. His name is Peter Toler. He sits on the  
12 Mobile Tree Commission to help manage the tree  
13 protection standards.

14 Are there any questions?

15 MARY LEE MONTGOMERY: So this all applies to  
16 new plantings; correct?

17 BRIAN UNDERWOOD: Correct.

18 ALLISON GREGG: So the question in the room  
19 is asking, verifying that the presentation we just  
20 gave is in regards to new plantings.

21 BRIAN UNDERWOOD: The Unified Development  
22 Code is going to administer the Right Tree, Right  
23 Place list. So it has not been adopted yet. It's

1 been developed over the past two years. But again,  
2 we're expecting to get the new Unified Development  
3 Code or zoning code adopted by the City Council  
4 hopefully by this fall. And at that point in time the  
5 new plant list would take effect.

6 MARY LEE MONTGOMERY: So there are no plans  
7 to remove trees that are like breaking up sidewalks  
8 and hardscape?

9 BRIAN UNDERWOOD: If there are kind of  
10 incidental removals that would be associated with the  
11 Capital Improvement Program implementation, I believe  
12 that that would happen just based on the need to  
13 enhance and replace and repair the infrastructure. So  
14 that could happen. And we would hope that the huge  
15 live oaks would be replaced with varieties that would  
16 be less detrimental over the long haul to that  
17 infrastructure once it's replaced.

18 DEVEREAUX BEMIS: Deveraux Bemis again with  
19 Restore Mobile. Doesn't that mean eventually we would  
20 -- it may be 50 or 100 years -- but we would lose our  
21 oak tree canopy that actually covers the streets? So  
22 we would lose our shady Government Street, Springhill  
23 Avenue?

1 BRIAN UNDERWOOD: The intent is not to  
2 destroy the live oak canopy but simply to require the  
3 appropriate planting area for replacement live oaks.  
4 Now, if our streetscape is not designed to incorporate  
5 those live oaks, then ultimately you could see a  
6 reduction. But we would like to -- Mobile is known  
7 for the live oak canopy. We want to promote that in  
8 an effective way by providing the right square footage  
9 to plant a new live oak. I mean it's complicated.

10 NICK AMBERGER: In many of those corridors  
11 you talked about, the reason those trees have thrived  
12 so well is because that square footage exists. So  
13 it's very easy to replace those.

14 DEVEREAUX BEMIS: I was just thinking on  
15 Government Street eventually they might die. And  
16 there's not 15 feet between the curb and the sidewalk.

17 NICK AMBERGER: In most places there are.  
18 It's in those residential streets where there's three  
19 feet or four feet and the live oak has been planted on  
20 top of a drainage structure, and the two just can't  
21 coexist.

22 ALLISON GREGG: So the discussion in the  
23 room is with regards to the live oaks that are

1 currently planted and the future of those as well.

2 BRIAN UNDERWOOD: Any more questions?

3 (NO RESPONSE.)

4 BRIAN UNDERWOOD: Thank you.

5 MISSI SHUMER: All right. So our next topic  
6 is the USS Alabama Memorial Battleship Park. And our  
7 consultation today has really centered around two  
8 primary concerns at the park, and that is access,  
9 existing and proposed to the park, as well as signage  
10 that directs the travelers along the I-10 corridor and  
11 along the local roadway system to the park.

12 So we've had a couple of recent meetings  
13 since we last met. One was on March 11th, 2016, and  
14 the other was on April 21st, 2017.

15 In terms of access, we looked at a variety  
16 of options to reconfigure access or to provide more  
17 direct access to the park. And this included things  
18 such as relocating the park's entrance to another  
19 location to try to give us room to give them a direct  
20 access or direct ramp from our facility. And none of  
21 the options that we were able to fit within this very  
22 tight area were able to meet design criteria for a  
23 safe roadway.

1           So our current plan is to leave access to  
2 the park as it currently exists. So there will be no  
3 changes to the Battleship Park entrance or access from  
4 the roadway network.

5           We are, however, preparing -- we are  
6 preparing a preliminary signage plan. And this  
7 signage plan will continue to evolve as we go through  
8 the design and construction and post construction. As  
9 you can probably imagine from driving along the I-10  
10 corridor, there are a whole lot of signs that have to  
11 be put along the route. And so we're still in the  
12 process of finalizing where some of these supplemental  
13 signs will be located. So we will have additional  
14 signs for the Battleship Park, and they will be  
15 located along the corridor in both directions. So  
16 travelers will have plenty of advance notice for how  
17 to get off, where to exit to get to the battleship,  
18 and they should be able to see the battleship from a  
19 pretty long way away given our high-level approaches  
20 and the bridge. So hopefully between the signs and  
21 being able to see it long before they have to get off,  
22 we will improve and mitigate those concerns related to  
23 access.

1           Again, we will continue to coordinate as we  
2 go through design and as we go through construction  
3 and even during post-construction activities to make  
4 sure that our access to the park is maintained and not  
5 adversely affected.

6           Are there any questions?

7           ELIZABETH STEVENS: I just have a comment.  
8 I'm glad you're showing the brown historic signs. It  
9 helps differentiate them from all the other green-sign  
10 clutter that's out there. And I would ask that you  
11 think about the same for the historic district signs,  
12 the downtown Mobile signs, all in the corridor of this  
13 study area.

14           ALLISON GREGG: So the comment in the room  
15 is just to possibly broaden the signage plan to other  
16 attractions and points of interest in the area.

17           MISSI SHUMER: Thank you. I was just making  
18 notes while you were talking. That's a valid point.  
19 We will see what we can do.

20           Anything else on the battleship or signage?

21           (NO RESPONSE.)

22           MISSI SHUMER: So since we last met, we had  
23 a couple of updates on historic structures. The first

1 one is the Union Hall, which was the only structure  
2 that had the potential for a direct impact related to  
3 this project. The property owner tore down the Union  
4 Hall not too long ago. I guess it's been about a year  
5 or so. Fortunately the University of South Alabama  
6 during their Historic Standing Structures Survey as  
7 part of this project prepared a vast amount of  
8 historic documentation on this building. So it is  
9 well documented in its original use and as it  
10 deteriorated. But that's an update and that's been  
11 coordinated and confirmed with the State Historic  
12 Preservation officer.

13 we also prepared -- when we were looking at  
14 bike/ped practices, one of them was to maybe repurpose  
15 the Bankhead Tunnel as a permanent bike/ped route.  
16 And so in consideration of this, we prepared a  
17 historic standing structures report for the Government  
18 Street/U.S. 90 corridor in downtown Mobile. That has  
19 also been put out for consulting party review and  
20 coordinated with the State Historic Preservation  
21 officer.

22 Archeology. So we've conducted -- the  
23 University of South Alabama has conducted numerous



1 phase 1, phase 2, and phase 3 surveys throughout the  
2 corridor. All of the surveys that have been conducted  
3 to date have been concurred with by the State Historic  
4 Preservation officer, and they have been provided to  
5 the consulting parties and the tribes for reviews and  
6 comments and information.

7           There were several parcels along the  
8 corridor that were not previously surveyed because we  
9 maybe didn't have access from the property owners or  
10 various other conditions. They may have buildings on  
11 them, they may have parking lots, things that made it  
12 very difficult for us to go out and do shovel testing  
13 or trenching or whatever else needs to be done. So we  
14 are currently in the process of conducting and  
15 continuing our archaeological surveys on those  
16 properties that have not previously been accessible.  
17 We will continue to provide the findings of those  
18 surveys as they are available. So those will be made  
19 available to the consulting parties and the tribes as  
20 we have the results and management summaries from  
21 those surveys.

22           So this map that's up, this is the Virginia  
23 Street area. You can see the Texas Street Recreation

1 Center area. The area in the light gray/light blue  
2 has been cleared and does not require additional  
3 archaeological surveys. The area in the  
4 yellow/orangish color, those are parcels that are  
5 going to require additional surveys that we are  
6 continuing to perform now. And it's hard to see even  
7 though we changed the color. There's some little blue  
8 squares along the map. Those are approximate pier  
9 locations for the high-level approaches as we lead up  
10 to the main span. It just kind of gives you an idea  
11 of where some of our impacts could be located. All of  
12 the pier locations through here appear to be --  
13 currently proposed are within the existing  
14 right-of-way.

15 As we move closer to the river, you can see  
16 an extensive amount of archaeological survey has been  
17 done in the west tunnel interchange as part of what  
18 was originally a separate project. The orange again,  
19 yellow-orange, are areas that need additional  
20 surveying. So we're going to be out doing some  
21 additional work in these areas that you see close to  
22 the river.

23 The purple squares that you see, these are

1 the anchor piers, which are a little bit further  
2 removed from the river. And these are the ones that  
3 are closest to the river on either side. Those are  
4 the main towers that will support the cable-stayed  
5 structure.

6 This is the east tunnel. It gives you just  
7 a view of what we'll be doing, some limited  
8 archaeological work on the east side of the river  
9 where the high-level approach is coming down from the  
10 bridge.

11 And then we have the Daphne interchange.  
12 And the area that's shown in the yellow-orange is all  
13 owned currently by the Alabama Department of  
14 Transportation. But we're going to go out and do some  
15 additional archaeological investigations to see what's  
16 still there, if anything, and make sure that  
17 everything is fully documented in accordance with  
18 federal regulations.

19 Questions?

20 DEVEREAUX BEMIS: Devereaux Bemis again.  
21 You mentioned the study on Government Street. Is that  
22 out?

23 MISSI SHUMER: It is. And actually we just

1 put it on our website, mobileriverbridge.com.

2 DEVEREAUX BEMIS: Okay.

3 MISSI SHUMER: And underneath the graphic  
4 that's at the top of the page there's a link to both  
5 the 2015 vibration study that Pat talked about earlier  
6 and the Government Street Historic Standing Structure  
7 Survey report, and the SHPO letter associated with  
8 that report.

9 DEVEREAUX BEMIS: All right. And then since  
10 the bridge is going to be raised, I think you said,  
11 eight feet, is that going to get outside of the  
12 corridor that it was originally done in? And if so,  
13 will that require more archeology or environmental  
14 study?

15 MISSI SHUMER: So the question for those of  
16 you on the phone -- you're talking about the Bayway?

17 DEVEREAUX BEMIS: The Bayway, yes.

18 MISSI SHUMER: So we're elevating the Bayway  
19 due to storm surge concerns. And the question was  
20 will the Bayway still be constructed within the  
21 previously disturbed construction limit or will we  
22 have to go outside of it and will we have to do  
23 additional archaeological surveys?

1           So the answer to that is the Bayway will be  
2 constructed between the existing bridges. So we will  
3 be using the previously disturbed construction  
4 channel. We will not be going outside of the existing  
5 footprint. So we will not need to do any additional  
6 underwater archaeological surveys.

7           DEVEREAUX BEMIS: Thank you.

8           ELIZABETH STEVENS: And then what will  
9 happen with the existing Bayway?

10          MISSI SHUMER: The question is what will  
11 happen with the existing Bayway. Matt?

12          MATT ERICKSEN: So the plan for the existing  
13 Bayway as it is now is to be fully replaced. So it  
14 will be totally removed and the new Bayway eight feet  
15 higher, approximately four lanes in each direction, is  
16 replacing the existing, which is approaching 50 years  
17 old, so we're starting to have some maintenance issues  
18 with it.

19          MISSI SHUMER: Yeah. And we are, as Pat  
20 mentioned, we are coordinating with the Coast Guard,  
21 and we will have to have bridge permits, obviously, to  
22 build this bridge. But they have some stipulations  
23 and regulations on what they will allow if you replace

1 a bridge. And they don't typically -- my  
2 understanding is they don't typically like you to  
3 leave an old bridge in place that could damage your  
4 new bridge. So we're working with them to make sure  
5 that everything is in accordance with their  
6 requirements. And we have a representative from the  
7 Coast Guard here today.

8 GERI ROBINSON: If it's a historic  
9 structure, it just depends on SHPO and all of that.  
10 If it's not historic, of course, or if won't be  
11 maintained in the long run, it's probably best to take  
12 it down. The issues that we find when having bridges  
13 that remain in place is that the long-term care and  
14 maintenance of the bridge is not provided for and it  
15 becomes a problem in the long run. If it's historic,  
16 all bets are off.

17 ALLISON GREGG: I have a question online.  
18 Have any archaeological studies been conducted on the  
19 Union Hall site? On a recent visit there were  
20 architectural remnants on the surface.

21 MISSI SHUMER: Bill or Bonnie, do you want  
22 to --

23 BILL TURNER: I'm sorry. I didn't hear the

1 question.

2 ALLISON GREGG: Have there been any  
3 archaeological surveys conducted on the Union Hall  
4 site?

5 BILL TURNER: It was done -- well, the  
6 answer is yes. And there's still a great deal more to  
7 be done. So we know from the work that South Alabama  
8 did -- excuse me. This is Bill Turner with ALDOT. We  
9 know from the previous work that South Alabama did  
10 when it was a private owner -- and that was for a  
11 completely different purpose -- that there are  
12 preserved archaeological deposits there, and that's  
13 very close to where one of the anchor piers will be.  
14 So there will be additional archaeological work  
15 conducted on there when we have access to do so. And  
16 currently that access is not available. But it will  
17 be -- the archeology will be addressed.

18 ELIZABETH MERRITT: This is Betsy Merritt.  
19 Can I ask a question about the historic structures and  
20 archeology topic?

21 ALLISON GREGG: Yes.

22 ELIZABETH MERRITT: I'm interested to hear  
23 more about the circumstances of the demolition of

1 Union Hall. There's a question in my mind as to  
2 whether it might be anticipatory demolition. And I'm  
3 interested to learn more. We could follow up after  
4 this meeting. But I'm thinking that I'd like to see  
5 an investigation into the circumstances in order to  
6 determine whether there was an element of anticipatory  
7 demolition involved. I'm very concerned about the  
8 timing.

9           MISSI SHUMER: I mean I think we can follow  
10 up. Mark, do you want to --

11           MARK BARTLETT: Betsy, this is Mark  
12 Bartlett. We were not aware of the owner's desire to  
13 remove that building at the time. And it came as  
14 quite a surprise to us. Our project limits never  
15 actually went into the site where the building was  
16 located. We are still not in that area. So it's  
17 outside of our project footprint as it stands today,  
18 and we're not intending to be in that parcel. So we  
19 were just as surprised as most.

20           ELIZABETH MERRITT: So you're not acquiring  
21 any portion of the owner's property?

22           MARK BARTLETT: I think we took a corner  
23 clip off of that block; is that right?



1           STEPHANIE DRAGOTTA: I believe we are  
2 purchasing that whole area.

3           MARK BARTLETT: The whole block is now being  
4 purchased but not really for the use of the bridge.  
5 It's just an entire block.

6           EDWIN PERRY: Correct. The block where the  
7 Union Hall sat is not really in the bridge footprint,  
8 but we've worked on purchasing the entire parcel from  
9 the property owner. And from what we were told --

10          ELIZABETH MERRITT: I'm not really hearing  
11 this current answer.

12          MARK BARTLETT: Do you want to move up  
13 front? This is Edwin Perry, ALDOT.

14          EDWIN PERRY: Yes. From what the bridge  
15 footprint -- we're not impacting the square area where  
16 the Union Hall was. But we've worked out with the  
17 property owner to acquire the whole property that they  
18 owned, which included the area where the Union Hall  
19 was. Because a majority of the remainder property we  
20 needed as part of building the high-level approach on  
21 that side.

22                 When we were speaking to the property owner,  
23 he told us he tore it down as part of safety reasons

1 for the integrity of the structure and then people  
2 trying to access -- or homeless people trying to  
3 access the property to stay in.

4 ELIZABETH MERRITT: Well, I guess the answer  
5 exacerbates rather than alleviates my concerns.  
6 Because it would certainly be more convenient for you  
7 if the historic structure was, you know, somehow gone  
8 so you don't have to deal with the adverse effect of  
9 it. And I'm just -- the circumstances raise questions  
10 that I think need to be looked into a little more  
11 carefully. Because you acquired the whole parcel.  
12 It's not like this is an owner who's sort of doing  
13 some independent redevelopment plan.

14 So I guess I would be interested in a little  
15 more followup on that. I just think the timing is  
16 troublesome. And the way these things usually work is  
17 there's kind of an unspoken understanding that it  
18 would be convenient or helpful to be rid of a nuisance  
19 like a historic property.

20 So anyway, perhaps we could follow up more  
21 on that later. I just think that the circumstances of  
22 the demolition do raise a question because you would  
23 have -- you know, the question is not whether you

1 demolished it but whether we had the legal authority  
2 to prevent it. And we certainly would have in this  
3 case. So let's pursue that more after this meeting is  
4 over.

5 MARK BARTLETT: Yeah. We can have further  
6 conversations, Betsy, on that issue. Like I say, it  
7 was not a required right-of-way take for the preferred  
8 alignment. But we can definitely have some more  
9 conversation.

10 ELIZABETH MERRITT: Okay. Thank you.

11 MISSI SHUMER: Any other questions on  
12 historic structures or archeology?

13 (NO RESPONSE.)

14 MISSI SHUMER: All right. With that, Matt  
15 is going to give us an overview of the next steps in  
16 the 106 process and the project as a whole.

17 MATT ERICKSEN: Okay, Missi. So this brings  
18 us to the final part of our meeting today, what are  
19 the next steps for section 106. And then I'll get  
20 into what is our anticipated schedule.

21 So leading up to this meeting, all the  
22 consulting parties should have received a Draft MOA.  
23 So at the conclusion of this meeting we would ask that

1 you would provide your comments on the Draft MOA by  
2 June 8th of this year. Upon receiving comments, ALDOT  
3 and Federal Highway will update the Draft MOA to  
4 address your comments. And then, moving forward, we  
5 will continue with our phase 1 and phase 2  
6 archaeological surveys. We will update the Draft MOA  
7 for archaeological findings. And we will continue to  
8 circulate the Draft MOA to the consulting parties  
9 based on the findings. We will then finalize the MOA  
10 along with our Record of Decision around that time,  
11 and we will continue the 106 consultation through the  
12 design and construction period.

13 So moving forward, ALDOT's anticipated  
14 project milestones, we, as was previously mentioned,  
15 have short-listed our concessionaires to three teams.  
16 That was done in February of this year. We are  
17 currently in an industry review period where we're  
18 having one-on-one meetings with the three short-listed  
19 concessionaires where they are reviewing our draft  
20 documents and giving us some input. And that process  
21 will continue now through -- the industry review  
22 period will continue now all the way up through the  
23 issuance of our final request for proposal.

1           We anticipate a Supplemental Draft EIS  
2 approval in the fall of this year. We will have  
3 another public hearing in Mobile and Baldwin Counties,  
4 as we did before. And we also anticipate that to be  
5 in the fall of this year. We think that we can have  
6 our final EIS and ROD approval. We're anticipating  
7 that to be the end of 2018. And once that happens, we  
8 will be able to issue our final request for proposal  
9 to our short-listed teams, accept their proposals and  
10 score them, evaluate them, and make our selection.  
11 And we anticipate a proposer selection for this  
12 project in June of 2019. At that time, once a  
13 proposer is selected, they will go through a  
14 commercial close and a financial close, similar to  
15 when you're buying a house. For a project this size,  
16 that's a pretty extended period that they have to take  
17 on with their lenders. And that should be around a  
18 five- to six-month process. And so we would  
19 anticipate starting construction on this project the  
20 end of 2019.

21           So any questions?

22           ELIZABETH STEVENS: where will that be?  
23 will it be like at one end and work to the other? Or

1 will it work from two sides to the middle?

2 MATT ERICKSEN: So that would be -- means  
3 and methods would be on the concessionaire. On this  
4 project, it being a toll revenue risk, they can't  
5 start making their debt payments and making any money  
6 until the tolls can be collected. So they would come  
7 in like gangbusters and work anyplace available.

8 Okay. With that, I'll turn it back over to  
9 Allison.

10 ALLISON GREGG: So what we would like to do  
11 now is -- we only have two folks who signed up to  
12 provide comments. So we'd like to offer Herndon and  
13 Devereaux three minutes to offer comments and then --

14 HERNDON INGE: Three minutes? I wasn't told  
15 three minutes. I wasn't told three minutes.

16 ALLISON GREGG: Okay.

17 HERNDON INGE: Okay? I wasn't told three  
18 minutes. Vince didn't tell me three minutes. Nobody  
19 told me three minutes. This is a discussion stage. I  
20 was not told three minutes. And I have more comments  
21 than three minutes.

22 ALLISON GREGG: Well, I want to make sure  
23 that we're respectful of everybody's time and make

1 sure that Devereaux also has a chance to speak.

2 HERNDON INGE: Fine. But Vince told me that  
3 this was my opportunity to discuss.

4 ALLISON GREGG: Okay. Great. Are you  
5 comfortable standing there --

6 HERNDON INGE: I would.

7 ALLISON GREGG: -- or do you want to come up  
8 here?

9 HERNDON INGE: The 500-pound gorilla in the  
10 room that nobody wants to discuss is the route of the  
11 Mobile River crossing that was selected which will  
12 change the focus of downtown Mobile and of two  
13 historic districts. In my 35 or 40 years of being an  
14 activist for the citizens of downtown and the one --  
15 several of us who have expressed our concerns against  
16 the headstrong highway planning, I'm reminded of a  
17 meeting when the I-210 connector was proposed right  
18 under what is now the Riverview Plaza -- I mean the  
19 Riverview Hotel. Several of us -- it was owned by  
20 Stauffer then. And several of us flew up to the  
21 highway director's office, Ray Bass, in Montgomery,  
22 and we said: we in Mobile, we're not for this I-210  
23 connector.

1           And Ray Bass said -- he said: Let me get  
2 something straight. He said: Go look at the sign on  
3 my door. He said it says: Highway director. He  
4 said: My job is the bigger the highway, the bigger  
5 the bridge, the more it costs, that's what I'm for.  
6 He said: I don't care whether you citizens want this  
7 I-210 connector or not. That's not my job.

8           And so I'll admit Vince is not the same way.  
9 But that's what's ringing in my ears after 40 years of  
10 talking about our downtown and historic neighborhoods.

11           It's been my experience over these years  
12 that in highway planning, at least in Alabama, the  
13 three -- highway planning has three Ds: decide, then  
14 design, then defend.

15           And so the corridor was chosen a long time  
16 ago, and that's the gorilla in the room which could be  
17 solved.

18           The selected route, the B prime, ignores half  
19 a century of FHWA reports and studies. These studies,  
20 which will become part of the Environmental Impact  
21 Statement, are not new. The damage to an urban area,  
22 a downtown area, is fragile. And everybody agrees  
23 that Mobile needs a Mobile River crossing. But the



1 Chamber of Commerce -- and I've talked to the  
2 presidents of the Chamber over the last 20 years. The  
3 Chamber, just like Sandy Stimpson said, is to improve  
4 business commerce. And the Highway Department is  
5 based on economic studies.

6 well, what I'm concerned about is the people  
7 that live and tour and shop downtown. Stop the Bridge  
8 is not opposed to the Mobile River crossing, but Stop  
9 the Bridge focuses on people.

10 So with the B Prime, the focus of  
11 downtown -- the focus of downtown and Church Street  
12 East will now be a bridge approximately the same  
13 height as the Golden Gate Bridge, not our bay, our  
14 harbor, our Mobile River and our Mobile delta.

15 Stop the Bridge -- if the location of  
16 routing of the bridge stops people from investing and  
17 renting in Church Street East and Detonti Square, if  
18 it stops tourists from coming to our downtown which is  
19 so fragile -- and Ms. Stevens has worked so hard and  
20 accomplished so much in our downtown. We remember  
21 what Mobile's downtown used to look like. If it's  
22 discouraged by the routing of the bridge, then it  
23 should be reconsidered.

1           The noise mitigation, which I'll go over in  
2 a minute -- this book was under the scrutiny of the  
3 federal court 33 years ago. When I-210 was proposed  
4 to cut Mobile's downtown off from its riverfront.  
5 (Indicating.) And this will become part of the  
6 Environmental Impact Statement as one example. There  
7 are other publications which will also be -- were  
8 scrutinized in 1983 by the federal court system and  
9 will be scrutinized in 2020 by the federal court  
10 system.

11           One example is noise mitigation. And I want  
12 these nice ladies to listen about noise mitigation.  
13 In 1968 the United States Department of Transportation  
14 said that noise from a high-speed interstate through a  
15 central business district is a serious problem. And  
16 the way to mitigate it is to install double and  
17 triple-paned glass within eight blocks.

18           Now, with I-210, that was all of downtown  
19 Mobile. So every window in downtown Mobile needed to  
20 be removed and replaced with triple-paned glass -- no,  
21 not triple pane, triple glazed. Apparently that means  
22 with an air space in between.

23           So this will again be scrutinized by the

1 federal court.

2 In 1968 the U.S. Department of  
3 Transportation did a study called The Freeway in the  
4 City. Page 8: "Highway transportation cannot be  
5 allowed to function apart from or in conflict with its  
6 environment. The potential of conflict between a  
7 highway and personal and community aspirations such as  
8 natural beauty, preservation of historic sites,  
9 cleaner air, general community are greatest in  
10 America's densely populated urban areas."

11 1968. That's been ignored by the route of  
12 B Prime.

13 On page 9: "How does the Department of  
14 Transportation provide needed mobility and at the same  
15 time consider other important social goals such as  
16 preservation of historic sites."

17 Page 33: "Urban highways should be so  
18 located and designed as to enhance rather than destroy  
19 a city's best attributes."

20 Mobile was founded on a river and a bay and  
21 a delta, not the Golden Gate Bridge.

22 Page 34: "Highways should not encroach upon  
23 parkland, playgrounds, squares, plazas, or other

1 open-space preserves."

2 Page 35: "Highway location and design must  
3 consider the viewpoint of the area residents."

4 Page 38: "Beauty in highway design is a  
5 result of the sum total of carefully planned and  
6 sensitively handled elements."

7 Page 40: "Highways entering the city should  
8 by their location and design present its city in its  
9 most interesting light."

10 1973, by the United States Department of  
11 Transportation: "when it's necessary to locate a  
12 freeway or other high-volume facility adjacent to a  
13 residential area, sufficient right-of-way width should  
14 be required for appropriate landscape development."

15 This will also be a part of the  
16 Environmental Impact Statement.

17 As an example, talk about -- you know, talk  
18 about what will be the focus of Mobile? what will be  
19 the focus of the people on the south and east sides of  
20 the Riverview Hotel and the Admiral Semmes Hotel and  
21 the First National Bank Building and the RSA Tower?  
22 It will not be our beautiful bay and our beautiful  
23 delta and our beautiful Mobile River and shipping. It

1 will be the Golden Gate Bridge.

2 Section 106 requires the consideration of  
3 both direct and indirect adverse impact. That's 36  
4 CFR, section 816. In the Cape Wind Energy Project in  
5 Nantucket Sound, the Highway Department said there  
6 would be no direct impact. But the federal court  
7 said: "If a visual intrusion will diminish the core  
8 significance of a national historic landmark or would  
9 radically change the feature of the setting that are  
10 vital to defining the character of the place, then the  
11 U.S. Park Service is likely to find the visual  
12 intrusion has a direct and adverse impact on the  
13 historic landmark."

14 So the visual impact of our downtown and two  
15 historic neighborhoods will be the Golden Gate Bridge.

16 So the solution would be to change the  
17 planned route from Alt B so the Mobile River crossing  
18 is farther from historic structures and farther from  
19 historic neighborhoods and farther from downtown.  
20 That solves most of the objections, like the  
21 Africatown Bridge, the Charleston bridge, the Savannah  
22 bridges. Move them away from towering directly over  
23 our historic neighborhoods and historic homes in our

1 downtown.

2 I was present with many of the viewshed --  
3 when the viewshed was looked at. And so many of the  
4 viewshed responses in the CD that was provided,  
5 they're all subjective.

6 Church Street East, it says the impact will  
7 be average. Church Street East? Well, Ms. Stevens'  
8 house and my house face east. We will be looking at  
9 the bridge. I don't say that the impact for our  
10 houses, my office and her house, I don't say it's  
11 going to be average. But that's what the engineers  
12 said, average. They said Detonti Square, average.

13 Government Street Presbyterian Church --  
14 when you come out of Government Street Presbyterian  
15 Church and get ready to walk down the steps, what you  
16 will see is the bridge superstructure. And they call  
17 it minimal impact. The consultant says minimal.

18 The Admiral Semmes Hotel, they said it's a  
19 moderate impact. The Battle House, minimal. Well,  
20 the rooms on the south and the rooms on the east, that  
21 won't be minimal. But in their subjective opinion, it  
22 was minimal.

23 Christ Church Cathedral, they said the

1 impact will be minimal when it will be -- it will  
2 dominate the sky over Christ Church Cathedral.

3 Fort Condé House -- now, listen to this,  
4 ladies -- it said the viewshed would be moderate. So  
5 when you walk out of Fort Condé-Charlotte House, it  
6 will be -- it will dominate the sky. And in the  
7 expert's opinion, the engineer's opinion, it's  
8 moderate.

9 So the mitigation, they talk about \$50,000  
10 to plant oak trees. In my opinion, that is not going  
11 to obscure the two towers that are 550 feet high. Oak  
12 trees which take 20 years -- 30 years to mature, it  
13 says that they will soften the visual effect of the  
14 bridge? That doesn't solve the problem.

15 The question before the federal court, which  
16 has been ignored for the last 15 years, is: Is there  
17 really no feasible, prudent alternative like section  
18 4(f) requires? Has Alabama Department of  
19 Transportation really made a good-faith objectivity in  
20 selecting the route like section 4332 requires in good  
21 faith and like NEPA requires?

22 The 50 years of highway planning is  
23 unanimous, that a high-speed interstate highway into

1 or adjacent to a central business district can be  
2 death. It is certainly not the only reason for the  
3 death of a downtown, but it could be a substantial --  
4 it can be substantial.

5 One more thing and I'll sit down. The  
6 economic disinvestment, the splash -- I think it's  
7 called the splash corridor, the dead zones under the  
8 highway, you know, the separation of one section --  
9 one neighborhood from another neighborhood, those  
10 cannot be lightly considered and say we've got to have  
11 B Prime. I don't anticipate that -- it's like I told  
12 somebody several years ago: If we want to build -- if  
13 it's going to take 10 years to construct the bridge  
14 and 10 years to litigate over the beginning of the  
15 construction of the bridge, then that defers the  
16 Mobile River crossing an extra 10 years. Or we can  
17 sit down at the table and say, wait a minute, there's  
18 some legitimate concerns of downtown Mobile and the  
19 residents of two historic districts, and therefore we  
20 need to reconsider the route. If it can be done at  
21 the negotiating table, that's fine. But we also have  
22 the federal courts.

23 ALLISON GREGG: Thank you. Mr. Bemis?



1           DEVEREAUX BEMIS: Sure. I'm Devereaux  
2 Bemis. As I said, I'm representing Restore Mobile.  
3 We are a small revolving fund, historic fund. And as  
4 this project has gone on for two decades now, so we  
5 have changed. And whereas we were working in Midtown  
6 when this project was beginning, we have now crept  
7 solely south. We're currently working in south  
8 Oakleigh, but we're looking at Oakdale.

9           And the way the road is run, we're getting  
10 closer and closer to where this bridge begins on the  
11 western side. And our concerns are getting more and  
12 more valid, I think.

13           I would remind you that in some of the  
14 letters that have been written and in some of the  
15 meetings we've been talking about what happens  
16 underneath the bridge for the aesthetics committee is  
17 still important to us and how it connects and doesn't  
18 separate us from our waterfront. We would like to see  
19 that to be an amenity for the City and not just  
20 something forgotten about in a no man's land.

21           But even more so, particularly with what  
22 Herndon has said about the light and noise and the  
23 impact on the Oakdale Historic District, that's where

1 we're going next. We're actually considering a  
2 project right next to Ladas Grocery, which is, you  
3 know, right within spitting distance basically of  
4 where this will be.

5 So for us we would like to see some  
6 mitigation considered for the Oakdale Historic  
7 District, whether it be public improvements, public  
8 investment in private housing, whatever. We believe  
9 that it's a fragile neighborhood. It's getting more  
10 and more fragile every day. And this will not -- not  
11 just the construction but the continuous use of the  
12 highway will have an adverse impact on that  
13 neighborhood. And we would like to see something  
14 happening, some plan for that area. We'd be happy to  
15 be a part of it. Currently there's a private  
16 developer investing in the area as well. So just  
17 something so that we can try to stabilize that  
18 neighborhood and make it stronger rather than have to  
19 just suffer the impact of the bridge, construction and  
20 continuous use. Thank you.

21 ALLISON GREGG: Thank you.

22 Does anybody else in the room have comments?  
23 I know that we have one person on the phone who has a

1 comment as well.

2 (NO RESPONSE.)

3 ALLISON GREGG: Betsy?

4 ELIZABETH MERRITT: Yes. I have a followup  
5 question that related to the portion of the  
6 presentation on tolling, the financing slides, and the  
7 P3 process. And I was especially looking at the slide  
8 that showed the different sources of funding and which  
9 portions are going to be paid back through toll  
10 revenue and then also the slide where they talk about  
11 having a toll-free route across the Causeway, the  
12 Cochrane Bridge, et cetera.

13 Is the tolling revenue projection something  
14 that's going to be studied in the supplemental EIS?  
15 Because I have to say that I'm skeptical about whether  
16 the tolling would bring sufficient revenue to finance  
17 this project.

18 I mean here in Washington, DC we have toll  
19 roads everywhere, and it's part of the culture.  
20 Everybody assumes that you have to pay tolls all the  
21 time. But in a place like Mobile, I'm just concerned  
22 that, particularly if there are toll-free routes, that  
23 it will be very difficult as a practical matter to get

1 enough tolling revenue to actually make the financing  
2 succeed. And I just wondered what kind of  
3 investigation, what kind of study is going to look at  
4 that. Because that's a place where people are not in  
5 the habit of paying tolls every time they get in their  
6 car. And I'm just skeptical about whether it's going  
7 to bring enough revenue to finance the project.

8 So I just wondered if you could address that  
9 a little bit more about what studies have been done so  
10 far and whether additional analysis is going to be  
11 looked at in the Supplemental EIS.

12 MATT ERICKSEN: This is Matt Ericksen. So  
13 yes, we have a consultant that is doing a traffic and  
14 revenue model. We have a draft version of it on our  
15 website. KPMG is our financial advisor. They're  
16 still finishing up their financial plan. So we are  
17 going through that process as we move forward.

18 In addition, we will have an independent  
19 review, an independent analysis done by Fitch. And  
20 then, in addition, these three short-listed teams will  
21 do their due diligence before they submit a proposal  
22 to make sure this is a financially viable project in  
23 their view. They will have a substantial amount of

1 equity that they will invest in this project and in  
2 the City of Mobile, estimated up around 3 to 400  
3 million.

4 So the due diligence will be done as far as  
5 the traffic and revenue forecasting.

6 ELIZABETH MERRITT: Okay. Could you tell me  
7 where on your website I can find that study? I'm  
8 looking at it now, and I just need some help figuring  
9 out exactly where to find it on the website.

10 ALLISON GREGG: Yes. I just sent it to you  
11 through the chat.

12 ELIZABETH MERRITT: Great. Thanks. Okay.

13 ELIZABETH STEVENS: Can I ask a question?

14 ALLISON GREGG: Yes.

15 ELIZABETH STEVENS: I was just not clear on  
16 what the connection -- what Brian's very nice  
17 presentation on the tree canopy had to do with this  
18 discussion today. That didn't seem to connect for me.

19 ALLISON GREGG: The question in the room is  
20 how does the Right Tree, Right Place align with the  
21 Mobile River Bridge project.

22 MISSI SHUMER: So what we've found and what  
23 we've heard from the consulting parties is that the

1 existing tree canopy is largely outside of ALDOT's  
2 right-of-way and outside of ALDOT's jurisdiction. And  
3 so the City maintains and is responsible for making  
4 sure that new developers don't damage the existing  
5 tree canopy. And any new development that comes into  
6 place has to put in certain requirements.

7 ALDOT is dictating what will be put in place  
8 in terms of landscaping and trees within the character  
9 areas that Pat referenced in the aesthetic guidelines.  
10 But that's within their right-of-way. Anything  
11 outside of ALDOT's right-of-way that's under the  
12 City's jurisdiction, we're trying to make those two  
13 marry so that we can provide mitigation funding to  
14 make sure that they can use the funds that are  
15 contributed by ALDOT to continue to preserve and  
16 enhance the tree canopy within the areas that are in  
17 the City's jurisdiction.

18 ELIZABETH STEVENS: But there is a plan --  
19 for example, the area where it goes through south of  
20 the cruise terminal, there's a plan for how that is  
21 going to be developed?

22 MISSI SHUMER: That is going to be -- so all  
23 of the teams have to develop -- the question was is

1 there a plan or will there be a plan for landscaping  
2 and tree canopy within ALDOT's right-of-way. And the  
3 answer to that is: Each of the teams will have to  
4 prepare a landscaping plan. The technical provisions  
5 include the types of trees and plants and shrubs that  
6 can be used, which are also in accordance with the  
7 City's policies and in accordance with ALDOT's  
8 policies. So yes, there will be a landscaping plan  
9 for the areas within ALDOT's jurisdiction as well.

10 Our goal with the Right Tree, Right Place  
11 presentation today was just, since it's a new program,  
12 to share what that meant. Because I'm sure that  
13 reading it in the MOA, nobody knew what it was.

14 ELIZABETH STEVENS: Thank you.

15 DEVEREAUX BEMIS: May I follow up on Betsy's  
16 question? The study that you were talking about that  
17 the three concessionaires will have to do, will that  
18 also take into account the shift in traffic from  
19 people -- and let's face it, Mobilians, we're just not  
20 willing to pay tolls -- from the shift in traffic for  
21 Mobilians or even people going -- not using the new  
22 street going through downtown Mobile? Will that be  
23 part of that study?

1 MATT ERICKSEN: Yeah. They'll be sure to  
2 consider the leakage as far as their analysis.

3 ALLISON GREGG: well, thank you everybody  
4 for coming out today. we look forward to meeting with  
5 you again when we come back.

6 (THE MEETING WAS CONCLUDED AT 11:54 A.M.)  
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C E R T I F I C A T E

I do hereby certify that the foregoing proceedings were taken down by me and transcribed using computer-aided transcription and that the foregoing is a true and correct transcript of said proceedings.

I further certify that I am neither of counsel nor of kin to any of the parties, nor am I in anywise interested in the result of said cause.

I further certify that I have earned the certifications awarded by the National Court Reporters Association of RPR,RMR,RDR,CRR,CRC,RSA and am duly licensed by the Alabama, Illinois, Louisiana and Mississippi Boards of Court Reporting as a Certified Court Reporter.

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DEBRA AMOS ISBELL, CCR,RDR,CRR  
 ALABAMA - ACCR #21 (expires 9/30/18)  
 ILLINOIS - CSR #084.004798 (expires 5/31/19)  
 LOUISIANA - CCR #2014003 (expires 12/31/19)  
 MISSISSIPPI - CSR #1809 (expires 4/10/19)  
 NCRA (expires 12/31/2018)  
 COURT REPORTER, NOTARY PUBLIC (expires 7/6/20)  
 STATE OF ALABAMA AT LARGE

## **Comments Received from May 8, 2018 Meeting**

**From:** [Clay, Natasha](#)  
**To:** [Missi Shumer](#); [Wood, Andrew](#); [Urquhart, Lynne \(FHWA\)](#); [Heisler, Timothy \(FHWA\)](#); [Walker, Steve](#); [Perry, Edwin L.](#); [Henry, Wade D.](#); [Greg Lowe](#)  
**Subject:** FW: Mobile River Crossing- § 106 COMMENT on MOA  
**Date:** Friday, June 8, 2018 1:48:34 PM

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**From:** Herndon Inge III [mailto:[hinge@herndoninge.com](mailto:hinge@herndoninge.com)]

**Sent:** Friday, June 8, 2018 1:00 PM

**To:** Clay, Natasha <[clayn@dot.state.al.us](mailto:clayn@dot.state.al.us)>; Calametti, Vince <[calamettiv@dot.state.al.us](mailto:calamettiv@dot.state.al.us)>; Powell, Don <[powelldo@dot.state.al.us](mailto:powelldo@dot.state.al.us)>; 'Bartlett, Mark (FHWA)' <[Mark.Bartlett@dot.gov](mailto:Mark.Bartlett@dot.gov)>; [acoffa@dot.state.al.us](mailto:acoffa@dot.state.al.us); Clay, Natasha <[clayn@dot.state.al.us](mailto:clayn@dot.state.al.us)>; Adams, Wanda <[adamsw@dot.state.al.us](mailto:adamsw@dot.state.al.us)>

**Subject:** Mobile River Crossing- § 106 COMMENT on MOA

My comment to the proposed Memorandum of Agreement:

\$50,000 for planting some trees (which will take 30 years to mature), “to soften the visual effects of the bridge”, will not affect the View Encroachment of TWO 515 foot towers, and of the ramps and the bridge with a 215 foot vertical clearance (higher than the Golden Gate Bridge), or the adverse impact to the “view shed”, or the “highway noise”, or the “splash zone”, creating a “physical and psychological barrier” and “dead zone”, and the “adverse effects” to historic structures and historic districts, the economic Dis-investment of residential and commercial structures, a decade if traffic disruption just outside of but within unmistakable and clear sight of the Central Business District, during construction, or the adverse impact on the “natural beauty” and the “general community”, in violation of the:

“The Freeway in the City”, 1968, U.S. Department of Transportation

“A Policy on Design of Urban Highways and Arterial Streets”, 1973, American Association of State Highways Officials

36 C.F.R. §§800.16(i), 800.5(a)(1)

Section 110(f)

There are “feasible alternatives” that have not been made in “good faith objectivity” in selecting the route, as 42 U.S.C. §4332 and §4(f) and National Environmental Policy Act of 1969 require.

Herndon Inge  
§106 Consulting Party

**From:** [Urquhart, Lynne \(FHWA\)](#)  
**To:** [Betsy Merritt \(emerritt@savingplaces.org\)](mailto:emerritt@savingplaces.org)  
**Cc:** [Mark Bartlett \(MBartlett@maynardcooper.com\)](mailto:MBartlett@maynardcooper.com)  
**Subject:** Federal-Aid Project DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening, Mobile & Baldwin Counties, Alabama  
**Date:** Monday, July 23, 2018 4:09:13 PM  
**Attachments:** [Union Hall Activities.docx](#)

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Dear Ms. Merritt

This e-mail is being sent based on your questions and comments at the May 2018 Section 106 Consulting Party meeting for the subject project. The goal is to supply you with information regarding the National Register of Historic Places (NRHP) eligible Union Hall and its demolition. The Union Hall was a structure eligible for the NRHP under Criteria A for its use as a union meeting hall associated with the development of Mobile's shipyard industry, Alabama Dry Dock & Shipbuilding Company (ADDSCO) and its possible role during World War II, see the Draft Environmental Impact Statement (DEIS) on the subject project.

The Union Hall is not needed to construct the preferred Alternative (B') for this project. And the Union Hall was not anticipated to be adversely effected by building the preferred Alternative (B') of this project.

Attached is a listing of ALDOT and property owner activities related to the Union Hall. ALDOT was notified of the owner's plans to demolish the Union Hall and sell the property in September 2012. Based on the owner's stated plans and the property's proximity to the subject project, ALDOT decided to complete Level III Historic American Buildings Survey (HABS) Documentation on the Union Hall. The HABS Documentation was completed in March 2013. No further correspondence was found with the property owners or their agents and no further activities regarding the property were observed by ALDOT or FHWA until 2016. The demolition of the Union Hall was discovered by ALDOT and FHWA personnel in April 2016, approximately 2 months after the demolition was permitted by the City of Mobile. The timeline shows several years between any discussions of purchasing the Union Hall and its' demolition. Based on the review of ALDOT's information, FHWA finds no evidence of an attempt by ALDOT to avoid or circumvent compliance with the Section 106 requirements.

Thank you for your input. Please let me know if you have any additional questions.

Lynne A. Urquhart

9500 Wynlakes Place  
Montgomery, AL 36117  
(334) 274-6371

Date	Union Hall Related Activity
July 2011	The Historic Structures Survey Reports for the Project’s Area of Potential Effect (APE) were completed. The Union Hall was identified as eligible for the National Register of Historic Places (NRHP).
09/07/2012	The Southwest Region of ALDOT was notified by Metcalfe & Company (Bender’s Real Estate Agency) of plans to auction the Union Hall property (300 S Royal St) and offered to sell property outright prior to the auction. The letter indicated that in order “to increase marketability”, the owner planned to demolish the Union Hall building.
09/18/2012	The Southwest Region of ALDOT forwarded this notification along with a request to buy the property to ALDOT’s Central Office ROW Bureau.
10/03/2012	The ALDOT’s ROW Bureau denied the Southwest Region’s request to purchase the property due to it being eligible for the NRHP.
March 2013	ALDOT had Level III Historic American Buildings Survey (HABS) Documentation prepared for the Union Hall (300 S Royal St).
07/22/2014	<p>The Draft Environmental Impact Statement (DEIS) was signed. Per the DEIS:</p> <ul style="list-style-type: none"> <li>• Alternative B would require acquisition and demolition of the Union Hall and therefore would have adverse effect on the Union Hall.</li> <li>• Alternatives A, B’ (preferred) and C would have visual effects, but no physical effects on the Union Hall. These alternative’s effects were determined not to diminish the Union Hall’s qualities and therefore had a finding of no adverse effect on the Union Hall.</li> </ul>
02/01/2016	Grady Dortch & Sons (CE LLC) obtained a building permit (2016-00261) to demolish the Union Hall structure and create a vacant lot.
04/14/2016	During a site visit, ALDOT and FHWA personnel discovered the Union Hall was demolished and the property listed for sale.

04/18/2016	The Alabama Historical Commission (AHC) was informed (via e-mail) of ALDOT's discovery that the Union Hall had been demolished.
07/10/17	ALDOT Letter to AHC determining Union Hall property no longer NRHP eligible.
07/12/17	AHC indicates concurrence that the Union Hall is no longer NRHP eligible.

**Disposition of Comments from May 2018 Consulting Party Meeting**

**Downtown Mobile Alliance – Verbal Comments during May 8, 2018 Meeting**

	<b>Comment</b>	<b>Response</b>
1.	Will the two tunnels be monitored for vibration impacts?	The Wallace and Bankhead Tunnels will be monitored for vibration impacts.
2.	I am glad you are showing the brown historic signs [for the Battleship]. Think about the same for the historic district signs, downtown Mobile signs, all in the corridor of this study area.	ALDOT will work with the Section 106 Consulting Parties to determine appropriate supplemental signage for historic districts and downtown Mobile, where feasible.
3.	How does the Right Tree, Right Place program align with the project?	ALDOT, in coordination with the Aesthetic Steering Committee, has created landscaping requirements for the project within their own right-of-way; however, many of the areas where trees could be planted to preserve or enhance the downtown tree canopy are within City right-of-way. Therefore, ALDOT is providing mitigation funding to the <i>Right Tree, Right Place</i> program to make sure that appropriate trees are placed in accordance with the City’s requirements.
4.	Is there a plan, for example, for the how the area south of the cruise terminal is going to be developed?	A specific plan for the development of that area has not been determined. It will be a collaborative process between ALDOT and the City of Mobile. However, ALDOT is requiring landscaping and bicycle/pedestrian facilities in this area regardless of how it is ultimately developed. The Aesthetic Steering Committee has provided and will continue to provide input regarding the development of this area, as noted in Section 6.8 of the Supplemental DEIS and the Draft Section 106 MOA in Appendix L.

**Restore Mobile – Verbal Comments during May 8, 2018 Meeting**

	<b>Comment</b>	<b>Response</b>
1.	What about the effects of vibrations on the buildings in Fort Conde Village? Are they outside of the 250-foot radius for vibration monitoring?	As shown at the meeting on May 8, 2018, Fort Conde Village is outside of the 250-foot radius from the closest potential foundation on Alternative B’. However, as requested at the meeting, the Conde-

	Comment	Response
		Charlotte Museum House will be monitored for vibrations. This is listed in the Draft MOA.
2.	What about the Phoenix Fire Museum?	As shown at the meeting on May 8, 2018, the Phoenix Fire Museum is outside of the 250-foot radius from the closest potential foundation on Alternative B'. However, due to concerns expressed by Section 106 Consulting Parties, the Phoenix Fire Museum will be monitored for vibrations. This is listed in the Draft MOA.
3.	Will there be landscaping in downtown Mobile to help mitigate the visual effects?	Yes. The selected team will be required to prepare an aesthetics and landscape plan showing the proposed landscaping within ALDOT ROW in and around the downtown area, including underneath the bridge, as well as along the remainder of the project corridor. The landscaping must comply with the Aesthetic Guidelines developed for the project. As discussed in Section 6.8 of the Supplemental DEIS and the Draft Section 106 MOA in Appendix L, the Aesthetic Steering Committee will review and provide input on the Landscape Plan for the project. The City of Mobile will also administer the monetary contribution from ALDOT to the <i>Right Tree Right Place</i> program to plant appropriately sized trees in appropriate locations within the City's right-of-way.
4.	In some of the letters that have been written and in some of the meetings we've been talking about what happens underneath the bridge for the aesthetics committee is still important to us and how it connects and doesn't separate us from our waterfront. We would like to see that be an amenity for the City and not just something forgotten about in a no man's land.	The project includes the removal of the existing I-10 ramps at the Canal/Water Street interchange. A new, at-grade roadway connection with bicycle/pedestrian facilities will be constructed to connect Canal Street/Claiborne Street to the riverfront. Additionally, shared use paths and landscaping underneath the elevated approach structures leading up to the base of the bridge tower will be included. A stair tower/elevator will be required on the west side of the river to provide access to a belvedere (observation platform) located on the new bridge. This belvedere and the pathways connecting to it are expected to be an amenity for the City. The Aesthetic Steering Committee has provided and will continue to provide input regarding the development of this area, as noted in Section 6.8 of the Supplemental DEIS and the Draft Section 106 MOA in Appendix L.
5.	We are concerned about noise and lighting on the Oakdale Historic District. We would like to see some mitigation considered for the Oakdale Historic District, whether it be public	The new bridge is located over a mile away from the Oakdale Historic District. Noise studies performed for the proposed project indicate that noise levels may actually decrease with the proposed project



	<b>Comment</b>	<b>Response</b>
	improvements, public investment in private housing, whatever. Not just the construction, but the continuous use of the highway will have an adverse impact on the neighborhood. Currently, there's a private developer investing in that area as well. So, just something so that we can try to stabilize that neighborhood and make it stronger rather than have to just suffer the impact of the bridge, construction, and continuous use.	compared to not building the proposed project. This is largely due to the lower traffic volumes predicted to be on this portion of I-10 with the Build Alternative. Roadway lighting will be designed to include measures to shield light released from light fixtures in proximity to residential neighborhoods, and light fixtures will not be selected until late in the design process to ensure the latest available technology is used. Commitments regarding roadway lighting requirements are included in the Draft Section 106 MOA in Appendix L.
6.	Will the study take into account the shift in traffic of people avoiding the toll?	Yes. The potential impacts associated with increased traffic along roadways due to traffic diversion to avoid the toll are addressed in the Supplemental DEIS.

#### **Christ Church Cathedral – Verbal Comments during May 8, 2018 Meeting**

	<b>Comment</b>	<b>Response</b>
1.	Will there be something on our campus measuring vibration levels?	Yes. Vibration monitoring will occur at Christ Church Cathedral. This is listed in the Draft MOA.

#### **National Trust for Historic Preservation – Verbal Comments during May 8, 2018 Meeting**

	<b>Comment</b>	<b>Response</b>
1.	What is the threshold at which the alarm would go off and the vibration monitor would tell you to stop work?	As specified in the Draft MOA, the threshold for historic structures is 0.1 inch per second, and the threshold for modern structures is 0.5 inch per second. These thresholds were developed based on the <i>Final Report on Vibrations Due to Pile Driving at the Mobile River Bridge Site</i> , dated June 12, 2015. This report is available on the project website at <a href="http://www.mobileriverbridge.com">www.mobileriverbridge.com</a> .
2.	I am interested to hear more about the circumstances of the demolition of the Union Hall. I would like to see an investigation into the circumstances in order to determine whether there was an element of anticipatory demolition involved.	On July 23, 2018, FHWA transmitted information to the National Trust for Historic Preservation regarding the circumstances and timeline of the demolition of Union Hall including concurrence from SHPO. Based on the review of ALDOT's information, FHWA finds no evidence of an attempt by ALDOT to avoid or circumvent compliance with the Section 106 requirements. No response has been received from the National Trust for Historic Preservation.

	<b>Comment</b>	<b>Response</b>
3.	Is the tolling revenue projection something that's going to be studied in the Supplemental EIS? I am skeptical about whether the tolling would bring sufficient revenue to finance this project.	A discussion of tolling is included in the Supplemental DEIS. A preliminary Traffic and Revenue Study, dated October 2017, has been prepared and determined that tolling could produce sufficient revenue to fund a portion of this project. Total funding will be achieved through a combination of private investment (which will be recovered using toll revenues) and state and/or Federal funds. A link to this 2017 study was provided to the National Trust for Historic Preservation during the May 2018 meeting. The study is available on the project website at <a href="http://www.mobileriverbridge.com">www.mobileriverbridge.com</a> . ALDOT has a financial advisor performing an independent assessment of the viability of tolling, and an independent analysis will be performed by a third party. Additionally, each team pursuing the proposed project is also conducting its own studies regarding traffic and revenue.

**Herndon Inge, Stop the Bridge Coalition – Verbal Comments (1 -11) during May 8, 2018 Meeting**

	<b>Comment</b>	<b>Response</b>
1.	The route of the Mobile River crossing that was selected will change the focus of downtown Mobile and of two historic districts. What I am concerned about is the people that live and tour and shop downtown. Stop the Bridge is not opposed to the Mobile River crossing, but Stop the Bridge focuses on people. With B', the focus of downtown and Church Street East will now be a bridge approximately the same height as the Golden Gate Bridge, not our bay, our harbor, our Mobile River, and our Mobile delta. If the location of routing of the bridge stops people from investing and renting in Church Street East and DeTonti Square, if it stops tourists from coming to our downtown which is so fragile, then it should be reconsidered.	The current setting of downtown Mobile is a mixture of modern and historic elements, including historic structures, modern skyscrapers, large cranes used by the port, and industrial uses along the Mobile River. Construction of the bridge will introduce a new modern element into the skyline, but it will not be the only modern component. The proposed project will offer unique views of the river, bay, delta, and the resources that exist within the city of Mobile, as well as those along the Causeway and Eastern Shore. The inclusion of a belvedere (observation platform) with access via a stair tower/elevator on the west side of the river at the new bridge will also provide views that have been requested by the public.
2.	In 1968, the USDOT said that noise from a high-speed interstate through a central business district is a serious problem. And the way to mitigate it is to install double and triple-paned glass within eight blocks.	The proposed project is not introducing a new high-speed interstate through a central business district. Rather, the majority of the alignment of the project will follow the existing I-10 corridor. Advancements in traffic noise analysis have been made since 1968. Traffic noise analyses have been performed for the proposed project in accordance with ALDOT's Noise Policy. The new Mobile River

	<b>Comment</b>	<b>Response</b>
		Bridge is expected to move traffic further away from downtown and the central business district, which would move the associated traffic noise further away from the central business district.
3.	In 1968, the USDOT did a study called "The Freeway in the City."	
	Page 8: "Highway transportation cannot be allowed to function apart from or in conflict with its environment. The potential of conflict between a highway and personal and community aspirations such as natural beauty, preservation of historic sites, cleaner air, general community interest are greatest in America's densely populated urban areas." That's been ignored by the route of B'.	The proposed project has been developed to minimize environmental impacts taking into consideration transportation needs and public input. The majority of the alignment of the project will follow the existing I-10 corridor. Additionally, air quality analysis indicates that the proposed project would not result in any exceedances of National Ambient Air Quality Standards. The proposed project will not directly affect the preservation of historic sites. The proposed project will be visible from areas of natural beauty, such as the Mobile River and Mobile Bay and Delta, as well as historic districts. Other tall structures in downtown Mobile are also visible from these areas. The bridge will also provide a belvedere (observation platform) that will allow new views of these areas of natural beauty. Responses from the community about the desire to be able to experience the views from the new Mobile River Bridge led to the decision to include a belvedere on the new bridge.
	Page 9: "How does the Department of Transportation provide needed mobility and at the same time consider other important social goals such as preservation of historic sites?"	NEPA requires that Federal agencies consider the effects of their proposed actions on the natural, social, and economic environment and that impacts that are unavoidable be mitigated. The proposed project has been developed in a manner that avoids and/or minimizes adverse effects to the extent practicable. Mitigation measures for the adverse visual effects on the Church Street East Historic District and the Lower Dauphin Street Historic District are defined in the MOA.
	Page 33: "Urban highways should be so located and designed as to enhance rather than destroy a city's best attributes." Mobile was founded on a river and a bay and a delta, not the Golden Gate Bridge.	The proposed project will offer unique views of the river, bay, delta, and the resources that exist within the city of Mobile, as well as those along the Causeway and Eastern Shore. The inclusion of a belvedere (observation platform) with access via a stair tower/elevator on the west side of the river at the new bridge will also provide views that have been requested by the public.
	Page 34: "Highways should not encroach upon parkland, playgrounds, squares, plazas, or other open-space preserves."	The proposed project would not encroach on parkland, playgrounds, squares, plazas, or other open-space preserves.

	<b>Comment</b>	<b>Response</b>
	Page 35: "Highway location and design must consider the viewpoint of the area residents."	Public involvement has been and continues to be an ongoing process on this project since the early 2000s. The results of public involvement activities are documented in the 2003 Environmental Assessment, 2014 Draft Environmental Impact Statement, and in the upcoming Supplemental Draft Environmental Impact Statement. As the project has developed and as congestion on I-10 has worsened, public support of the project has increased. Public input, including Section 106 consultation, has also resulted in the evaluation of 14 alignments for consideration as Build Alternatives, the inclusion of bicycle and pedestrian facilities on the project, identification of appropriate mitigation measures for adverse impacts, and the development of an Aesthetic Steering Committee to guide the aesthetic treatments and design of the project in a manner that is compatible with the project's setting and the viewpoints of area residents.
	Page 38: "Beauty in highway design is a result of the sum total of carefully planned and sensitively handled elements."	ALDOT is committed to providing an aesthetically pleasing project. An Aesthetic Steering Committee has developed aesthetic guidelines to direct the vision for this project to make sure the large-scale and small-scale elements are well planned and implemented.
	Page 40: "Highways entering the city should by their location and design present its city in its most interesting light."	The current setting of downtown Mobile is a mixture of modern and historic elements, including historic structures, modern skyscrapers, large cranes used by the port, and industrial uses along the Mobile River. Construction of the bridge will introduce a new modern element into the skyline, but it will not be the only modern component. The proposed project will offer unique views of the river, bay, delta, and the resources that exist within the city of Mobile, as well as those along the Causeway and Eastern Shore. The inclusion of a belvedere (observation platform) with access via a stair tower/elevator on the west side of the river at the new bridge will also provide views that have been requested by the public.
4.	1973, by the USDOT, "When it's necessary to locate a freeway or other high-volume facility adjacent to a residential area, sufficient right-of-way width should be required for appropriate landscape development."	The proposed project is expected to move some traffic further away from residential areas compared to the current setting. Landscaping will be required throughout the project, as defined in the Aesthetic Guidelines for the project and included in the MOA.

	<b>Comment</b>	<b>Response</b>
5.	<p>What will be the focus of the people on the south and east sides of the Riverview Hotel and the Admiral Semmes Hotel and the First National Bank Building and the RSA Tower? It will not be our beautiful bay and our beautiful delta and our beautiful Mobile River and shipping. It will be the Golden Gate Bridge.</p>	<p>The current setting of downtown Mobile is a mixture of modern and historic elements, including historic structures, modern skyscrapers, large cranes used by the port, and industrial uses along the Mobile River. Construction of the bridge will introduce a new modern element into the skyline, but it will not be the only modern component.</p>
6.	<p>Section 106 requires the consideration of both direct and indirect adverse impact. In the Cape Wind Energy Project in Nantucket Sound, the Highway Department said there would be no direct impact. But the federal court said: “If a visual intrusion will diminish the core significance of a national historic landmark or would radically change the feature of the setting that are vital to defining the character of the place, then the U.S. Park Service is likely to find the visual intrusion has a direct and adverse impact on the historic landmark.” So, the visual impact of our downtown and two historic neighborhoods will be the Golden Gate Bridge.</p>	<p>FHWA has determined that the proposed project would have adverse visual effects on two historic districts. Mitigation measures outlined in the MOA will be implemented to offset unavoidable adverse visual effects. The historic districts and historic resources will continue to be eligible for listing and/or listed on the National Register of Historic Places.</p>
7.	<p>The solution would be to change the planned route from Alternative B so the Mobile River crossing is farther from historic structures and farther from historic neighborhoods and farther from downtown. That solves most of the objections, like the Africatown Bridge, the Charleston bridge, the Savannah bridges. Move them away from towering directly over our historic neighborhoods and historic homes in our downtown.</p>	<p>Alternative B' has been identified as the preferred alternative because it shifts the bridge as far away from downtown Mobile as possible while minimizing impacts on the maritime industry. The new bridge and the elevated structures approaching the actual river crossing will be located over existing I-10 and/or industrial areas on the east side of existing I-10. These structures will not be located directly over historic districts or historic homes listed on or eligible for listing on the National Register of Historic Places.</p> <p>One of the Build Alternatives, Alternative C, is located further to the south from the historic neighborhoods and downtown is still under consideration. However, this alternative would go directly above and require acquisition of right-of-way from the BAE Systems Southeast Shipyards, which is a maritime historic district eligible for listing on the National Register of Historic Places. The acquisition of ROW from the district would result in a “use” of a Section 4(f) resource as described in Chapter 5 of the DEIS and Supplemental DEIS.</p>

	<b>Comment</b>	<b>Response</b>
8.	So many of the viewshed responses in the CD that was provided, they're all subjective.	Viewshed renderings were prepared in an effort to show what the bridge may look like from a variety of locations identified in consultation with the Consulting Parties. Every effort was made to make the renderings as accurate as possible. The viewshed renderings were prepared for locations selected by the Section 106 Consulting Parties during a field review/site visit to discuss resources of concern within the area of potential effect.
9.	So the mitigation, they talk about \$50,000 to plant oak trees. In my opinion, that is not going to obscure the two towers that 550 feet high.	Comment noted. The contribution to the City's Right Tree, Right Place program is intended to provide mitigation in areas outside of ALDOT's right-of-way. Landscaping is required in areas within ALDOT's right-of-way, which will help minimize visual impacts. This commitment was added to the MOA in response to the Section 106 Consulting Parties' request that the existing tree canopy in the City be maintained to minimize visibility of the bridge from within the historic districts.
10.	The question before the federal court, which has been ignored for the last 15 years, is: Is there really no feasible, prudent alternative like Section 4(f) requires? Has the Alabama Department of Transportation really made a good faith objectivity in selecting the route like NEPA requires?	A full range of reasonable alternatives has been evaluated, as described in the Draft Environmental Impact Statement. The Draft Environmental Impact Statement and the Supplemental Draft Environmental Impact Statement evaluate Section 4(f).
11.	The dead zones under the highway, you know, the separation of one section, one neighborhood from another neighborhood, those cannot be lightly considered.	When it was originally constructed, I-10 created a separation between the areas east and west of the interstate. The proposed project will not introduce new separation of communities. In fact, the new project will result in improved connectivity between areas east and west of the interstate by eliminating the existing I-10 ramps at Water Street/Canal Street at Wallace Tunnel. A new at-grade roadway connector street will connect Canal Street and Claiborne Street to Water Street and the riverfront. Bicycle and pedestrian facilities will be required along this new road, providing additional connectivity to the waterfront.

**Herndon Inge – Written Comments (12 and 13) Received via E-mail on June 8, 2018**

12.	My comment to the proposed Memorandum of Agreement: \$50,000 for planting some trees (which will take 30 years to mature), "to soften the visual effects of the bridge", will not	See responses to comments 3-11 above.
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	<b>Comment</b>	<b>Response</b>
	<p>affect the View Encroachment of TWO 515 foot towers, and of the ramps and the bridge with a 215 foot vertical clearance (higher than the Golden Gate Bridge), or the adverse impact to the “view shed,” or the “highway noise,” or the “splash zone,” creating a “physical and psychological barrier” and “dead zone,” and the “adverse effects” to historic structures and historic districts, the economic dis-investment of residential and commercial structures, a decade if traffic disruption just outside of but within unmistakable and clear sight of the Central Business District, during construction, or the adverse impact on the “natural beauty” and the “general community,” in violation of the: “The Freeway in the City,” 1968; U.S. Department of Transportation “A Policy on Design of Urban Highways and Arterial Streets,” 1973; American Association of State Highways Officials, 36 C.F.R. §§800.16(i), 800.5(a)(1), Section 110(f).</p>	
13.	<p>There are “feasible alternatives” that have not been made in “good faith objectivity” in selecting the route, as 42 U.S.C. §4332 and §4(f) and National Environmental Policy Act of 1969 require.</p>	<p>A full range of reasonable alternatives has been evaluated, as described in the Draft Environmental Impact Statement.</p>

## **APPENDIX L-7:**

**Correspondence since May 8, 2018  
Section 106 Consulting Party Meeting**



**Correspondence with National Park  
Service, Government Street Presbyterian  
Church, and Organizations Individuals with  
Interest in Africatown Historic District**



U.S. Department  
of Transportation  
**Federal Highway  
Administration**

**Alabama Division**

May 4, 2018

9500 Wynlakes Place  
Montgomery, AL 36117  
334-274-6350  
334-274-6352  
[Alabama.FHWA@dot.gov](mailto:Alabama.FHWA@dot.gov)

In Reply Refer To:  
HDA-AL

MS. Cynthia Walton  
Regional NHL Program Manager  
National Park Service  
Southeast Regional Office  
Atlanta Federal Center  
1924 Building  
100 Alabama St, SW  
Atlanta, GA 30303

Subject: Federal-Aid Project DPI-0030(005)  
I-10 Mobile River Bridge & Bayway Widening  
Mobile and Baldwin Counties, Alabama

Dear Ms. Walton:

For the subject project, we have determined that there may be adverse visual effects to historic properties within the Church Street East Historic District. The proposed project will be clearly visible from the edge of the historic district. Due to the extensive modern structures (infill) to the southeast of the district, the visual effects of the project will be limited. However, when the project is combined with the visual effects of the numerous other modern structures within view, a cumulative impact will occur. The cumulative impact on the historic district's viewshed may further diminish the setting of the historic district, which would be an adverse effect.

One of the historic properties on the edge of the Church Street East Historic District is the Old Southern Market and City Hall, a national historic landmark. Three other national historic landmarks are within the project's Area of Potential Effect (APE), but we have determined that there are no adverse effects to these properties. The other landmarks are the Government Street Presbyterian Church, also within the Church Street East Historic District, and Battleship USS Alabama Memorial Park, containing the USS Alabama and USS Drum. Attached is a CD containing the subject project's draft environmental impact statement (DEIS) and Section 106 Consultation information.

Per 36 CFR 800.10(c), we are inviting you to participate in the Section 106 consultation for the subject federal action where there may be an adverse effect to a national historic landmark. Please provide your input on subject project's effects to historic properties and measures to avoid, minimize, and mitigate any adverse effects to historic properties.

If you have any questions or concerns, please contact Ms. Lynne Urquhart of my staff at (334) 274-6371 or lynne.urquhart@dot.gov.

Sincerely,



Mark D. Bartlett, P. E.  
Division Administrator

Enclosure:  
CD

cc: Mr. Steve Walker (electronic w/o enclosure)  
Mr. Wade Henry (electronic w/o enclosure)  
Ms. Natasha Clay (electronic w/o enclosure)  
Ms. Missy Shumer (electronic w/o enclosure)



# United States Department of the Interior



NATIONAL PARK SERVICE  
Southeast Regional Office  
Atlanta Federal Center  
1924 Building  
100 Alabama St., SW.  
Atlanta, Georgia 30303

IN REPLY REFER TO:  
I.A.2

MAY 14 2018

Mark D. Bartlett, P.E.  
Division Administrator  
Federal Highway Administration  
9500 Wynlakes Place  
Montgomery, AL 36117

RE: Federal-Aid Project DPI-0030(005)  
1-10 Mobile River Bridge and Bayway Widening  
Mobile and Baldwin Counties

Dear Mr. Bartlett:

Thank you for your letter on May 4, 2018, inviting the National Park Service (NPS) to participate in the Section 106 consultation for the Federal Highway Administration's (FHWA) proposed undertaking, widening of the I-10 Mobile River Bridge and Byway. The FHWA has determined that there will be an adverse effect to the Old Southern Market and City Hall,<sup>1</sup> a National Historic Landmark (NHL). It was determined by your organization that there will be no adverse effect to three other NHLs within the Area of Potential Effect (APE), the Government Street Presbyterian Church, the USS Alabama, and the USS Drum; the latter two located within the USS Alabama Memorial Park.

Along with our National Parks, NHLs are considered to be the most important historic properties in the United States. Upon designation by the Secretary of the Interior, National Historic Landmarks are automatically listed in the National Register of Historic Places and therefore included in the review of federal undertakings that are subject to Section 106 of the National Historic Preservation Act. Section 110(f) of the National Historic Preservation Act and the Section 106 regulations contain provisions that set a higher standard of consideration and care for National Historic Landmarks (54 U.S.C. 306107 and 36 CFR 800.10). Where National Historic Landmarks are concerned Section 110(f) provides that:

*Prior to the approval of any Federal undertaking which may directly and adversely affect any National Historic Landmark, the head of the responsible agency shall, to the maximum extent possible, undertake such planning and actions as may be necessary to*

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<sup>1</sup> Although it is referred to as Old Southern Market and City Hall in documentation prepared by FHWA, the official name of this National Historic Landmark is, City Hall.

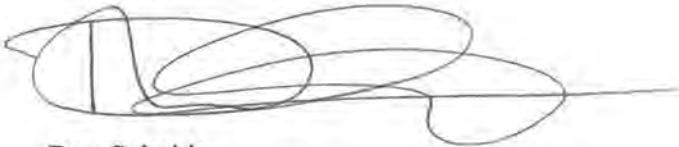
*minimize harm to such landmark, and shall afford the Advisory Council on Historic Preservation a reasonable opportunity to comment on the undertaking.*

Federal Agencies are required to notify the Secretary of the Interior (delegated to NPS regional offices) of any Section 106 consultation involving a National Historic Landmark and invite the Secretary to participate in the consultation where there may be an adverse effect (36 CFR 800.10(c)). Because the proposed undertaking has the potential to affect four NHLs, the NPS welcomes the opportunity to participate in this consultation. Additionally, if you have not already done so, we encourage you to contact the owners of the four NHLs and invite them to participate.

We have reviewed the draft Memorandum of Agreement (MOA), dated March 29, 2018, and would like to better understand the history of its development, especially as regards the involvement of consulting parties. In our experience the most successful agreement documents result from meaningful involvement from consulting parties. We have concerns regarding adverse effects to the Old Southern Market and City Hall NHL and would like to engage in a dialogue with other consulting parties to discuss how the adverse effects can be avoided or minimized. We would appreciate it if you could provide a general timeline outlining next steps for this consultation.

Please direct correspondence to Cynthia Walton at the address shown in the letterhead, or by email to [Cynthia\\_Walton@nps.gov](mailto:Cynthia_Walton@nps.gov). Ms. Walton can be reached by telephone at (404) 507-5792.

Sincerely,

A handwritten signature in black ink, appearing to read 'Dan Scheidt', with a horizontal line extending to the right.

Dan Scheidt  
SER Chief, Cultural Resources, Partnerships & Science

cc:

Lee Anne Wofford, AL Historical Commission (via email)  
Sarah Stokely, Advisory Council on Historic Preservation (via email)

**From:** Clay, Natasha  
**To:** [Missi Shumer](#); [Wood, Andrew](#); [Urquhart, Lynne \(FHWA\)](#); [Heisler, Timothy \(FHWA\)](#); [Walker, Steve](#); [Perry, Edwin L.](#); [Henry, Wade D](#); [Greg Lowe](#)  
**Subject:** FW: Mobile River Crossing- § 106 COMMENT on MOA  
**Date:** Friday, June 8, 2018 1:48:34 PM

---

**From:** Herndon Inge III [mailto:[hinge@herndoninge.com](mailto:hinge@herndoninge.com)]

**Sent:** Friday, June 8, 2018 1:00 PM

**To:** Clay, Natasha <[clayn@dot.state.al.us](mailto:clayn@dot.state.al.us)>; Calametti, Vince <[calamettiv@dot.state.al.us](mailto:calamettiv@dot.state.al.us)>; Powell, Don <[powelldo@dot.state.al.us](mailto:powelldo@dot.state.al.us)>; 'Bartlett, Mark (FHWA)' <[Mark.Bartlett@dot.gov](mailto:Mark.Bartlett@dot.gov)>; [acoffa@dot.state.al.us](mailto:acoffa@dot.state.al.us); Clay, Natasha <[clayn@dot.state.al.us](mailto:clayn@dot.state.al.us)>; Adams, Wanda <[adamsw@dot.state.al.us](mailto:adamsw@dot.state.al.us)>

**Subject:** Mobile River Crossing- § 106 COMMENT on MOA

My comment to the proposed Memorandum of Agreement:

\$50,000 for planting some trees (which will take 30 years to mature), “to soften the visual effects of the bridge”, will not affect the View Encroachment of TWO 515 foot towers, and of the ramps and the bridge with a 215 foot vertical clearance (higher than the Golden Gate Bridge), or the adverse impact to the “view shed”, or the “highway noise”, or the “splash zone”, creating a “physical and psychological barrier” and “dead zone”, and the “adverse effects” to historic structures and historic districts, the economic Dis-investment of residential and commercial structures, a decade if traffic disruption just outside of but within unmistakable and clear sight of the Central Business District, during construction, or the adverse impact on the “natural beauty” and the “general community”, in violation of the:

“The Freeway in the City”, 1968, U.S. Department of Transportation

“A Policy on Design of Urban Highways and Arterial Streets”, 1973, American Association of State Highways Officials

36 C.F.R. §§800.16(i), 800.5(a)(1)

Section 110(f)

There are “feasible alternatives” that have not been made in “good faith objectivity” in selecting the route, as 42 U.S.C. §4332 and §4(f) and National Environmental Policy Act of 1969 require.

Herndon Inge  
§106 Consulting Party



U.S. Department  
of Transportation  
**Federal Highway  
Administration**

**Alabama Division**

July 10, 2018

9500 Wynlakes Place  
Montgomery, AL 36117  
334-274-6350  
334-274-6352  
Alabama.FHWA@dot.gov

In Reply Refer To:  
HDA-AL

Ms. Cynthia Walton  
Regional NHL Program Manager  
National Park Service  
Southeast Regional Office  
Atlanta Federal Center  
1924 Building  
100 Alabama St, SW  
Atlanta, GA 30303

Subject: Federal-Aid Project DPI-0030(005)  
I-10 Mobile River Bridge & Bayway Widening  
Mobile and Baldwin Counties, Alabama

Dear Ms. Walton:

Thank you for agreeing to participate in the Section 106 consultation for the subject project. Per your comments, we are inviting the Government Street Presbyterian Church to participate. The City of Mobile (Old Southern Market and City Hall) and USS Alabama Battleship Memorial Park (USS Alabama and USS Drum) are already participating in the Section 106 Consultation process.

We have been in Section 106 consultation on this project off and on since 1999. Attached is a write-up of the history of the Section 106 Memorandum of Agreement (MOA) with regards to the consulting parties. Including the NPS there are currently 23 individuals and organizations, plus 18 tribes, consulting on this project. Attached is a list of the consulting parties and their contact information. The timeline for the next steps for consultation is also attached.

We appreciate your input. If you have any questions or concerns, please contact Ms. Lynne Urquhart of my staff at (334) 274-6371 or [lynne.urquhart@dot.gov](mailto:lynne.urquhart@dot.gov).

Sincerely,

Mark D. Bartlett, P. E.  
Division Administrator

By: e-mail

Enclosure

cc: Mr. Steve Walker (electronic w/enclosure)  
Mr. Wade Henry (electronic w/enclosure)  
Ms. Natasha Clay (electronic w/enclosure);  
Ms. Missy Shumer (electronic w/enclosure)

## **History of the Section 106 Memorandum Of Agreement (MOA) for Federal-Aid Project DPI-0030(005), Mobile and Baldwin Counties, I-10 Mobile River Bridge and Bayway Widening as of June 29, 2018**

The Draft Environmental Impact Statement (DEIS) for this project was signed July 18, 2014. The following steps led to the development of the Memorandum of Agreement (MOA):

- A Section 106 consultation meeting was held September 23, 2014. Meeting was to discuss:
  - FHWA's finding of No Adverse Effect;
  - ALDOT's DEIS commitments; and
  - the next steps in the Section 106 process.
- Based on that meeting, FHWA revised our determination of effects from No Adverse Effect and indicated mitigation measures would be discussed and included in a MOA. The consulting parties were notified by letter June 25, 2015.
- The initial draft of the MOA was sent to the consulting parties on July 27, 2016 with a request for comments. The draft MOA was based on:
  - the environmental commitments from the DEIS related to historic resources; and
  - an attempt to address comments and concerns expressed by the Section 106 consulting parties.
- Comments received on the draft MOA were distributed to all the consulting parties on July 27, 2016.
- The invitation to the May 2018 Section 106 consulting party meeting sent. Attached was an updated draft of the MOA and a document indicating the disposition of all the previous comments received on the MOA
- A Section 106 Consulting Party meeting was held May 8, 2018 focusing on three areas.
  - An overview of the project and a discussion of changes that have occurred in the project and present additional information that has been developed since the DEIS was signed.
  - Discuss the topics that are included as stipulations in the Draft MOA.

Identifying the next steps in the consultation process



## Section 106 Consulting Parties

Mr. John Sledge  
Mobile-Historic Development  
Commission  
PO Box 1827  
Mobile, AL 36633-1827

Ms. Elizabeth Merritt  
National Trust for Historic  
Preservation 2600 Virginia Ave NW  
Suite 1100  
Washington, DC 20037

Mr. John Hildreth  
National Trust for Historic  
Preservation William Aiken House  
456 King St - 3rd Floor  
Charleston, SC 29403

Ms. Lee Anne Wofford  
Alabama Historical Commission  
468 S Perry St  
Montgomery, AL 36130

The Honorable Sandy Stimpson  
Mayor of Mobile  
PO Box 1827  
Mobile, AL 36633-1827

Commissioner Connie Hudson  
President  
Mobile County Commission  
205 Government St  
Mobile, AL 36644-1001

The Honorable Dane Haygood  
Mayor of Daphne  
PO Box 400  
Daphne, AL 36526

The Honorable Michael McMillan  
Mayor of Spanish Fort  
PO Box 7226  
Spanish Fort, AL 36527

Commissioner Chris Elliot  
Baldwin County Commission  
1100 Fairhope Ave  
Fairhope, AL 36532

Mr. Walter Meigs  
BAE Systems/Southeast Shipyards  
Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

Ms. Wendy Crocker  
BAE Systems/Southeast Shipyards  
Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

Ms. Mary Cousar  
6 St Joseph St  
Mobile, AL 36602

Ms. Elizabeth Stevens  
Downtown Mobile Alliance  
PO Box 112  
Mobile, AL 36601

Ms. Elizabeth Harris  
Colonial Dames and Conde-  
Charlotte Museum House  
57 Oakland Ave  
Mobile, AL 36608

Mr. Ray Harris  
Signal Shipyard/Bender  
Shipbuilding & Repair Co  
60 I S Royal St  
Mobile, AL 36602

Mr. Douglas Burtu Kearley  
Ten Wisteria Ave  
Mobile, AL 36607

Mr. Herndon Inge  
PO Box 40188  
Mobile, AL 36640

Ms. Ann Bedsole  
6 St Joseph St  
Mobile, AL 36602

Ms. Carolyn Jeffers  
Christ Church Cathedral  
115 S Conception St  
Mobile, AL 36602

Historic Mobile Preservation Society  
300 Oakleigh Place  
Mobile, AL 36604

Ms. Katherine Frangos  
Friends of the Museum  
PO Box 2068  
Mobile, AL 36602

Major General Janet Cobb  
USS ALABAMA Battleship  
Memorial Park  
PO Box 65  
Mobile, AL 36601-0065

Mr. Tilmon Brown  
Restore Mobile  
PO Box 40037  
Mobile, AL 36640

Ms. Sarah Stokely  
Advisory Council on Historic  
Preservation 40 I F Street NW  
Suite 308  
Washington, DC 20001-2637

Ms. Amanda McBride  
Alabama Historical Commission  
468 S Perry St  
Montgomery, AL 36130

Ms. Cynthia Walton  
Regional NHL Program Manager  
National Park Service  
Southeast Regional Office  
Atlanta Federal Center  
1924 Building  
100 Alabama St, SW  
Atlanta, GA 30303

## Section 106 Tribal Contacts

Tribe/Agency	Name	Title	E-Mail	PHONE #	ADDRESS
Absentee-Shawnee Tribe	Mr. Leonard Longhorn	THPO	llonghorn@astribe.com	405-275-4030	2025 S Gordon Cooper Dr. Shawnee, OK 74801
Alabama-Coushatta Tribe of TX	Mr. Bryant J. Celestine	Historic Preservation Officer	celestine.bryant@actribe.org	936-563-1181	571 State Park Road 56 Livingston, Texas 77351
Alabama-Quassarte Tribe of OK	Ms. Samantha Robinson	THPO	AQhpo@mail.com	405-452-3881	PO Box 187 Wetumka, Oklahoma 74883
Cherokee Nation	Ms. Elizabeth Toombs	THPO	elizabeth-toombs@cherokee.org	918-453-5389	PO Box 948 Tahlequah, OK 74465
Chickasaw Nation	Ms. Karen Brunso	THPO	HPO@chickasaw.net (all official correspondence); Karen.Brunso@chickasaw.net	580-272-1106	PO Box 1548 Ada, OK 74820
Choctaw Nation of Oklahoma	Ms. Lindsey Bilyeu (pronounced Blue)	Senior Section 106 Compliance Review Officer	lbilyeu@choctawnation.com	580-924-8280 ext. 2631	PO Drawer 1210 Durant, OK 74702-1210
Coushatta Tribe of Louisiana	Dr. Linda Langley	THPO	llangley@coushattatribela.org	337-584-1560	PO Box 10 Elton, Louisiana 70532
Eastern Band of The Cherokee Nation	Mr. Russell Townsend	THPO	russtown@nc-chokeee.com	828-554-6851 or 828-554-6854	Qualla Boundary Reservation PO Box 455 Cherokee, N.C. 28719
	Mr. Stephen Yerka	Tribal Historic Preservation Specialist for DOT/FHWA	syerka@nc-chokeee.com		
Eastern Shawnee Tribe of Oklahoma	Ms. Robin Dushane	THPO	rdushane@estoo.net	918-666-2435 ext.1845	PO Box 350 127 West Oneida Seneca, MO 64865
Kialegee Tribal Town	Mr. David Cook	Preservation	kialegeettcpo@gmail.com	405-452-3262	PO Box 332 Wetumka, Oklahoma 74883
Mississippi Band of Choctaw Indians	Mr. Ken Carleton	THPO	kcarleton@choctaw.org or chocta	601-562-0032	PO Box 6257 Choctaw, Mississippi 39350
Muscogee (Creek) Nation	Ms. RaeLynn Butler	Historical & Cultural Preservation Manager	section106@mcn-nsn.gov (all official correspondence); raebutler@mcn-nsn.gov	918-732-7678	PO Box 580 Okmulgee, Oklahoma 74447
Poarch Band of Creek Indians	Ms. Carolyn White	Acting THPO	cwhite@pci-nsn.gov	251-368-9136 ext. 2532	5811 Jack Springs Road Atmore, Alabama 36502
Seminole Nation of OK	Mr. Theodore (Ted) Isham	Historic Preservation Officer	isham.t@sno-nsn.gov	405-234-5218 or (cell) 918-304-9443	PO Box 1498 Seminole, Ok 74868
Seminole Tribe of FL	Mr. Bradley Mueller	Compliance Supervisor, Tribal Historic Preservation Office	bradleymueller@semtribe.com	954-661-6712 cell	Tribal Historic Preservation Office Seminole Tribe of Florida 30290 Josie Billie Highway PMB 1004 Clewiston, FL 33440
	Mr. Andrew Weidman	Compliance Specialist for the THPO office	AndrewWeidman@semtribe.com	865-385-0012 cell	
Thlopthocco Tribal Town	Mr. Terry Clouthier	THPO/NAGPRA Contact	thpo@tttown.org	918-560-6113	PO Box 188 Okemah, Oklahoma 74859
Tunica-Biloxi Tribe of Louisiana	Mr. Earl Barbry, Jr.	Tribal Historic Preservation Officer	earlii@tunica.org	318-253-8174 ext. 6451	PO Box 1589 Marksville, LA 71351
United Keetoowah Band of the Cherokee Indians in Oklahoma	Ms. Sheila Bird	THPO and Director of Natural Resources	sbird@ukb-nsn.gov	918-871-2852	PO Box 746 Tahlequah, OK 74465
Jena Band of Choctaw Indians	Ms. Alina J. Shively	Deputy THPO	ashively@jenachoctaw.org	318-992-1205	PO Box 14 Jena, LA 71342

## Future Section 106 Activities

Provide Comments on Draft MOA by June 8, 2018

ALDOT/FHWA Update Draft MOA to Address Comments

Conduct Phase 1 and Phase 2 Archaeological Surveys

Update Draft MOA for Archaeological Findings

Circulate Draft MOA to Consulting Parties

Finalize MOA

Continue 106 Consultation through Design and Construction



Mobile River Bridge and Bayway



U.S. Department  
of Transportation  
**Federal Highway  
Administration**

**Alabama Division**

July 24, 2018

9500 Wynlakes Place  
Montgomery, AL 36117  
334-274-6350  
334-274-6352  
Alabama.FHWA@dot.gov

In Reply Refer To:  
HDA-AL

Ms. Lois McNorton  
Office & Property Manager  
Government Street Presbyterian Church  
300 Government Street  
Mobile, AL 36602

Subject: Federal-Aid Project DPI-0030(005)  
I-10 Mobile River Bridge & Bayway Widening  
Mobile and Baldwin Counties, Alabama

Dear Ms. McNorton:

The Federal Highway Administration (FHWA) would like to invite you to be a Section 106 Consulting Party for the above subject project. Section 106 of the National Historic Preservation Act requires Federal agencies to consider the effects of their projects on historic properties. The regulation implementing Section 106 may be found in Title 36, Code of Federal Regulations (CFR), Part 800. Under this regulation, certain individuals or organizations with demonstrated interest in the project may participate as consulting parties due to the nature of their legal or economic relation to the project or affected properties or their concern with the projects' effects on historic properties: 36 CFR 800.2(c)(5).

The FHWA, in cooperation with the Alabama Department of Transportation (ALDOT), is studying the above subject project. Additional information on the project, including a copy of the Draft Environmental Impact Statement (DEIS) signed in 2014, may be found at [www.mobileriverbridge.com](http://www.mobileriverbridge.com). The project will include constructing a new bridge on I-10 over the Mobile River in four possible locations, Build Alternatives A, B, B-prime (B') and C.

We have determined that there may be adverse visual effects to historic properties within the Church Street East Historic District by the Build Alternatives. The proposed project will be clearly visible from the southern and eastern edges of the historic district. Since the Government Street Presbyterian Church is a national historic landmark located within the Church Street East Historic District, the Church was determined to be a potentially affected property. Therefore, the Federal Highway Administration would like to invite you to participate in the process of addressing the potential impacts to the Church. This includes topics like impacts (effects) on the properties and measures to avoid, minimize, and mitigate any adverse effects on the properties.

If you are interested in participating in this capacity or have any questions, please contact Ms. Lynne A. Urquhart of my staff by phone 334-274-6371 or e-mail [Lynne.urquhart@dot.gov](mailto:Lynne.urquhart@dot.gov).

Sincerely,



Mark D. Bartlett, P. E.  
Division Administrator

cc: Mr. Steve Walker (electronic)  
Mr. Wade Henry (electronic)  
Ms. Natasha Clay (electronic)  
Ms. Missy Shumer (electronic)



## ALABAMA DEPARTMENT OF TRANSPORTATION

1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050



**Kay Ivey**  
Governor

**John R. Cooper**  
Transportation Director

December 19, 2018

«Title» «First\_Name» «Last\_Name»  
«Company\_Name»  
«Address\_Line\_1»  
«Address\_Line\_2»  
«City», «State» «ZIP\_Code»

**RE:** Section 106 Consulting Parties Invitation  
ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Project  
Mobile and Baldwin Counties, Alabama

Dear Sir or Madam:

The Federal Highway Administration (FHWA), in cooperation with the Alabama Department of Transportation (ALDOT), is studying the above-referenced project. The project will include constructing a new bridge on I-10 over the Mobile River in four possible locations, Build Alternatives A, B, B prime (B'), and C. Additional information on the project, including a copy of the Draft Environmental Impact Statement (DEIS) signed in 2014, may be found at [www.mobileriverbridge.com](http://www.mobileriverbridge.com).

Since the 2014 DEIS, it has been determined that tolling I-10 across the Mobile River and Bayway will be necessary to fund the project. Current traffic studies indicate that this may result in increased traffic in several areas, including Bay Bridge Road. As part of the Supplemental DEIS currently being prepared, we have evaluated potential effects of the proposed project on the Africatown Historic District. At this time, we do not anticipate adverse effects on the historic district.

We extend to your organization (you) an invitation to become a Section 106 Consulting Party with the FHWA and ALDOT for the proposed project. Section 106 of the National Historic Preservation Act requires Federal agencies to consider the effects of their projects on historic properties. The regulation implementing Section 106 may be found in Title 36, Code of Federal Regulations (CFR), Part 800. Under this regulation, certain individual or organizations with demonstrated interest in the project may participate as consulting parties due to the nature of their legal or economic relation to the project or affected properties or their concern with the project's effects on historic properties, 36 CFR 800.2(c)(5). The acceptance of this invitation does not imply that your organization (you) either supports the proposed project or has any special expertise with respect to the evaluation of the Section 106 process.



Page 2  
December 19, 2018

If you accept this invitation, please respond in writing by January 21, 2019, regarding your acceptance of this invitation to become a Section 106 Consulting Party.

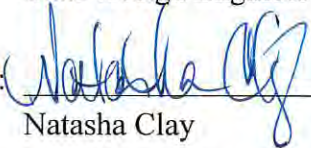
Your response may be mailed to:

**Alabama Department of Transportation  
Attn: Ms. Natasha Clay  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, AL 36110**

Should you have any questions, please feel free to contact our office at (334) 242-6315. Thank you for your cooperation and interest in this project.

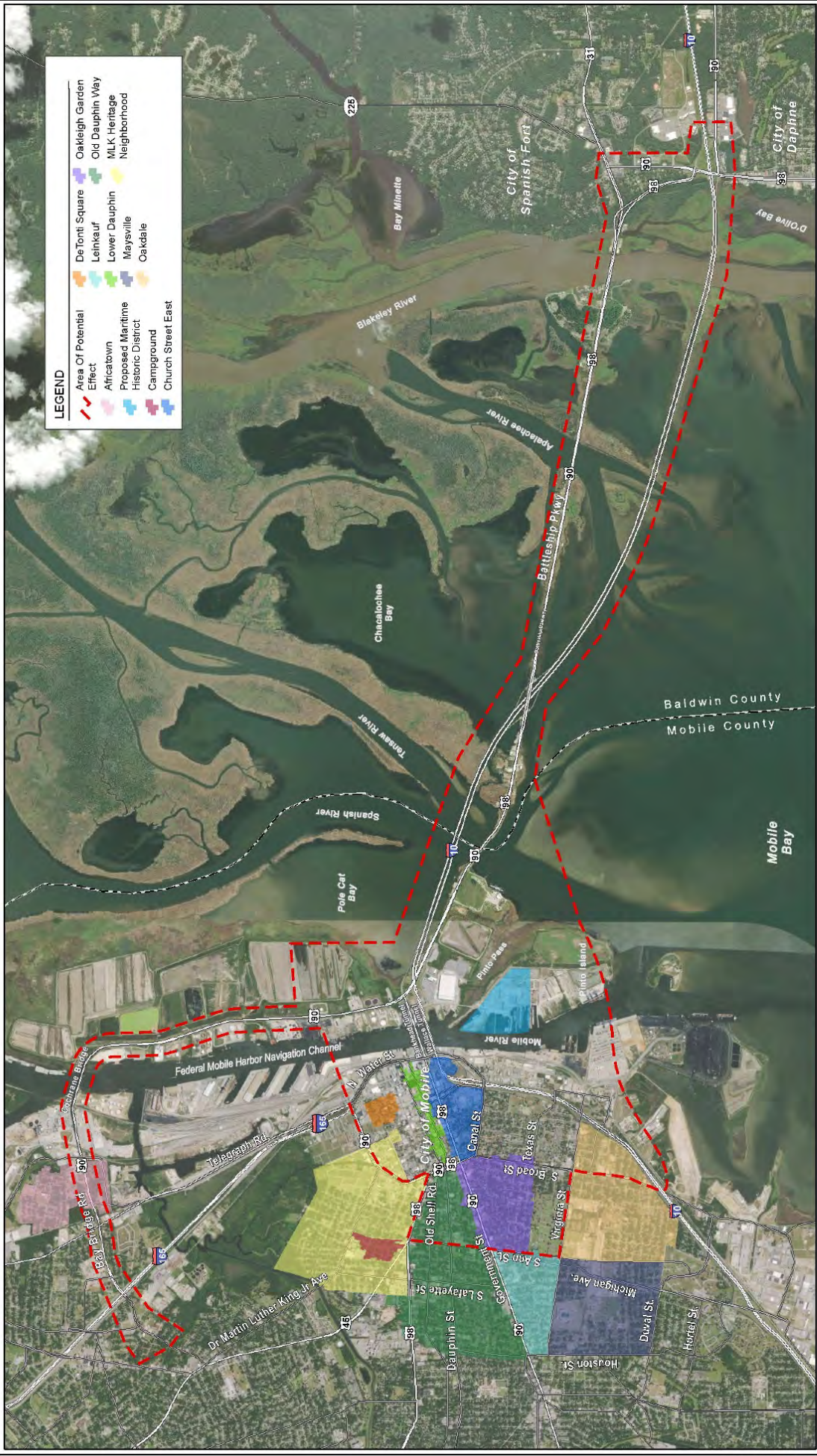
Sincerely,

Steven E. Walker, P. E.  
State Design Engineer

By:   
Natasha Clay  
State Environmental Administrator

SW/NC  
Attachments

c: FHWA  
ALDOT Mobile River Bridge Project Office  
Design Bureau File  
ETS Project Manager  
Cultural Resources File



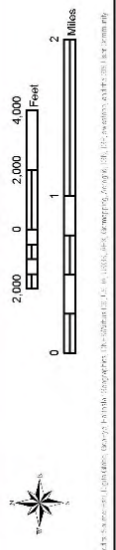
**LEGEND**

Area Of Potential Effect	De'Onti Square	Oakleigh Garden
Africatown	Leikhauf	Old Dauphin Way
Proposed Maritime Historic District	Lower Dauphin	MLK Heritage Neighborhood
Campground	Maysville	
Church Street East	Oakdale	

**HISTORIC DISTRICTS AND AREA OF POTENTIAL EFFECT**

PROJECT NO.: 15-1101-0300

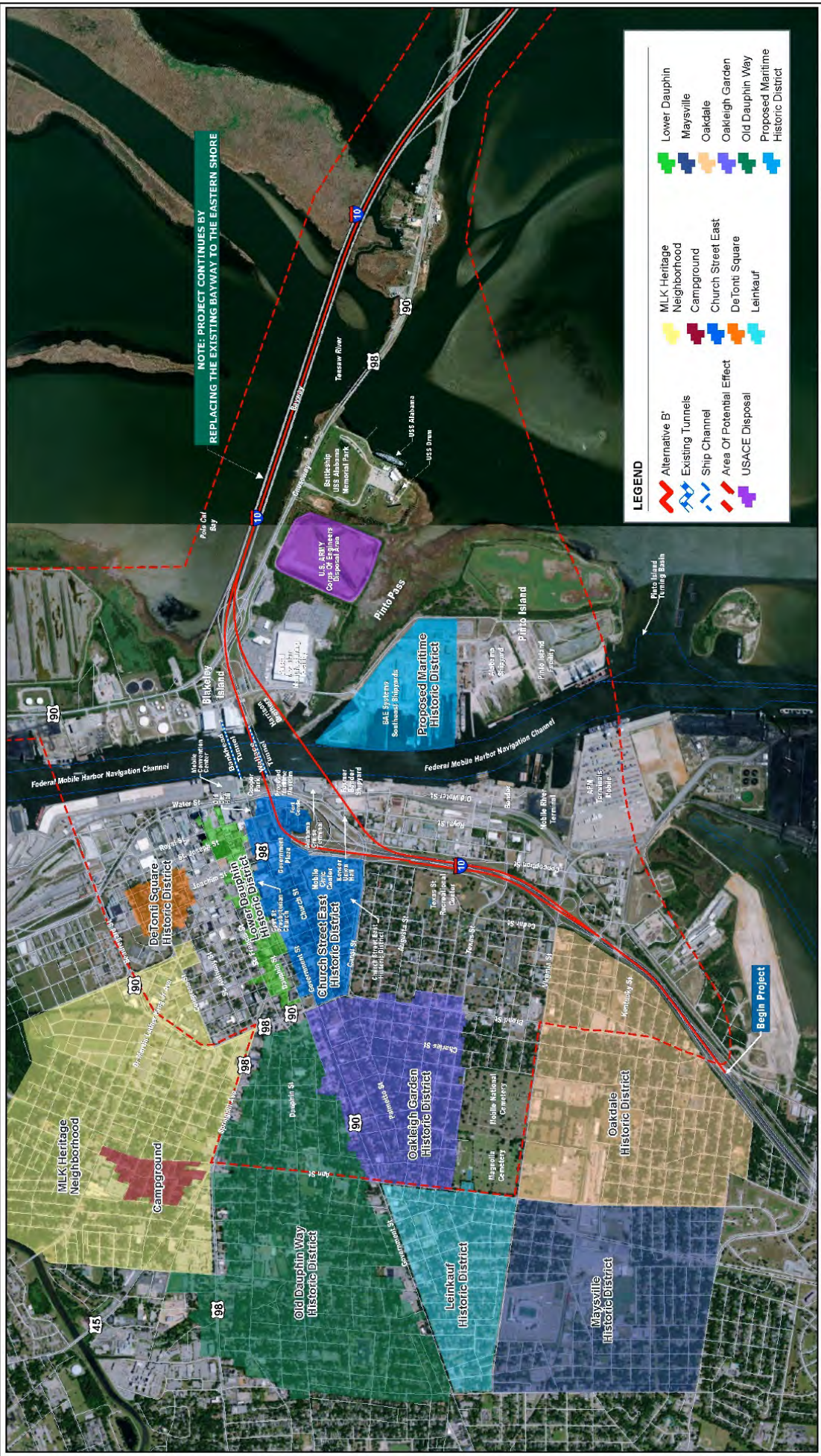
DATE: DECEMBER 2018



ALABAMA DEPARTMENT OF TRANSPORTATION  
 I-10 MOBILE RIVER BRIDGE  
 AND BAYWAY PROJECT  
 PROJECT No. DPI-0030 (005)  
 MOBILE AND BALDWIN COUNTIES, ALABAMA







NOTE: PROJECT CONTINUES BY REPLACING THE EXISTING BAYWAY TO THE EASTERN SHORE

**LEGEND**

- Alternative B'
- Existing Tunnels
- Ship Channel
- Area Of Potential Effect
- USACE Disposal
- Lower Dauphin
- Maysville
- Oakdale
- Oakleigh Garden
- Old Dauphin Way
- Proposed Maritime Historic District
- MLK Heritage Neighborhood
- Campground
- Church Street East
- DeTonti Square
- Leinkauf

1:000 0 1:000 Feet

0 0.5 1 Miles

ALABAMA DEPARTMENT OF TRANSPORTATION  
 I-10 MOBILE RIVER BRIDGE AND BAYWAY PROJECT  
 PROJECT No. DPL-0030 (005)  
 MOBILE AND BALDWIN COUNTIES, ALABAMA

**M M**  
 MOTT MACDONALD

**FTR**  
 THOMPSON CONSULTANTS

HISTORIC DISTRICTS AND AREA OF POTENTIAL EFFECT

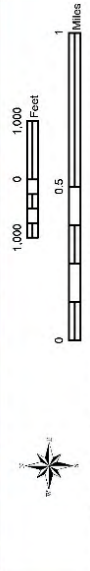
PROJECT NO.: 15-1101-0300

DATE: DECEMBER 2018





- LEGEND**
- Alternative B
  - Existing Tunnels
  - Ship Channel
  - Area Of Potential Effect
  - USACE Disposal Area
  - Africatown
  - MLK Heritage Neighborhood
  - Campground
  - Church Street East
  - DeTonti Square
  - Leinkauf
  - Lower Dauphin
  - Maysville
  - Oakdale
  - Oakleigh Garden
  - Old Dauphin Way
  - Proposed Maritime Historic District



ALABAMA DEPARTMENT OF TRANSPORTATION  
 I-10 MOBILE RIVER BRIDGE  
 AND BAYWAY PROJECT  
 PROJECT No. DPI-0030 (005)  
 MOBILE AND BALDWIN COUNTIES, ALABAMA

**M M**  
 MOTT  
 MACDONALD

**FDR**  
 thompson  
 CONSULTANTS

DATE: DECEMBER 2018  
 PROJECT NO.: 15-1101-0300

Historic Africatown Welcome Center  
1959 Bay Bridge Cutoff Rd  
Mobile, AL 36610

Senator Vivian Figures  
PO Box 7985  
Mobile, AL 36670

Mr. Elvin D. Lang  
1409 S Court St  
Montgomery, AL 36104

Mr. Anderson Flen  
Mobile County Training School  
800 Whitley St  
Prichard, AL 36610

Mr. Joe Womack, Executive  
Director  
Africatown C.H.E.S.S.  
Bay Bridge Rd  
Mobile, AL 36610

Dr. Jocelyn A. Finley  
University Commons  
307 University Blvd N  
Mobile, AL 36688

Robert L. Hope Community Center  
C/O Mr. James Hope  
850 Edwards St  
Mobile, AL 36610

Dr. Kern Jackson, Director  
African American Studies Program  
University of South Alabama  
PO Box 1233  
Mobile, AL 36633-1233

Ms. Ossia Edwards  
2313 W Main St  
Mobile, AL 36610

Ms. Vickii Howell  
M.O.V.E. Mobile  
Gulf Coast Community  
Development Corporation  
3223 Spring Hill Ave  
Mobile, AL 36607





## MOBILE COUNTY TRAINING SCHOOL

Alumni Association

Post Office Box 10274 - Prichard, Alabama 36610

<http://www.mctswhippets.org>

Email: [blueandwhite@mctswhippets.org](mailto:blueandwhite@mctswhippets.org)

"Another Step Forward"

### Officers:

Anderson Flen  
President  
Charles Holloway  
Vice Pres.  
Minnie Dewberry  
Rec. Sec.  
Carlee Russell  
Asst. Rec. Sec.  
Joe Womack  
Fin. Secretary  
Bettye Greene  
Treasurer  
Emory Johnson  
Parliamentarian  
Brandon Spradley  
Chaplain

### Board Members:

James Hope  
Beatrice Morse  
Isaiah Pinkney  
Helen Porter  
Leslie Walker

January 16, 2019



Alabama Department of Transportation  
Attn: Ms. Natasha Clay  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, AL 36110

RE: Section 106 Consulting Parties Invitation  
ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Project  
Mobile and Baldwin Counties, Alabama

Dear Ms. Clay,

On behalf of the Mobile County Training School Alumni Association and the Mobile County Training School family, we accept your gracious invitation to become a Section 106 Consulting Party.

Please note that our physical address is 800 Whitley Street Mobile, Alabama 36610.

Our Sincere Thanks,

Anderson Flen, President  
404-219-7890 cell



Robert L. Hope Community Center

Steven E. Walker, P. E.

Alabama Department of Transportation

Environmental Technical Section

1409 Coliseum Blvd.

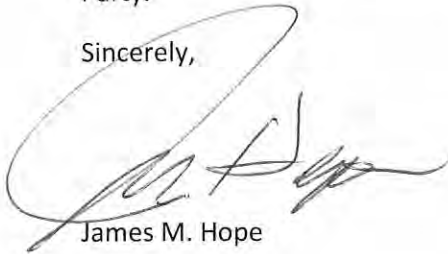
Montgomery, Alabama 36110

January 24, 2019

Dear Mr. Walker,

Per your letter dated December 19, 2018, I accept your invitation to become a Section 106 Consulting Party.

Sincerely,

A handwritten signature in black ink, appearing to read 'J. Hope', written over a large, light-colored circular scribble.

James M. Hope



## **Patterson, Pat M**

---

**From:** Joe Womack <jnwomack1@yahoo.com>  
**Sent:** Friday, January 25, 2019 2:40 AM  
**To:** Patterson, Pat M  
**Cc:** Clay, Natasha; Turner, William; May, Melinda  
**Subject:** Re: ALDOT Project DPI-0030(005) I-10 Mobile River Bridge and Bayway Widening Project

Yes, I would love to serve on this committee. I do not have a printer so I hope this email can serve as acceptance to your invitation. My address is 812 Center Street, Mobile, Al. 36610.

On Thursday, January 24, 2019, 10:17:13 AM EST, Patterson, Pat M <pattersonp@dot.state.al.us> wrote:

Mr. Womack,

On December 19, 2018 our agency transmitted Consulting Parties Invitation letters concerning the above subject project. Due to unforeseen issues your letter was returned undeliverable. Please find a copy of the said letter attached. Should you choose to accept, please submit your response as directed, as soon as possible.

Thank you sincerely,

***Pat M. Patterson***

*Environmental Specialist, Sr.*

*Cultural Resources*

*Alabama Department of Transportation*

*Environmental Technical Section*

*1409 Coliseum Blvd*

*Montgomery, AL 36130*

***(334) 242-6633***

*fax (334) 269-0826*



*Jimmy Gardner, Mayor*  
*Ossia Edwards*  
*Councilwoman*

---

***City of Prichard***

*216 East Prichard Avenue*

*Prichard, Alabama 36610*

*Office: (251) 452-7810 Fax: (251) 452-7921*

*"The City of Champions"*

***Lorenzo Martin***  
*Council, District 1*

***Severia Campbell-Morris***  
*Councilwoman District 2*  
*D. Griffin*  
*Councilwoman District 3*

***Samantha Richardson***  
*Councilman District 4*

***Ossia Edwards***  
*Councilwoman District 5*

*Council Office (251) 452-7810*

February 1, 2019

Ms. Natasha Clay  
Alabama Department of Transportation  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, Alabama 36110

Dear Ms. Clay,

First, let me apologize regarding the lateness responding to your letter. Secondly, I am very elated to be asked to be a part as a Consulting Party. I do accept and again, thank you for the invite.

If further information is needed, please call me. My cell number is, (251) 421-7294 and office number is, (251) 452-7805.

Yours truly,

*Ossia Edwards*

Ossia Edwards, Councilwoman  
City of Prichard, Alabama

# **APPENDIX L-8:**

**March 12, 2019**

**Section 106 Consulting Party Meeting**



**March 12, 2019 Section 106 Consulting Party Meeting Invitation,  
Meeting Summary, and Presentation**  
*(transcript and disposition of comments to be included in FEIS/ROD)*

February 12, 2019

«Title» «First\_Name» «Last\_Name»  
«Company\_Name»  
«Address\_Line\_1»  
«Address\_Line\_2»  
«City», «State» «ZIP\_Code»

**RE: Section 106 Consulting Parties Invitation**  
ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Project  
Mobile and Baldwin Counties, Alabama

Dear Sir or Madam:

The Federal Highway Administration (FHWA), in cooperation with the Alabama Department of Transportation (ALDOT), is studying the above-referenced project. The project will include constructing a new bridge on I-10 over the Mobile River.

Since the Draft Environmental Impact Statement (DEIS) approval on July 22, 2014, and as discussed at our May 8, 2018 Consulting Party Meeting, it has been determined that tolling I-10 across the Mobile River and Bayway will be necessary to fund the project. An Interstate Modification Request (IMR) was performed to evaluate potential changes in traffic and congestion related to the proposed project. The IMR indicated that traffic will increase on Bay Bridge Road, Cochrane-Africatown USA Bridge, US-90 between the Cochrane-Africatown USA Bridge and the Bankhead Tunnel, and the US-90/US-98 Causeway, due to diverted traffic avoiding the toll on the Bayway.

As part of the Supplemental DEIS currently being prepared, we have expanded the Area of Potential Effect (APE). In accordance with 36 CFR 800.6(b)(2), the Consulting Parties on this project are offered the opportunity to provide input on the limits of the APE and historic properties that should be included within the APE, provide information regarding historic properties in the area, and identify issues related to the proposed project's potential effects on historic properties.

The projected increase in traffic resulted in the delineation of new APE boundaries to accommodate the study area. The Newly Delineated APE includes a 1000' corridor to the north on either side on US-90 then to Bay Bridge Road, past I-165 to Velma Street. Please see attached maps.

The purpose of this letter is to provide you, as a Consulting Party:

- An invitation to participate in the upcoming Section 106 Consulting Party coordination meeting for the referenced project;
- The cultural resources report for the expanded APE, which includes Africatown;
- A disposition of comments received on the previous Draft MOA from Section 106 Consulting Parties;

- The Revised Draft MOA for your review;
- Previous Cultural Resource Reports and Coordination information for your review;
- Background information regarding previous Section 106 Coordination.

Section 106 Consulting Party coordination activities that have occurred to-date are described on the attached list. The most recent coordination with Section 106 Consulting Parties included an April 06, 2018 invitation letter regarding the previously held May 08, 2018 meeting.

A Section 106 Consulting Party coordination meeting will be held on Tuesday, March 12, 2019 from 10:00 a.m. to 12:00 pm. The location is:

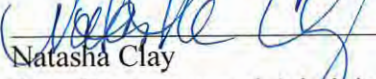
**Alabama Department of Transportation  
Southwest Region  
1701 I-65 West Service Road North  
Building T  
Mobile, AL 36618**

This meeting will focus on three areas. First, we will provide the newer Consulting Parties with an interest in Africatown an overview of the project and a discussion of changes that have occurred in the project and present additional information that has been developed since the DEIS was signed. Second, we will discuss the topics that are included as stipulations in the Draft MOA. Finally, we will conclude the meeting by identifying the next steps in the consultation process.

Please provide any comments within thirty days of receipt of this letter. Should you have any questions, please feel free to contact our office at (334) 242-6315. Thank you for your cooperation and interest in this project.

Sincerely,

Steven E. Walker, P. E.  
State Design Engineer

By:   
Natasha Clay  
State Environmental Administrator

SW/NC/mem

Attachments

c: FHWA

Mr. David S. Clarke, FHWA  
ALDOT Mobile River Bridge Project Office  
Design Bureau File  
ETS Project Manager  
Cultural Resources File

Mr. John Sledge  
Mobile Historic Development  
Commission  
PO Box 1827  
Mobile, AL 36633-1827

Ms. Elizabeth Merritt  
National Trust for Historic  
Preservation  
2600 Virginia Ave NW  
Suite 1100  
Washington, DC 20037

Mr. John Hildreth  
National Trust for Historic  
Preservation  
William Aiken House  
456 King St - 3rd Floor  
Charleston, SC 29403

Ms. Lee Anne Wofford  
Alabama Historical Commission  
468 S Perry St  
Montgomery, AL 36130

The Honorable Sandy Stimpson  
Mayor of Mobile  
PO Box 1827  
Mobile, AL 36633-1827

Commissioner Connie Hudson  
President  
Mobile County Commission  
205 Government St  
Mobile, AL 36644-1001

The Honorable Dane Haygood  
Mayor of Daphne  
PO Box 400  
Daphne, AL 36526

The Honorable Michael McMillan  
Mayor of Spanish Fort  
PO Box 7226  
Spanish Fort, AL 36527

Commissioner Chris Elliot  
Baldwin County Commission  
1100 Fairhope Ave  
Fairhope, AL 36532

Mr. Walter Meigs  
BAE Systems/Southeast Shipyards  
Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

Ms. Wendy Crocker  
BAE Systems/Southeast Shipyards  
Alabama, LLC  
PO Box 3202  
Mobile, AL 36652

Ms. Mary Cousar  
6 St Joseph St  
Mobile, AL 36602

Ms. Elizabeth Stevens  
Downtown Mobile Alliance  
PO Box 112  
Mobile, AL 36601

Ms. Elizabeth Harris  
Colonial Dames and Conde-  
Charlotte Museum House  
104 Theatre St  
Mobile, AL 36602

Mr. Ray Harris  
Signal Shipyard/Bender  
Shipbuilding & Repair Co  
601 S Royal St  
Mobile, AL 36602

Mr. Douglas Burtu Kearley  
Ten Wisteria Ave  
Mobile, AL 36607

Mr. Herndon Inge  
PO Box 40188  
Mobile, AL 36640

Ms. Ann Bedsole  
6 St Joseph St  
Mobile, AL 36602

Ms. Carolyn Jeffers  
Christ Church Cathedral  
115 S Conception St,  
Mobile, AL 36602

Historic Mobile Preservation Society  
300 Oakleigh Place  
Mobile, AL 36604

Ms. Katherine Frangos  
Friends of the Museum  
PO Box 2068  
Mobile, AL 36602

Major General Janet Cobb  
USS ALABAMA Battleship  
Memorial Park  
PO Box 65  
Mobile, AL 36601-0065

Mr. Tilmon Brown  
Restore Mobile  
PO Box 40037  
Mobile, AL 36640

Ms. Mandy Ranslow  
Advisory Council on Historic  
Preservation  
401 F Street NW  
Washington, DC 20001-2637

Ms. Amanda McBride  
Alabama Historical Commission  
468 S Perry St  
Montgomery, AL 36130

Mr. Anderson Flen  
Mobile County Training School  
800 Whitley St  
Prichard, AL 36610

Mr. Joe Womack  
Africatown C.H.E.S.S.  
812 Center St  
Mobile, AL 36610

Robert L. Hope Community Center  
c/o Mr. James Hope  
50507 Stonebridge Ln  
Birmingham, AL 35242

Ms. Ossia Edwards  
Prichard City Council  
216 East Prichard Ave  
Prichard, AL 36610

Ms. Cynthia Walton  
National Park Service, SE Region  
100 Alabama St. SW  
Atlanta, GA 30303



# ALABAMA DEPARTMENT OF TRANSPORTATION

1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050



**Kay Ivey**  
Governor

**John R. Cooper**  
Transportation Director

February 12, 2019

Ms. Geri A. Robinson  
Bridge Management Specialist  
Eighth Coast Guard District  
500 Poydras Street, Room 1313  
New Orleans, LA 70130-3310

**RE:** ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening EIS  
Mobile and Baldwin Counties

Dear Ms. Robinson:

The Alabama Department of Transportation and the Federal Highway Administration would like to invite you to attend a Section 106 Consulting Parties Consultation Meeting. March 12, 2019 from 10:00 a.m. to 12:00 pm. The location is:

Alabama Department of Transportation  
Southwest Region  
1701 I-65 West Service Road North  
Building T  
Mobile, AL 36618

This meeting will focus on three areas. First, we will provide the newer Consulting Parties with an interest in Africatown an overview of the project and a discussion of changes that have occurred in the project and present additional information that has been developed since the DEIS was signed. Second, we will discuss the topics that are included as stipulations in the Draft MOA. Finally, we will conclude the meeting by identifying the next steps in the consultation process.

Please feel free to contact Ms. Natasha Clay at (334) 242-6315 if you should have any questions or comments regarding this meeting. We look forward to your participation in the Section 106 Consulting Parties consultation meeting on March 12, 2019.

Sincerely,

Steven E. Walker, P.E.  
State Design Engineer

By: *Natasha Clay*  
for Natasha Clay  
State Environmental Administrator

NC/mem

cc: FHWA  
ALDOT Mobile River Bridge Project Office  
Missi Shumer  
ETS Cultural Resources File







**ALABAMA DEPARTMENT OF TRANSPORTATION**

1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050



**Kay Ivey**  
Governor

**John R. Cooper**  
Transportation Director

February 12, 2019

Mr. Steven B. Crosson  
Mobile District, U.S. Army Corps of Engineers  
PO Box 2288  
Mobile, Alabama 36628-0001

**RE:** ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening EIS  
Mobile and Baldwin Counties

Dear Mr. Crosson:

The Alabama Department of Transportation and the Federal Highway Administration would like to invite you to attend a Section 106 Consulting Parties Consultation Meeting. March 12, 2019 from 10:00 a.m. to 12:00 pm. The location is:

Alabama Department of Transportation  
Southwest Region  
1701 I-65 West Service Road North  
Building T  
Mobile, AL 36618

This meeting will focus on three areas. First, we will provide the newer Consulting Parties with an interest in Africatown an overview of the project and a discussion of changes that have occurred in the project and present additional information that has been developed since the DEIS was signed. Second, we will discuss the topics that are included as stipulations in the Draft MOA. Finally, we will conclude the meeting by identifying the next steps in the consultation process.

Please feel free to contact Ms. Natasha Clay at (334) 242-6315 if you should have any questions or comments regarding this meeting. We look forward to your participation in the Section 106 Consulting Parties consultation meeting on March 12, 2019.

Sincerely,

Steven E. Walker, P.E.  
State Design Engineer

By: John R. Cooper  
for Natasha Clay  
State Environmental Administrator

NC/mem  
cc: FHWA  
ALDOT Mobile River Bridge Project Office  
Missi Shumer  
ETS Cultural Resources File





# ALABAMA DEPARTMENT OF TRANSPORTATION

1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050



**Kay Ivey**  
Governor

**John R. Cooper**  
Transportation Director

February 12, 2019

Mr. Dylan C. Hendrix  
Mobile District, U.S. Army Corps of Engineers  
PO Box 2288  
Mobile, Alabama 36628-0001

**RE:** ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening EIS  
Mobile and Baldwin Counties

Dear Mr. Hendrix:

The Alabama Department of Transportation and the Federal Highway Administration would like to invite you to attend a Section 106 Consulting Parties Consultation Meeting. March 12, 2019 from 10:00 a.m. to 12:00 pm. The location is:

Alabama Department of Transportation  
Southwest Region  
1701 I-65 West Service Road North  
Building T  
Mobile, AL 36618

This meeting will focus on three areas. First, we will provide the newer Consulting Parties with an interest in Africatown an overview of the project and a discussion of changes that have occurred in the project and present additional information that has been developed since the DEIS was signed. Second, we will discuss the topics that are included as stipulations in the Draft MOA. Finally, we will conclude the meeting by identifying the next steps in the consultation process.

Please feel free to contact Ms. Natasha Clay at (334) 242-6315 if you should have any questions or comments regarding this meeting. We look forward to your participation in the Section 106 Consulting Parties consultation meeting on March 12, 2019.

Sincerely,

Steven E. Walker, P.E.  
State Design Engineer

By: *Natasha Clay*  
for Natasha Clay  
State Environmental Administrator

NC/mem

cc: FHWA  
ALDOT Mobile River Bridge Project Office  
Missi Shumer  
ETS Cultural Resources File



**Consultation with SHPO on Expanded Area of Potential Effect**

00-1934/00-0352



ALABAMA DEPARTMENT OF TRANSPORTATION  
Design Bureau  
1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050  
Phone: 334-242-6178 FAX: 334-269-0826

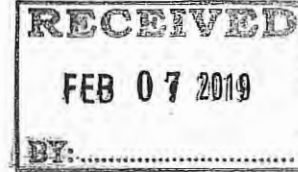


Kay Ivey  
Governor

John R. Cooper  
Transportation Director

February 6, 2019

Mrs. Lee Anne Wofford  
Deputy State Historic Preservation Officer  
Alabama Historical Commission  
468 South Perry Street  
Montgomery, Alabama 36130-0900



RE: **AHC# 00-1934 and AHC# 00-0352 Newly Delineated APE Assessment**  
ALDOT Project DPI-0030(005) I-10 Mobile River Bridge and Bayway Project Mobile and Baldwin Counties, Alabama

Dear Mrs. Wofford:

In previous correspondence it was determined that the above referenced project would have adverse visual effects on two NRHP Historic Districts, the Church Street East Historic District and Lower Dauphin Street Historic District. Your agency concurred in a letter dated June 11, 2015, (*please see attached letters*). At that time the Area of Potential Effect was described as "A range starting at I-10 and Broad Street in the Oakdale Historic District, moving northward to Virginia Street then west to Ann Street. The Ann Street border goes north to Springhill Avenue then east to Beauregard Street. The APE follows Beauregard Street and then crosses the Federal Mobile Harbor 500' North of US-90 and 500' South of I-10 to the Eastern Shore."

In 2018 an Interstate Modification Request (IMR) was performed to evaluate potential changes in traffic and congestion related to the proposed project. The IMR indicated that traffic will increase on US-90/98 (Bay Bridge Road) from Bankhead Tunnel to Velma Street, due to diverted traffic avoiding the toll on the Bayway. The projected increase in traffic required the delineation of new APE boundaries to accommodate the study area. The Newly Delineated APE includes a 1000' corridor to the north on either side on US-90 then to Bay Bridge Road, past I-165 to Velma Street. *Please see attached New APE Map.*

Within the Newly Delineated APE, ALDOT will provide a bicycle and pedestrian shared use path from the I-165 southbound on-ramp at Bay Bridge Road to the Cochrane-Africatown USA Bridge. ALDOT will retrofit the Cochrane-Africatown USA Bridge to provide two protected bicycle and pedestrian lanes (one on each side of the bridge). The bicycle and pedestrian path will be a minimum of eight feet wide. ALDOT proposes to provide a shared use path on the south side of Bay Bridge Road and a sidewalk on the north side of Bay Bridge Road with crosswalks at appropriate locations. More detailed studies, design, and coordination with the local community will be required to finalize the details of the bicycle and pedestrian facilities along this route. The length of this proposed corridor is approximately 2.6 miles. The bicycle and pedestrian facilities will be constructed within existing ALDOT right-of-way. This facility will be constructed using Federal and/or state funds and will be completed within the same timeframe as the completion of the Mobile River Bridge and Bayway project. The purpose of this letter is to request your review and concurrence that the subject project will not adversely affect any property listed on or eligible for the NRHP within the Newly Delineated APE.

One National Register-listed Historic District, Africatown and one property eligible for inclusion in the National Register, US-98/90 the Causeway, are located within the project's newly delineated APE. Africatown, also known as AfricaTown USA and Plateau was listed on the National Register under Criterion A; its association with events that have made a contribution to our history and Criterion D; property that is likely to yield information important to our history or prehistory. Located three miles north of downtown Mobile it is bounded by Jakes Lane, Paper Mill and Warren Roads, and Chin and Railroad Streets. Africatown was formed by a group of 32 West African slaves, the last known illegal shipment of slaves to the United States aboard the Clotilda.

US-90/98 is along the *Old Spanish Trail*, property considered eligible for inclusion on the *National Register of Historic Places* under Criterion A; Its association with a Transportation Event. The construction of the Causeway helped complete the *Spanish Trail*, a transcontinental highway designed to increase tourism and commerce on the Gulf of Mexico coast and westward. This area was cleared per Cultural Resources in AHC# 19-0196.

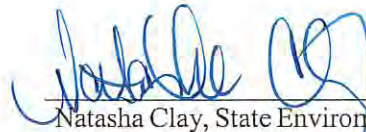
Regarding the archaeological assessment, it is our opinion that there are no archaeological resources within the project's Newly Delineated APE on or eligible for the NRHP. All proposed work will be completed within existing, previously disturbed R-O-W.

ALDOT will provide a Historical Marker to be placed at the entrance of Africatown and Interpretive Panels, with historical information about Africatown and the Mobile River, on the Africatown USA Bridge for the pedestrians and bicyclists. Therefore, it is our determination that, as proposed, *ALDOT Project DPI-0030(005) I-10 Mobile River Bridge and Bayway Project Mobile and Baldwin Counties, Alabama* will not adversely affect the Africatown National Register Historic District. Please review this information. If the SHPO agrees please sign and return a copy to the Design Bureau. Thank you in advance for your attention in this matter. If you should have any questions, please feel free to contact Bill Turner of our staff at 242-6144.

Sincerely,

Steven E. Walker, P.E.  
State Design Engineer

By:



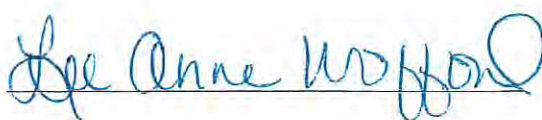
Natasha Clay, State Environmental Administrator  
Environmental Technical Section

Attachments

NC/WBT/pmp

cc: FHWA  
South West Region  
Section 106 Consulting Parties  
Dolha Kayisavera, ETS Project Manager  
Cultural Resources File

CONCUR:



DATE:

2/8/19



U.S. Department  
of Transportation  
**Federal Highway  
Administration**

Alabama Division

June 25, 2015

9500 Wynlakes Place  
Montgomery, AL 36117  
334-274-6350  
334-274-6352  
[Alabama.FHWA@dot.gov](mailto:Alabama.FHWA@dot.gov)

In Reply Refer To:  
HDA-AL

To: All Mobile River Bridge  
Section 106 Consulting Parties

Re: Project DPI-0030(005), Mobile & Baldwin Counties  
I-10 Mobile River Bridge & Bayway Widening  
Section 106 Determination of Effects Revisions  
AHC Reference Numbers: AHC 00-1934; AHC 00-0352

Dear Sir/Madam:

Since the September 23, 2014 Section 106 Consulting Party Meeting, we have been in consultation with the Alabama Historical Commission (ACH) and the Advisory Council on Historic Preservation (ACHP). Based on these discussions and follow-up meetings, we have made some revisions to our Determination of Effects for the referenced project. Attached is our recent correspondence with the AHC.

The primary areas of concern were visual impacts to historic properties within the Church Street East Historic District and the Lower Dauphin Street Historic District. We have determined that there may be an adverse visual effect to these historic districts by the Build Alternatives. The proposed project will be clearly visible from the edges of these historic districts. Due to the extensive modern structures (infill) to the southeast of the historic districts, the visual effects of the project will be limited. However, when the project is combined with the visual effects of the numerous other modern structures within view, a cumulative impact will occur. This cumulative impact may further diminish the settings of these historic districts, which would be an adverse effect. We are hereby notifying you of our finding of additional impacts due to visual changes to the settings of these two historic districts.

We now propose to begin discussing appropriate mitigation measures to resolve and/or minimize any adverse impacts. Ultimately, agreed upon mitigation measures would be included in a Memorandum of Agreement that would not only address adverse visual effects but potential effects to archaeological sites, documentation of the Union Hall, bridge aesthetics, access to the Battleship Park, and measures to avoid impacts from lighting and vibration, as well.

If you have any questions or comments regarding this matter, please contact Ms. Lynne Urquhart of this office at (334) 274-6371 or [lynne.urquhart@dot.gov](mailto:lynne.urquhart@dot.gov).

Sincerely,

Mark D. Bartlett, P. E.  
Division Administrator

MB/lw/af

Attachments:

1. Section 106 Consulting Parties List
2. FHWA Determination Letter May 2015
3. AHC Response June 2015

STATE OF ALABAMA  
ALABAMA HISTORICAL COMMISSION  
468 SOUTH PERRY STREET  
MONTGOMERY, ALABAMA 36130-0900

June 11, 2015

FRANK W. WHITE  
EXECUTIVE DIRECTOR

TEL: 334-242-3184  
FAX: 334-240-3477

Mark D. Bartlett  
U.S. Department of Transportation  
9500 Wynlakes Place  
Montgomery, AL 36117

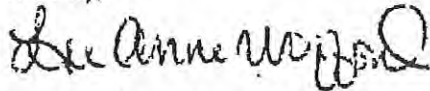
Re: AHC 00-0352  
Widen I-10 from Broad Street in Mobile to East of US 98 in Baldwin County  
Baldwin and Mobile Counties

Dear Mr. Bartlett:

Upon review of the above referenced project, we concur with the adverse affect determination. We look forward to developing the MOA.

We appreciate your commitment to helping us preserve Alabama's historic archaeological and architectural resources. Should you have any questions, please contact Amanda McBride at 334.230.2692 or [Amanda.McBride@preserveala.org](mailto:Amanda.McBride@preserveala.org). Have the AHC tracking number referenced above available and include it with any future correspondence.

Sincerely,



Lee Anne Wofford  
Deputy State Historic Preservation Officer

LAW/AMH/amh



190134



# ALABAMA DEPARTMENT OF TRANSPORTATION

Design Bureau

1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050  
Phone: 334-242-6178 FAX: 334-269-0826



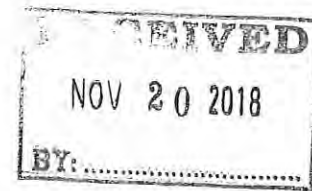
Kaye Ivey

Governor

John R. Cooper

Transportation Director

November 19, 2018



Mrs. Lee Anne Wofford  
Deputy State Historic Preservation Officer  
Alabama Historical Commission  
468 South Perry Street  
Montgomery, Alabama 36130-0900

RE: *ALDOT Project BRF-0016 (522) Cultural Resources Assessment for the Proposed Bridge Replacement on SR-16(US-90) Westbound Over Tensaw-Spanish River; Grade, Drain, Base, Pave Mobile & Baldwin Counties, AL*

Dear Mrs. Wofford:

The Alabama Department of Transportation proposes to replace the westbound bridge on *SR-16(US-90)* over the Tensaw-Spanish River on the Mobile/Baldwin County Line. For your review and concurrence, please find attached one CD and one hard copy of the Cultural Resources Assessment for *ALDOT Project BRF-0016 (522) the Proposed Bridge Replacement on SR-16(US-90) Westbound Over Tensaw-Spanish River; Grade, Drain, Base, Pave Mobile & Baldwin Counties, AL*.

The proposed project is to remove and replace the westbound bridge on *SR-16 (US-90)/(US-98)* over the Tensaw Spanish River between Mobile and Baldwin Counties. The purpose of this project is to replace the existing functionally obsolete and structurally deficient bridge with a modern structure more capable of handling both current and future traffic demands. No additional right-of- way is anticipated.

The bridge replacement is along the *Old Spanish Trail*, property considered eligible for inclusion in the *National Register of Historic Places* under Criterion A; Its association with a Transportation Event. In the Mobile Bay, the original Trail consisted of a two-lane facility carrying bridges over the Tensaw-Spanish River, Apalachee River and Blakeley River from Mobile County to Baldwin County. In 1963-65 construction of a new adjacent two-lane facility was underway. This facility became the Westbound lanes and the original facility became the Eastbound lanes with bridges over the same rivers. In 1995 the bridges in the Eastbound lanes were replaced. The bridge over the Tensaw-Spanish River was considered NR eligible, consequently prior to demolition it was recorded to HAER Documentation standards.

Now 55 years old, the westbound lane bridge of the Tensaw-Spanish River will be removed, and a new bridge replacement will take place south of the existing Eastbound *US-98/90 SR-16* bridge. Constructed in 1965, this concrete stringer will be replaced with a modern structure more capable of handling both current and future traffic demands. This bridge is not considered NR eligible. The proposed project will not adversely affect the Old Spanish Trail route.

Within the vicinity of the proposed project location, approximately one-third mile away, is the Battleship USS Alabama Memorial Park housing two National Historic Landmarks, the USS *Alabama* and the USS *Drum*. The *Proposed Bridge Replacement on SR-16(US-90) Westbound Over Tensaw-Spanish River; Grade, Drain, Base, Pave Mobile & Baldwin Counties, AL* will be viewable from the Battleship USS Alabama Memorial Park; however, given the distance, the proposed project will not adversely affect the NHL properties. There are no other structures within the APE.

Regarding the archaeological assessment, it is our opinion that there are no archaeological resources within the project's APE on or eligible for the NRHP. For the terrestrial areas approaching the bridge, project activities are staying within the existing ROW. The existing ROW includes many paved and gravel parking lots, driveways, overhead and underground utilities. The area has been heavily modified and built up due to road construction activities. Due to these disturbances, no shovel tests occurred. Please see previous Archaeological Assessment attached. No further archaeological assessment was deemed necessary.

Therefore, it is our determination that *ALDOT Project BRF-0016 (522) The Proposed Bridge Replacement on SR-16(US-90)/(US-98) Westbound Over Tensaw-Spanish River; Grade, Drain, Base, Pave Mobile & Baldwin Counties, AL* will not be adversely affect any property found eligible for or included in the NRHP.

Please review this information. If the SHPO agrees please sign and return a copy to the Design Bureau. Thank you in advance for your attention in this matter. If you should have any additional questions, please feel free to contact Bill Turner of our staff at 242-6144.

Sincerely,

Steven E. Walker, P.E.

State Design Engineer

By:

  
Natasha Clay, State Environmental Administrator  
Environmental Technical Section

Enclosures

NC/WBT/pmp

cc: FHWA  
South East Region  
Jesse Chambless, ETS Project Manager  
Cultural Resources File

CONCUR: 

DATE: 11/21/18



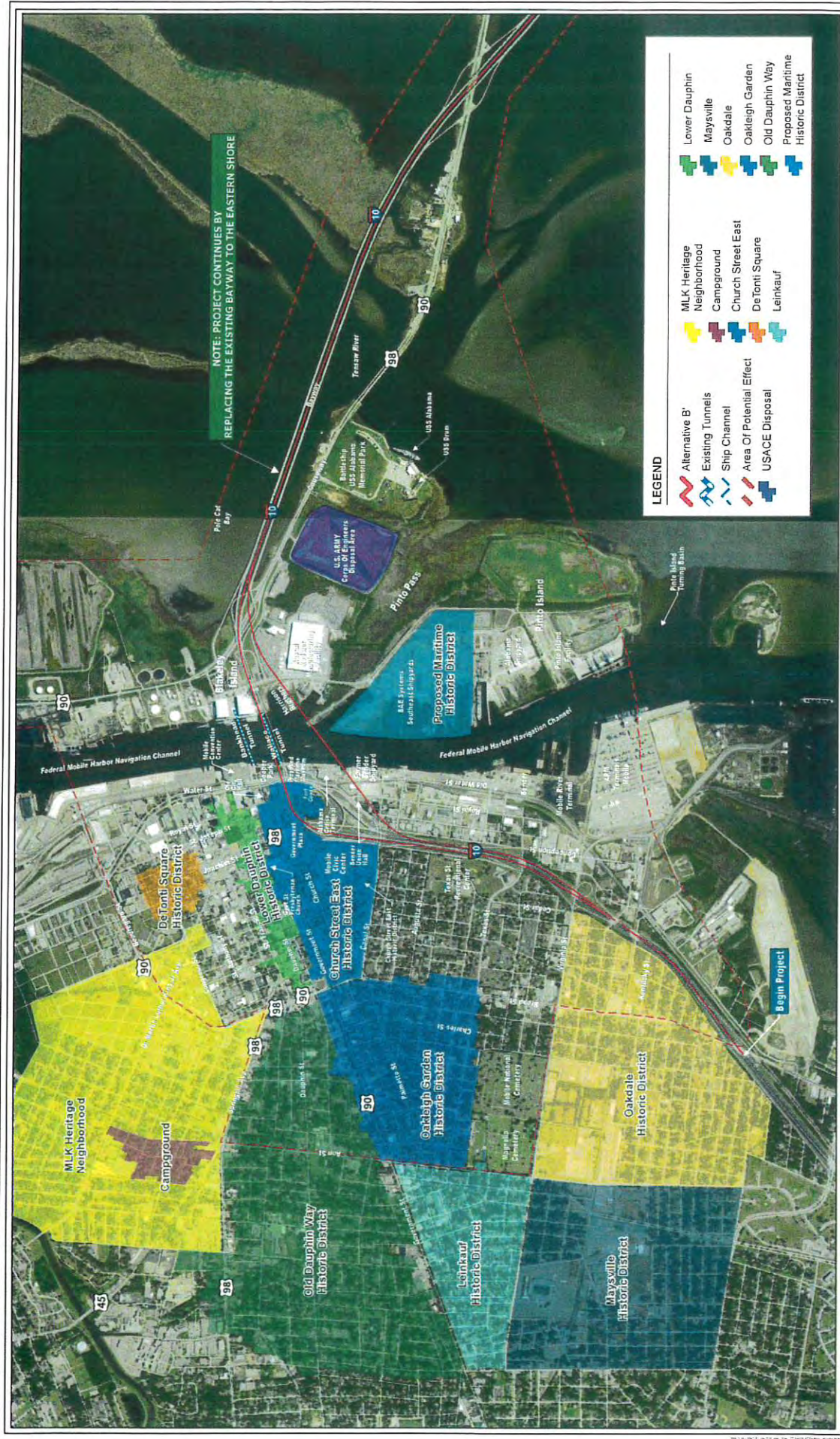
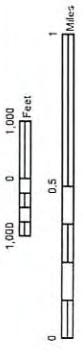


FIGURE 20  
HISTORIC DISTRICTS

PROJECT NO.: 15-1101-0300  
DATE: DECEMBER 2018



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MCDONALD



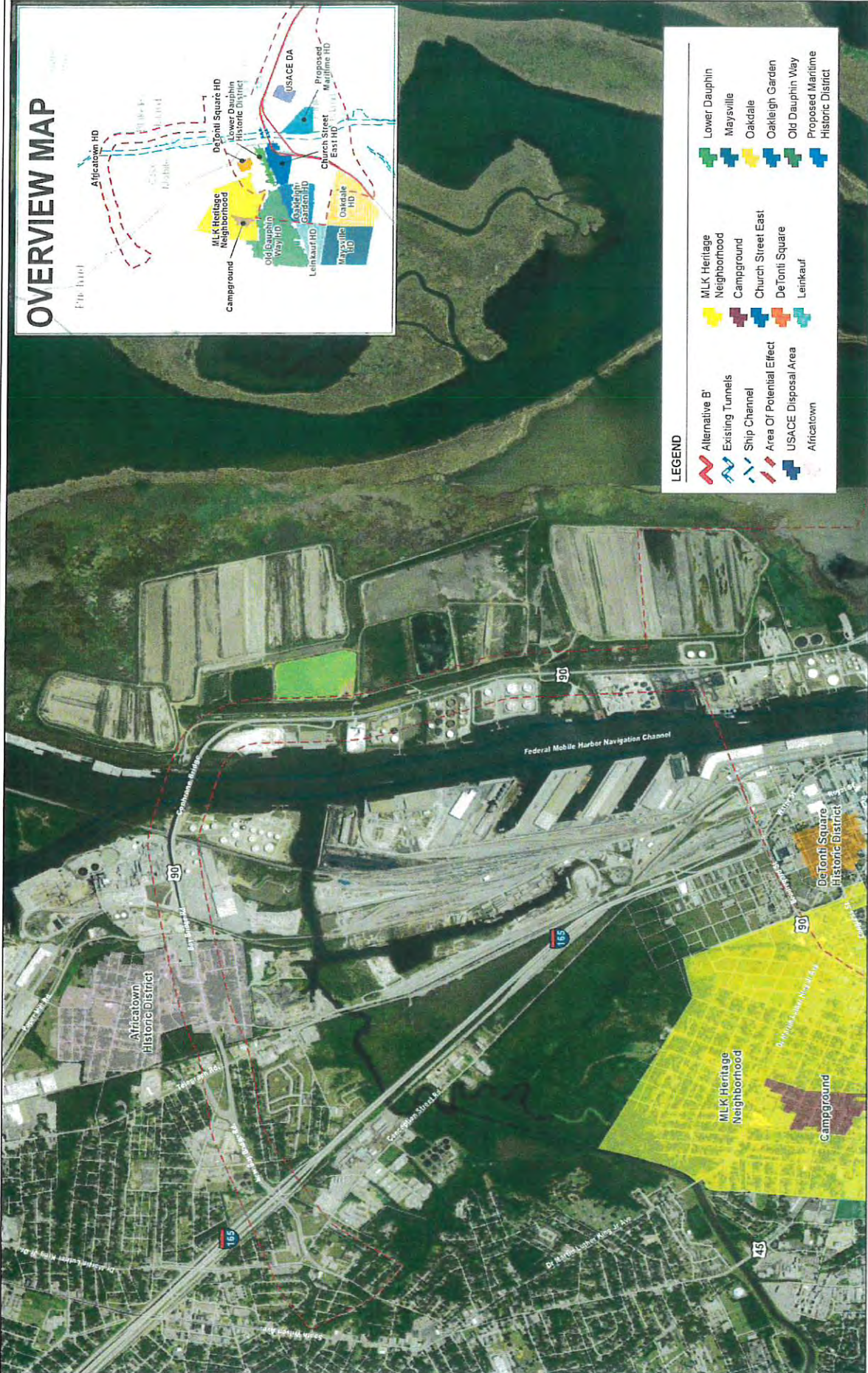
ALABAMA DEPARTMENT OF TRANSPORTATION  
I-10 MOBILE RIVER BRIDGE  
AND BAYWAY PROJECT  
PROJECT No. DPI-0030 (005)  
MOBILE AND BALDWIN COUNTIES, ALABAMA



ORIGINAL PROJECT AREA OF POTENTIAL EFFECT

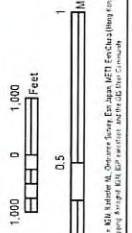


# OVERVIEW MAP



**FIGURE 20B**  
**AFRICATOWN HISTORIC DISTRICT**

PROJECT NO.: 15-1101-0300  
 DATE: DECEMBER 2018



ALABAMA DEPARTMENT OF TRANSPORTATION  
 I-10 MOBILE RIVER BRIDGE  
 AND BAYWAY PROJECT  
 PROJECT No. DPH-0030 (005)  
 MOBILE AND BALDWIN COUNTIES, ALABAMA

M M  
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 MACDONALD  
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ALABAMA DEPARTMENT OF TRANSPORTATION  
 I-10 MOBILE RIVER BRIDGE  
 AND BAYWAY PROJECT  
 PROJECT No. DPH-0030 (005)  
 MOBILE AND BALDWIN COUNTIES, ALABAMA



# NEWLY DELINEATED AREA OF POTENTIAL EFFECT



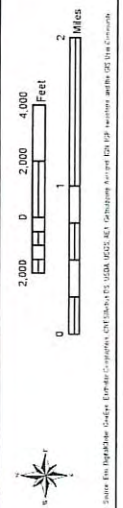


**LEGEND**

Area Of Potential Effect	DeTooni Square	Oakleigh Garden
Africatown	Leinkauf	Old Dauphin Way
Proposed Maritime Historic District	Lower Dauphin	MLK Heritage Neighborhood
Campground	Maysville	
Church Street East	Oakdale	

**FIGURE 20A**  
**HISTORIC DISTRICTS AND**  
**AREA OF POTENTIAL EFFECT**

PROJECT NO.: 15-1101-0300  
 DATE: DECEMBER 2018



ALABAMA DEPARTMENT OF TRANSPORTATION  
 I-10 MOBILE RIVER BRIDGE  
 AND BAYWAY PROJECT  
 PROJECT NO. DPH-0030 (005)  
 MOBILE AND BALDWIN COUNTIES, ALABAMA

**M M**  
**MOBILE**  
**REGIONAL**

**F3R**

DEPARTMENT OF TRANSPORTATION

# PROJECT AREA OF POTENTIAL EFFECT



**AHC# 00-1934 and AHC# 00-0352 Newly Delineated APE Assessment**  
ALDOT Project DPI-0030(005) I-10 Mobile River Bridge and Bayway Project  
Mobile and Baldwin Counties, Alabama

## **INTRODUCTION**

The Area of Potential Effect was originally described as “A range starting at I-10 and Broad Street in the Oakdale Historic District, moving northward to Virginia Street then west to Ann Street. The Ann Street border goes north to Springhill Avenue then east to Beauregard Street. The APE follows Beauregard Street and then crosses the Federal Mobile Harbor 500’ North of US-90 and 500’ South of I-10 to the Eastern Shore.” In 2018 An Interstate Modification Request (IMR) was performed to evaluate potential changes in traffic and congestion related to the proposed project. The IMR indicated that traffic will increase on Bay Bridge Road, the Cochrane-Africatown USA Bridge, US-90 between the Cochrane-Africatown USA Bridge and the Bankhead Tunnel, and the US-90/US-98 Causeway, due to diverted traffic avoiding the toll on the Bayway. Increased traffic will also result in increased congestion on these routes. This additional area expands the project’s APE to include a 1000’ corridor to the north on either side on US 90, to Bay Bridge Road, past I-165 to Velma St. in Pritchard. *Please see attached Newly Delineated APE Maps.*

## **PHYSICAL ENVIRONMENT**

The Mobile-Tensaw Delta includes sections of Baldwin, Clarke, Mobile, Monroe, and Washington counties in southwestern Alabama. The region features numerous interconnected stream systems, floodplains, swamps, bayous, lakes, and forests, and it is home to an abundance of species of flora and fauna. The Mobile Bay Causeway is a unique asset to the Mobile Bay Region. It serves as a vital transportation link, but also provides access to an abundance of natural resources and as an economic generator for the adjacent cities. In a previous project, AHC# 19-0196 *ALDOT Project BRF-0016 (522) Cultural Resources Assessment for the Proposed Bridge Replacement on SR-16(US-90) Westbound Over Tensaw-Spanish River; Grade, Drain, Base, Pave Mobile & Baldwin Counties, AL*, Cultural Resources in this area were cleared. There will be no more discussion of this area in this assessment.

The Africatown Historic District is located approximately three miles north of downtown Mobile. The District includes 253 contributing resources and 203 non-contributing resources. They range from single-family and multi-family residences, businesses, a cemetery, recreational facilities and churches. The unincorporated community was annexed by the City of Mobile in 1948. Africatown's development had two distinct stages. The first stage (1866-1900), located in the southern portion of the district and south of Susie Ansley Street, appears to have been unplanned. The later stage (1900-1945), located in the northern portion of the district, follows typical late 19<sup>th</sup> and early 20<sup>th</sup> century city planning in regard to the street grid pattern. However, housing continued to be built in the area after the last portion was platted in 1945. The housing is comprised generally of modest, one-story, wood-frame buildings reflecting the architectural styles and residential building typologies of the early to mid-twentieth century. The area is well shaded with oaks and pines. An athletic field with a baseball field surrounds the Plateau Community Center at 850 Edwards Street and the John Kidd Park (established in 1968) at 800 & 810 East Street contains a swimming pool and playground for community children. The Old Plateau Cemetery, an integral part of Africatown, lies south of Bay Bridge Road, but north of Chin Street. It is bordered on the west by the Bay Bridge Road Cut-Off. The Bay Bridge Road is a major highway that slices through the southern portion of Africatown from west to east on its way to the Cochrane Bridge. – *from Africatown NR Nomination*

## **ARCHITECTURAL AND HISTORICAL BACKGROUND**

A group of West Africans arrived in Mobile on the last documented slave ship to reach the United States and ultimately settled in the area now known as Africatown. In July 1860 the Clotilda sailed secretly into Mobile Bay with 110 men, women, and children hailing from Yoruba, Atakora, Nupe and Dendi. Most of the group had been captured in present day Benin. The international slave trade had been made illegal in the United States in 1808. Fearing they would be caught; the captain transferred the slaves to a riverboat and burned and sunk the Clotilda in Big Bayou Canot.

After their arrival in Mobile about 25 slaves were sold upriver. Timothy Meaher, who had arranged the illegal expedition, and his family kept 60 slaves. After the end of the Civil War, those formerly enslaved on a plantation in Clarke County joined their fellow West Africans in the Plateau area. Local legend has it that the Clotilda Africans joined a community of Moors, who had settled in the area. Unable to afford passage back to Africa, they

remained in Mobile and founded African Town, the first town established and controlled by Africans in the United States.

Working in local shipyards and mills, they saved money to buy land including some from their former owners. African Town originally included a 50-acre community in the Plateau area and a smaller one, Lewis Quarters, which consisted of seven acres over a mile to the west of the larger settlement. Lewis Quarters was named after one of its founders, Charlie Lewis. The settlers appointed Peter Lee as their chief and established a governmental system based on African law.

The residents of African Town built the first school in the area. In 1872 they built Old Landmark Baptist Church, which is now Union Missionary Baptist Church. While the community retained much of their West African culture, construction of the church signaled the conversion to Christianity of many of the Africans. They were a tight-knit community known for sharing and helping one another but reportedly had tense relations with both Whites and African Americans and so largely kept to themselves.

Cudjo Lewis, who was nineteen when he boarded the *Clotilda*, is one of the most renowned of Africatown's original settlers. He rang the bell at Old Landmark Baptist Church, worked as a shingle maker and shared the story of the *Clotilda* Africans with journalists, writers and historians. Having died in 1935 at the age of 94, Lewis was the last known African-born slave in the United States and the only one to have been captured on film. Sources: *Encyclopedia of Alabama*, *Dreams of Africa in Alabama* (Diouf), *The Slave Ship Clotilda and the Making of AfricaTown, USA: Spirit of Our Ancestors* (Robertson).

## **METHODOLOGY**

The *National Register of Historic Places* was consulted to determine if there were any properties listed on or eligible for listing within the vicinity of the Newly Delineated APE. Africatown, also known as AfricaTown USA and Plateau was listed on the National Register under Criterion A; its association with events that have made a contribution to our history and Criterion D; property that is likely to yield information important to our history or prehistory.

## **PROPERTY INVENTORY AND EVALUATIONS**

### **Resource: Africatown USA**

The Africatown Historic District is located approximately 3 miles north of downtown Mobile and consists of 455 primary buildings covering an area of approximately 921 acres. The district is bounded on the north by Jakes Lane and Whitley Street, on the east by Paper Mill Road, on the south by Chin Street and on the west by Railroad and Author Streets.

The district is primarily residential, with 443 out of 455 resources constructed as housing. Residences are concentrated throughout the district except along Bay Bridge Road. Most of the residential buildings are one story in height with a few divided between one-and-one half story and two-story examples. The majority of all resources are frame construction. The remainder are typically brick or brick veneer. The remaining 3% of building stock is comprised of schools, churches, and businesses.

The Africatown Historic District (1866-1962) was settled in the years immediately following the Civil War by the last group of Africans brought into the United States for the purposes of slavery. The land-use in a good portion of the area does not appear to follow any particular plan. The blocks located in the southern portion of the district, which was the site of the earliest settlement, are irregularly shaped. Those in the northern portion of the district, which were platted between 1900 and 1945, follow a more usual grid-like pattern. Most of these blocks in the northern portion are approximately 300 feet by 400 feet. Also, these streets are asphalt paved and mostly concrete curbed with a few being flanked by concrete sidewalks. Those streets in the older, southern portion are asphalt paved with a few appearing to follow the contour of the land.

Most streets in Africatown average a width of 50 feet, although Bay Bridge Road is a major highway and is approximately 115 foot wide. A few streets in the oldest portion of the district are only 15 feet wide. The streets are lined with rectangular lots averaging a frontage of 50 feet by 150 feet deep. Occasionally, lots have been doubled or subdivided.

Houses in the district are set back 15 to 20 feet with a few set back 30 feet and with informally landscaped yards, although a few are more heavily planted and well shaded. In a few instances houses are situated so that their entrances are at right angles to the street. Occasionally, one finds the use of landscaping stones or concrete blocks





**Bay Bridge Road Looking East Towards Africatown USA Bridge**



**Bay Bridge Road Looking West From the Africatown USA Bridge**

AHC# 00-1934 and AHC# 00-0352 Newly Delineated APE Assessment  
ALDOT Project DPI-0030(005) I-10 Mobile River Bridge and Bayway Project  
Mobile and Baldwin Counties, Alabama





**Bay Bridge Road Looking East Near Union Baptist Church**



**Bay Bridge Road Looking North Towards Africatown**

AHC# 00-1934 and AHC# 00-0352 Newly Delineated APE Assessment  
ALDOT Project DPI-0030(005) I-10 Mobile River Bridge and Bayway Project  
Mobile and Baldwin Counties, Alabama



used for retaining walls. In many cases these are decorative, but a few provide stability for areas in the eastern and southern portions of the district that are hilly.

Housing forms and styles throughout the district reflect the range of styles and forms popular from 1900 to 1962. Most of the housing forms found in the district include one story T-plans, irregularly massed plans, shotguns, bungalows, Minimal Traditional and Ranch. Because of the working-class nature of the district and the economic disadvantages faced by African-Americans, these forms, which are mostly in the Victorian and Craftsman styles, are often very modest. Ac. 1870 cottage remains in the district; however, it has been altered extensively at the rear of the structure. *Excerpts from the Africatown NR Nomination*

### **POTENTIAL EFFECTS ON PROPERTIES**

Results of an Interstate Modification Request (IMR) indicated that traffic will increase on US-90/98 (Bay Bridge Road) from Bankhead Tunnel to Velma Street, due to diverted traffic avoiding the toll on the Bayway. The projected increase in traffic required the delineation of new APE boundaries to accommodate the study area. The Newly Delineated APE includes a 1000' corridor to the north on either side on US-90 then to Bay Bridge Road, past I-165 to Velma Street. *Please see attached New APE Map.*

ALDOT will provide a bicycle and pedestrian shared use path from the I-165 southbound on-ramp at Bay Bridge Road to the Cochrane-Africatown USA Bridge. ALDOT will retrofit the Cochrane-Africatown USA Bridge to provide two protected bicycle and pedestrian lanes (one on each side of the bridge). The bicycle and pedestrian path will be a minimum of eight feet wide. ALDOT also proposes to provide a shared use path on the south side of Bay Bridge Road and a sidewalk on the north side of Bay Bridge Road with crosswalks at appropriate locations. The length of this proposed corridor is approximately 2.6 miles. The bicycle and pedestrian facilities will be constructed within existing ALDOT right-of-way.

Consultations between the Alabama SHPO, Federal Highway Administration, the Advisory Council on Historic Preservation, ALDOT, and Section 106 Consulting Parties, led to stipulations put forth in a *draft* Memorandum of Agreement. The MOA will not only address adverse visual effects but potential effects to archaeological sites, documentation of the Union Hall, bridge aesthetics, access to the Battleship Park, and measures to avoid impacts from lighting and vibration.

Bay Bridge Road approximately 115 foot wide. The proposed bicycle and pedestrian facilities will be constructed within existing ALDOT right-of-way in areas that have been previously disturbed. Sidewalks currently exist along portions of Bay Bridge Road, and the addition of a shared use path would not result in adverse effects. There will be no acquisition of property from the historic district.

There are no archaeological resources within the project's Newly Delineated APE on or eligible for the NRHP. All proposed work will be completed within existing, previously disturbed R-O-W. The historic district is located approximately three miles north of the proposed project therefore construction of the new Mobile River Bridge and approach structures would not result in adverse visual effects. Because of the distance, the district would not be affected by the roadway or aesthetic lighting that will be included as part of the proposed project. Based on the results of the Final Vibrations Study prepared for this project, the Africatown Historic District would be too far from the proposed pile driving activities to experience vibrations from construction activities.

This historic district is located in a developed environment, with industry surrounding it and is in close proximity to the existing transportation network. The properties were reviewed, and the increase in the projected noise levels for the proposed project over the existing condition is less than 2 dBA, which is imperceptible by most people. Air quality was analyzed at the intersection that would exhibit the worst congestion for the proposed project, which occurs on Bay Bridge Road at I-165, west of the Africatown Historic District limits. No air quality impacts were identified for the proposed project.

ALDOT will provide a Historical Marker to be placed at the entrance of Africatown and Interpretive Panels, with historical information about Africatown and the Mobile River, on the Africatown USA Bridge for the pedestrians and bicyclists. Therefore, it is our determination that, as proposed, *ALDOT Project DPI-0030(005) I-10 Mobile River Bridge and Bayway Project Mobile and Baldwin Counties, Alabama* will not adversely affect the Africatown National Register Historic District. Therefore, it is our submitted determination that the proposed activities associated with the Newly Delineated APE Assessment of ALDOT Project DPI-0030(005) I-10 Mobile River

Bridge and Bayway Project Mobile and Baldwin Counties, Alabama will not adversely affect any properties on or eligible for the National Register of Historic Places.

**AHC# 00-1934 and AHC# 00-0352 Newly Delineated APE Assessment**  
ALDOT Project DPI-0030(005) I-10 Mobile River Bridge and Bayway Project  
Mobile and Baldwin Counties, Alabama



**February 27, 2019 E-mail from Herndon Inge to ALDOT and  
Responses to E-mail**

**From:** [Ericksen, Matthew](#)  
**To:** [Missi Shumer](#)  
**Subject:** FW: Mobile River Crossing- Interstate 10  
**Date:** Wednesday, February 27, 2019 1:13:03 PM

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fyi

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**From:** Powell, Don  
**Sent:** Wednesday, February 27, 2019 12:58 PM  
**To:** Ericksen, Matthew <[ericksenm@dot.state.al.us](mailto:ericksenm@dot.state.al.us)>; Aaron, Brian <[aaronb@dot.state.al.us](mailto:aaronb@dot.state.al.us)>; Perry, Edwin L. <[perrye@dot.state.al.us](mailto:perrye@dot.state.al.us)>; Wood, Andrew <[wooda@dot.state.al.us](mailto:wooda@dot.state.al.us)>  
**Subject:** Fw: Mobile River Crossing- Interstate 10

FYI

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**From:** Herndon Inge III <[hinge@herndoninge.com](mailto:hinge@herndoninge.com)>  
**Sent:** Wednesday, February 27, 2019 12:09 PM  
**To:** Clay, Natasha  
**Cc:** Calametti, Vince; Powell, Don  
**Subject:** Mobile River Crossing- Interstate 10

Ms. Clay,

The Area of Potential Effect includes neighborhoods, buildings and cemeteries on the National Register of Historic Places and the traffic, congestion, chance of damages from increased traffic, noise pollution, air pollution and vibration pollution to historic assets of OUR community would be minimal if the route was moved away from these historic assets.

The bridge spires and road surface and the traffic will adversely impact the historic assets, and the increased of OFF- Interstate 10 traffic will adversely impact the historic assets, and the present route is in violation of almost half a century of highway design guidelines of the Federal Highway Administration and other highway design agencies.

The traffic that leaves Interstate 10 to avoid the toll will exit directly into historic neighborhoods and by historic places, causing actual and threatened damages.

If the highway designers agree that all of this traffic will exit Interstate 10 to avoid the toll, then there is less need for the I-10 bridge, at all.

This bridge routing is fraught with damages that will be long lasting to all residents of OUR community and its historic assets.

STOP THE BRIDGE.

Herndon Inge

**Disposition of Comments Received from Mr. Herndon Inge**

**E-mail Dated February 27, 2019**

	<b>Comment</b>	<b>Response</b>
1.	The Area of Potential Effect includes neighborhoods, buildings and cemeteries on the National Register of Historic Places and the traffic, congestion, chance of damages from increased traffic, noise pollution, air pollution and vibration pollution to historic assets of OUR community would be minimal if the route was moved away from these historic assets.	Alternative B' has been identified as the preferred alternative because it shifts the bridge as far away from downtown Mobile as possible while minimizing impacts on the maritime industry. One of the Build Alternatives, Alternative C, is located further to the south from the historic neighborhoods and downtown is still under consideration. However, this alternative would go directly above and require acquisition of right-of-way from the BAE Systems Southeast Shipyards, which is a maritime historic district eligible for listing on the National Register of Historic Places. The acquisition of ROW from the district would result in a "use" of a Section 4(f) resource as described in Chapter 5 of the DEIS and Supplemental DEIS.
2.	The bridge spires and road surface and the traffic will adversely impact the historic assets, and the increased of OFF- Interstate 10 traffic will adversely impact the historic assets, and the present route is in violation of almost half a century of highway design guidelines of the Federal Highway Administration and other highway design agencies.	<p>FHWA has determined that the proposed project would have adverse visual effects on two historic districts. Mitigation measures outlined in the MOA will be implemented to offset unavoidable adverse visual effects. The historic districts and historic resources will continue to be eligible for listing and/or listed on the National Register of Historic Places.</p> <p>NEPA requires that Federal agencies consider the effects of their proposed actions on the natural, social, and economic environment and that impacts that are unavoidable be mitigated. The proposed project has been developed in a manner that avoids and/or minimizes adverse effects to the extent practicable. Mitigation measures for the adverse visual effects on the Church Street East Historic District and the Lower Dauphin Street Historic District are defined in the MOA.</p>

	<b>Comment</b>	<b>Response</b>
3.	The traffic that leaves Interstate 10 to avoid the toll will exit directly into historic neighborhoods and by historic places, causing actual and threatened damages.	Traffic diversion impacts on historic resources are addressed in Section 4.13 of the Supplemental DEIS.
4.	If the highway designers agree that all of this traffic will exit Interstate 10 to avoid the toll, then there is less need for the I-10 bridge, at all.	Sections 3.6, 3.7, and 4.4.1 of the Supplemental DEIS discuss traffic and tolling. The traffic models do not indicate that all of the traffic will exit I-10 to avoid the toll. The traffic diversion assumptions and associated impacts identified in the Supplemental DEIS are based upon the worst-case scenario with the maximum toll rates in place. It is anticipated that the Concessionaire selected to design, build, finance, operate, and maintain the project will set the toll rate at an amount that will encourage drivers to use the toll and reduce the amount of traffic that diverts to the non-tolled route, as this would maximize the Concessionaire's return on its investment.
5.	This bridge routing is fraught with damages that will be long lasting to all residents of OUR community and its historic assets.	The Draft MOA contained in Appendix L includes mitigation measures to offset adverse effects on historic resources.

**March 12, 2019 Meeting Minutes**

# Mobile River Bridge and Bayway Project

## Section 106 Consulting Party Meeting

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Date: March 12, 2019  
Time: 10:00 a.m. – 12:00 p.m.  
Location: ALDOT, Southwest Region, Building T, 1701 I-65 West Service Road North, Mobile, Alabama 36618

### **MEETING SUMMARY**

#### **I. Welcome/Introductions**

ALDOT opened the meeting by welcoming everyone and thanking the Consulting Parties for participating in the meeting. Each attendee stated their name and who they represented. A sign-in sheet is attached to this meeting summary.

#### **II. Project Presentation**

The purpose of the meeting was to: provide the newer Consulting Parties with an interest in Africatown an overview of the project and discuss the changes that have occurred in the project since the DEIS was signed; discuss the topics that are included in the Draft Memorandum of Agreement (MOA); and identify the next steps in the Section 106 consultation process. An agenda is attached to this meeting summary.

The project team gave a presentation, a copy of which is attached to this meeting summary. The presentation covered the following items:

- Purpose and need,
- Project scope and limits,
- Project history,
- Activities since the July 2014 Draft Environmental Impact Statement was signed,
- Change from traditional design-bid-build process to an alternative delivery method,
- NEPA status and Section 106 Consultation activities,
- Expanded area of potential effect (APE) and new Consulting Parties,
- Draft MOA topics, and
- Next Steps.

Questions were taken throughout the presentation, and a court reporter was present to prepare a transcript. The transcript will be included in the project record when it is received.

ALDOT noted that the Draft MOA had been revised based on previous comments from the Consulting Parties and would be revised again, as needed, to address comments received. A Final MOA signed by the Advisory Council on Historic Preservation, Federal Highway Administration, Alabama Historical Commission, and the Alabama Department of Transportation will be included in the combined Final Environmental Impact Statement/Record of Decision.



### **III. Section 106 Consulting Party Discussion**

Reverend Williams of Yorktown Missionary Baptist Church asked if churches in the Africatown area had been contacted. The project team stated that a variety of individuals and organizations who have expressed an interest in the Africatown Historic District were invited to participate as a Section 106 Consulting Party and five had accepted the invitation. Yorktown Missionary Baptist Church did not receive an invitation, and ALDOT stated that they would send the package of Section 106 information to Reverend Williams.

Reverend Williams recommended adding representatives from Africatown to the Aesthetic Steering Committee for the project.

Ramsey Sprague represented the Africatown C.H.E.S.S. at the meeting. He asked if future Section 106 Consultation meetings would be held and asked about the process to resolve disagreements in opinions about adverse effects on the Africatown Historic District. The project team stated that additional meetings can be held if needed. The Section 106 MOA identifies commitments to avoid, minimize, and/or mitigate adverse effects under Section 106 to historic resources. It was also noted that the Section 106 consultation process does not end with the signing of the MOA or the signing of the environmental document. Consultation will continue through the design, construction, and post-construction phases of the project to make sure the environmental commitments and stipulations in the MOA are followed.

The difference between Section 106 regulations, which focus on effects of a project on historic resources, and impacts to environmental justice communities was also discussed. The State Historic Preservation Office (SHPO) stated that they review historic resources for adverse effects under Section 106 regulations, not under the Executive Order for environmental justice. This project proposes to disturb areas within ALDOT's previously disturbed existing right-of-way which does not qualify as adverse effects under Section 106. Therefore, SHPO concurred with the no adverse effect finding for the Africatown Historic District. ALDOT acknowledged that the proposed project is expected to have adverse impacts on the Africatown community, and ALDOT has reached out to the community and will continue to reach out to the community to discuss those impacts and mitigation measures. A meeting is scheduled for Tuesday, March 19, 2019, at the Union Missionary Baptist Church with Councilman Manzie to discuss the project and the potential impacts on the Africatown community. Input is needed from the community regarding their concerns about how the project may impact Africatown from an environmental justice perspective and how ALDOT can work with the community to address those concerns. The representatives from Africatown were encouraged to attend that meeting to have their voices heard and to provide input to ALDOT. The Africatown Consulting Parties were asked to make sure they provide their mailing addresses and contact information before they leave so that they can be invited to participate in future environmental justice meetings and discussions.

Ramsey Sprague (C.H.E.S.S.) also stated that he believes the Union Missionary Baptist Church would be adversely affected by increased traffic because cars would not be able to get in or out of the church. ALDOT stated that they are going to place a traffic signal at the Union Missionary Baptist Church to make sure access to and from the church is provided.

The National Trust for Historic Preservation (NTHP) stated that they may disagree with the no adverse effect determination made by SHPO. SHPO stated that the Section 106 regulations do

not consider disturbance within previously disturbed right-of-way an adverse effect on a historic property.

The NTHP questioned whether tolling on this project is viable and whether a private partner will choose to take on this project. ALDOT stated that a traffic and revenue study has been prepared for this project and will be made available on the project website.

**IV. Closing/Adjourn**

ALDOT stated that the presentation from the meeting, along with the sign-in sheet, would be e-mailed to participants. Additional comments and questions on the Draft MOA or the Section 106 Consultation process as a whole should be sent to Natasha Clay at ALDOT ([clayn@dot.state.al.us](mailto:clayn@dot.state.al.us)). ALDOT thanked everyone for attending the meeting.

**Attachments:**

- 1) List of Attendees
- 2) Meeting Agenda
- 3) PowerPoint Presentation

**List of Attendees (in alphabetical order by first name)**

The following individuals attended the meeting on March 12, 2019:

<b>Name</b>	<b>Organization</b>	<b>E-mail</b>
Allison Oakes	ALDOT	<a href="mailto:oakesa@dot.state.al.us">oakesa@dot.state.al.us</a>
Amanda McBride	Alabama Historical Commission	<a href="mailto:Amanda.mcbride@ahc.alabama.gov">Amanda.mcbride@ahc.alabama.gov</a>
Anderson Flen	Africatown	<a href="mailto:Aflen30@yahoo.com">Aflen30@yahoo.com</a>
Andrew Wood	ALDOT	<a href="mailto:wooda@dot.state.al.us">wooda@dot.state.al.us</a>
Betsy Merritt (by phone)	National Trust for Historic Preservation	<a href="mailto:emerritt@savingplaces.org">emerritt@savingplaces.org</a>
Brandon Brazil	ALDOT	<a href="mailto:brazilb@dot.state.al.us">brazilb@dot.state.al.us</a>
Chester Patterson	City of Spanish Fort	<a href="mailto:buildingofficial@cityofspanishfort.com">buildingofficial@cityofspanishfort.com</a>
Chris Kinder	Alabama Historical Commission	<a href="mailto:Chrisopher.kinder@ahc.alabama.gov">Chrisopher.kinder@ahc.alabama.gov</a>
Chris Williams	Yorktown Missionary Baptist Church	<a href="mailto:pastorymbc@bellsouth.net">pastorymbc@bellsouth.net</a>
Dolha Kayisavera	ALDOT	<a href="mailto:kayisaverad@dot.state.al.us">kayisaverad@dot.state.al.us</a>
Edwin Perry	ALDOT	<a href="mailto:perrye@dot.state.al.us">perrye@dot.state.al.us</a>
Elizabeth Harris	Condé-Charlotte Museum House	<a href="mailto:Ewharris85@comcast.net">Ewharris85@comcast.net</a>
James Hope	Hope Center	<a href="mailto:jamesmhope@bellsouth.net">jamesmhope@bellsouth.net</a>
Ted Isham (by phone)	Seminole Nation of Oklahoma	<a href="mailto:isham.t@sno-nsn.gov">isham.t@sno-nsn.gov</a>
Jesse Chambless	ALDOT	<a href="mailto:chamblessj@dot.state.al.us">chamblessj@dot.state.al.us</a>
John Sledge	Mobile Historic Development Commission	<a href="mailto:sledge@cityofmobile.org">sledge@cityofmobile.org</a>
Leanne Waller-Trupp	ALDOT	<a href="mailto:truppl@dot.state.al.us">truppl@dot.state.al.us</a>
Louise Rayford	Condé-Charlotte Museum House	<a href="mailto:frayford@aol.com">frayford@aol.com</a>
Lynne Urquhart	Federal Highway Administration	<a href="mailto:Lynne.urquhart@dot.gov">Lynne.urquhart@dot.gov</a>
Lynne Price	USS ALABAMA Battleship Memorial Park	<a href="mailto:icobb@ussalabama.com">icobb@ussalabama.com</a>
Mandy Ranslow (by phone)	Advisory Council on Historic Preservation	<a href="mailto:mranslow@achp.gov">mranslow@achp.gov</a>
Mark Bartlett	Federal Highway Administration	<a href="mailto:Mark.bartlett@dot.gov">Mark.bartlett@dot.gov</a>
Mary Lee Montgomery	Condé-Charlotte Museum House	<a href="mailto:condecharlottehouse@gmail.com">condecharlottehouse@gmail.com</a>
Missi Shumer	Consultant – ALDOT Procurement Advisory Team	<a href="mailto:missi@shumerconsulting.com">missi@shumerconsulting.com</a>
Natasha Clay	ALDOT	<a href="mailto:clayn@dot.state.al.us">clayn@dot.state.al.us</a>
Nick Amberger	City of Mobile	<a href="mailto:Nick.amberger@cityofmobile.org">Nick.amberger@cityofmobile.org</a>
Patrick Hickox	Consultant – ALDOT Procurement Advisory Team	<a href="mailto:Patrick.hickox@hrdinc.com">Patrick.hickox@hrdinc.com</a>
Pat Patterson	ALDOT	<a href="mailto:pattersonp@dot.state.al.us">pattersonp@dot.state.al.us</a>
Ramsey Sprague	C.H.E.S.S./MEJAC	<a href="mailto:infomejac@gmail.com">infomejac@gmail.com</a>
Stephanie Dragotta	ALDOT	<a href="mailto:dragottas@dot.state.al.us">dragottas@dot.state.al.us</a>
Steve Walker	ALDOT	<a href="mailto:walkers@dot.state.al.us">walkers@dot.state.al.us</a>
Wade Henry	ALDOT	<a href="mailto:henryw@dot.state.al.us">henryw@dot.state.al.us</a>
William Lowe	Alabama Historical Commission	<a href="mailto:William.lowe@ahc.alabama.gov">William.lowe@ahc.alabama.gov</a>

# Mobile River Bridge and Bayway Project Section 106 Consulting Party Meeting

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Date: March 12, 2019

Time: 10:00 a.m.

## **AGENDA**

- I. Welcome/Introductions
- II. Project Overview
- III. Review of Activities since July 2014 Draft Environmental Impact Statement (DEIS)
- IV. Expanded Area of Potential Effect and New Consulting Parties
- V. Draft Memorandum of Agreement (MOA)
- VI. Next Steps
- VII. Discussion
- VIII. Closing



# Section 106 Consulting Party Meeting

Project DPI-0030(005)  
March 12, 2019

[MobileRiverBridge.com](http://MobileRiverBridge.com)



1

## Today's Agenda

- Welcome
- Project Overview
- Review of Activities since July 2014 DEIS
- Expanded Area of Potential Effect and New Consulting Parties
- Draft Memorandum of Agreement
- Next Steps



2



# Project Overview






MobileRiverBridge.com

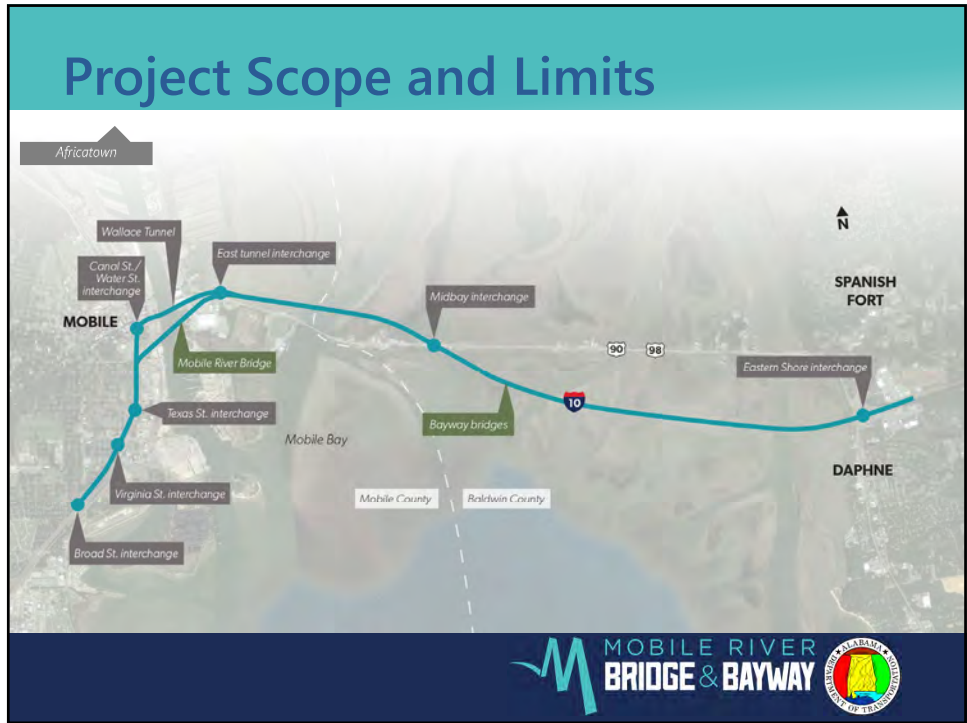
3

## Purpose and Need

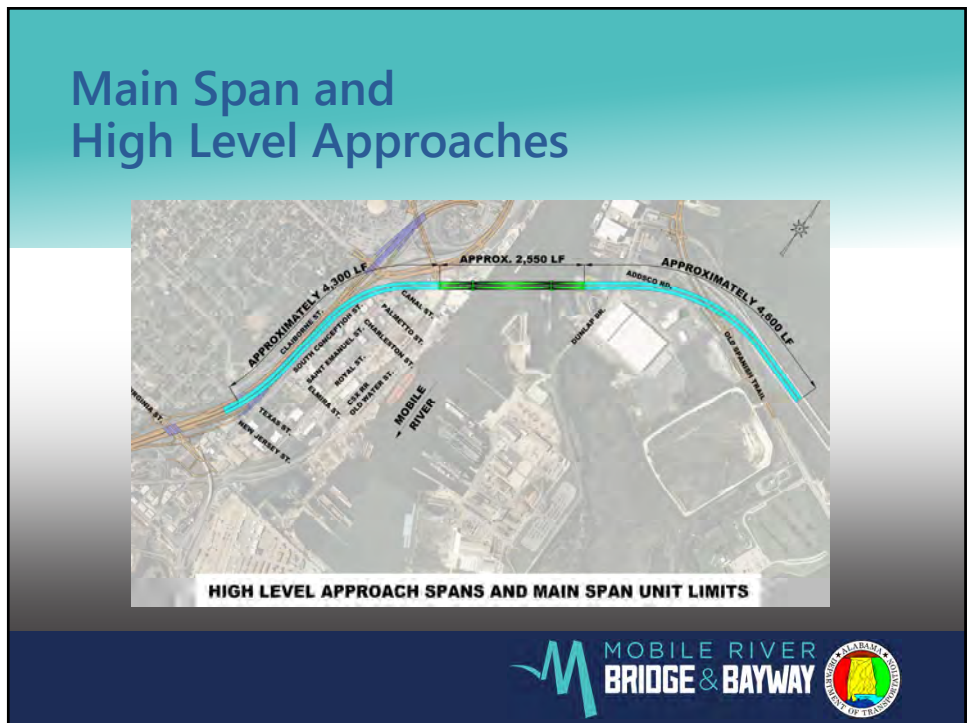
- Increase capacity of I-10 to meet existing and predicted future traffic volumes
- Provide a more direct route for vehicles transporting hazardous materials
- Minimize impacts to Mobile's maritime industry



4

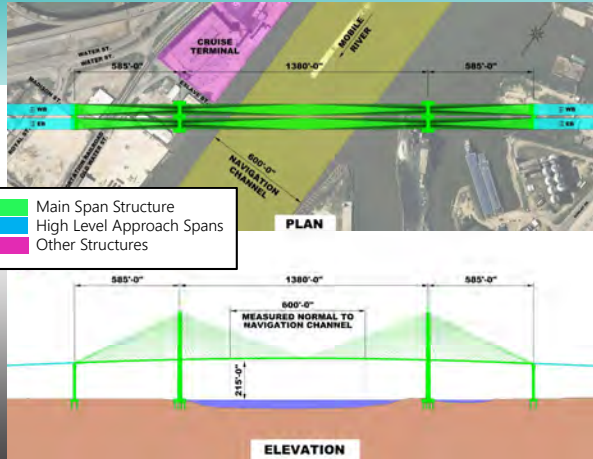


5



6

# Main Span Bridge



- Design Requirements
  - Structure type
  - Cross-section options
  - Span length
  - Vertical clearance
  - Horizontal clearance



7

# Bridge Height Comparison



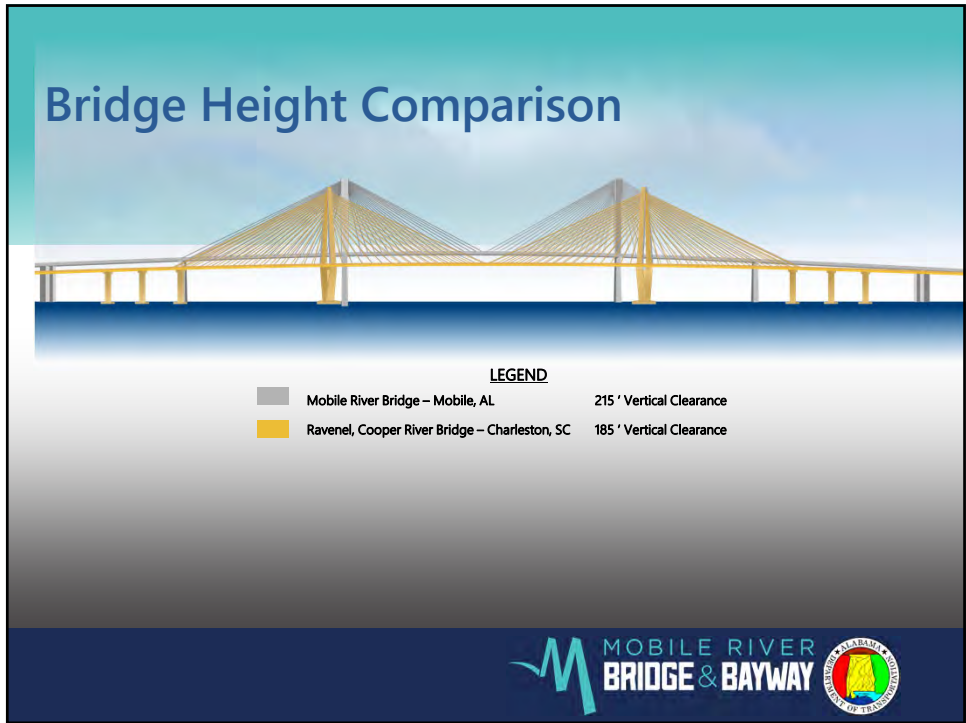
**LEGEND**

- Mobile River Bridge – Mobile, AL      215' Vertical Clearance
- Cochrane-Africatown Bridge – Mobile, AL      155' Vertical Clearance

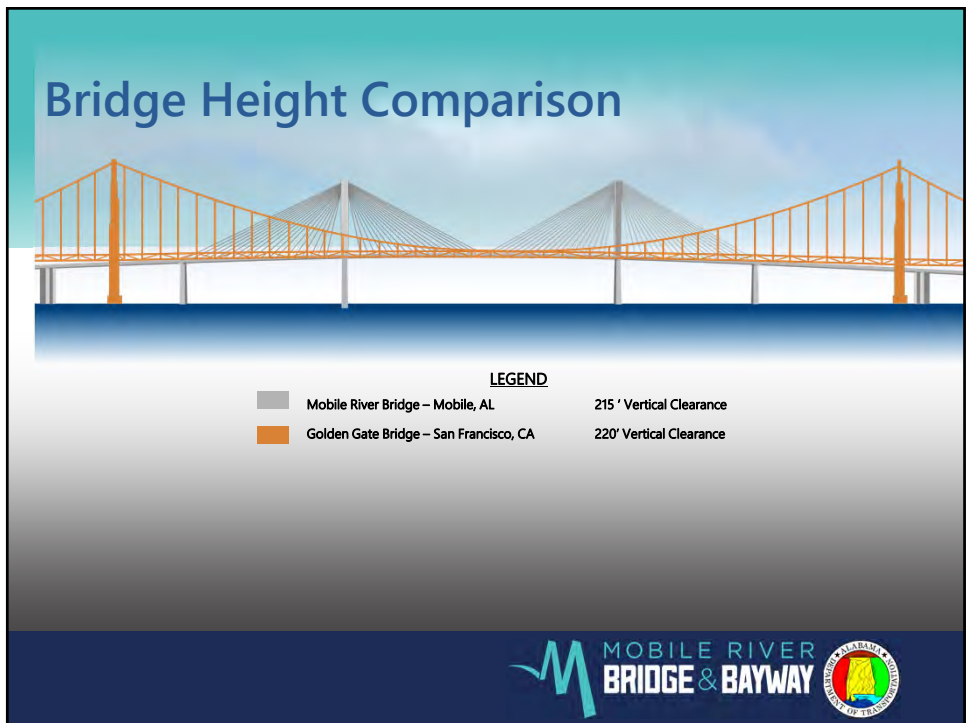


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10

## West Side High Level Approach Span



11

## East Side High Level Approach Span



12

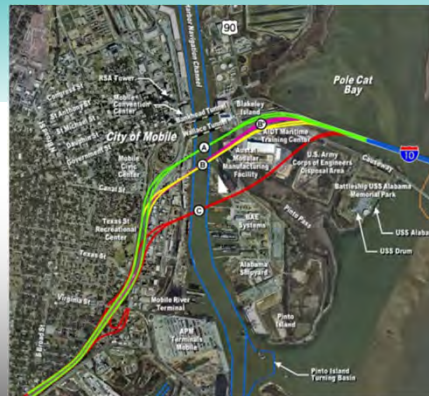
## Conceptual Renderings



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## Project History

- Alternatives Analysis
- Identification of Preferred Alternative (B')
- No Build Alternative
- Signed DEIS July 2014
  - Included environmental commitments
- Public Hearings September 2014



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# Review of Activities since July 2014 DEIS






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## Additional Studies

- Geotechnical Studies
- Survey
- Storm Surge Analysis
- Tolling and Traffic
- Bike/Ped Alternatives
- Hazardous Materials
- Noise and Air
- Draft Mitigation Plan
- Refinements to Alternative B'



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# Refinements to Alternative B'






MobileRiverBridge.com

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## West Mainline Alignment

- Shifted further away from downtown Mobile, reducing impacts on neighborhoods




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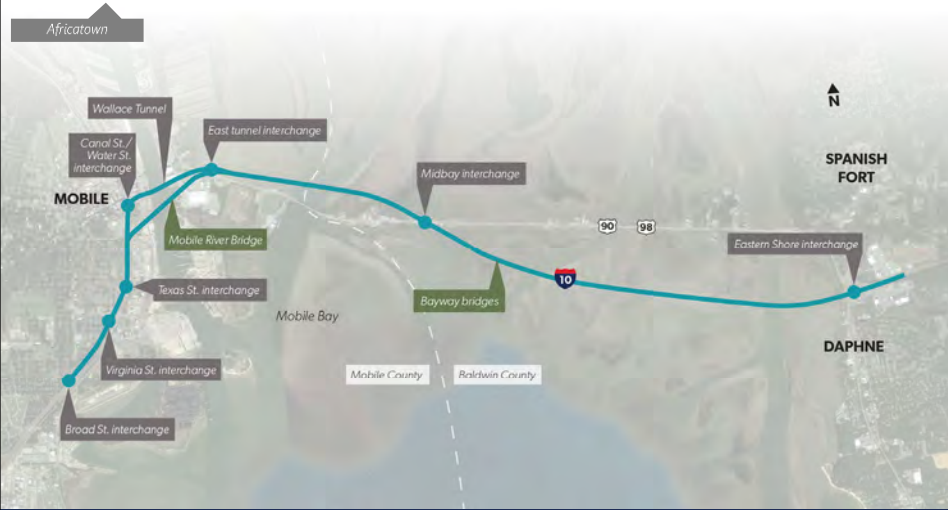
# Interchanges





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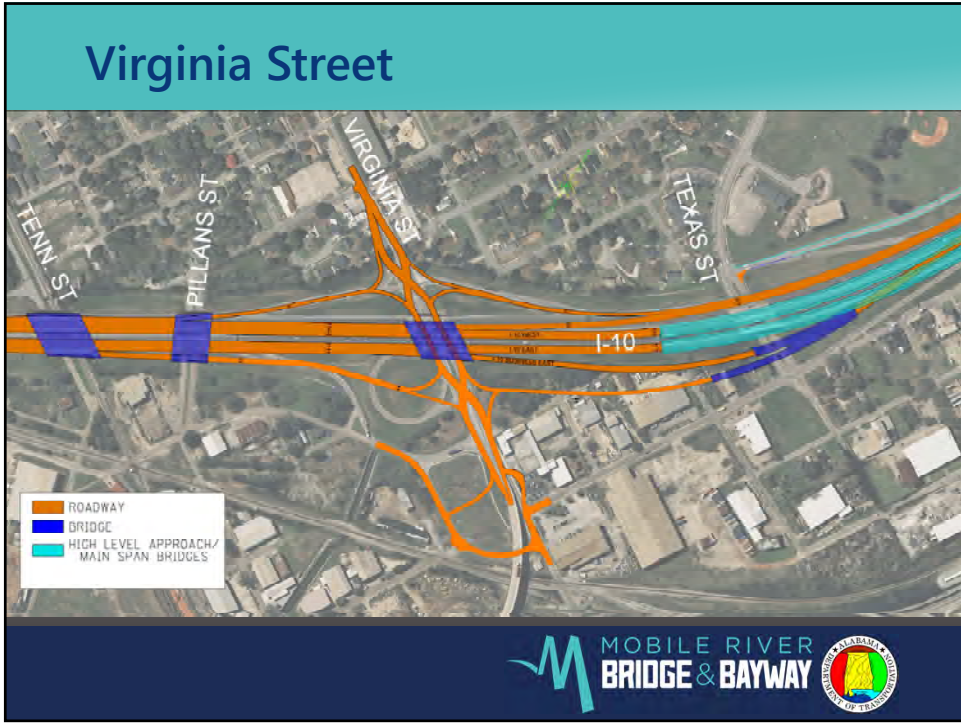
## Project Scope and Limits



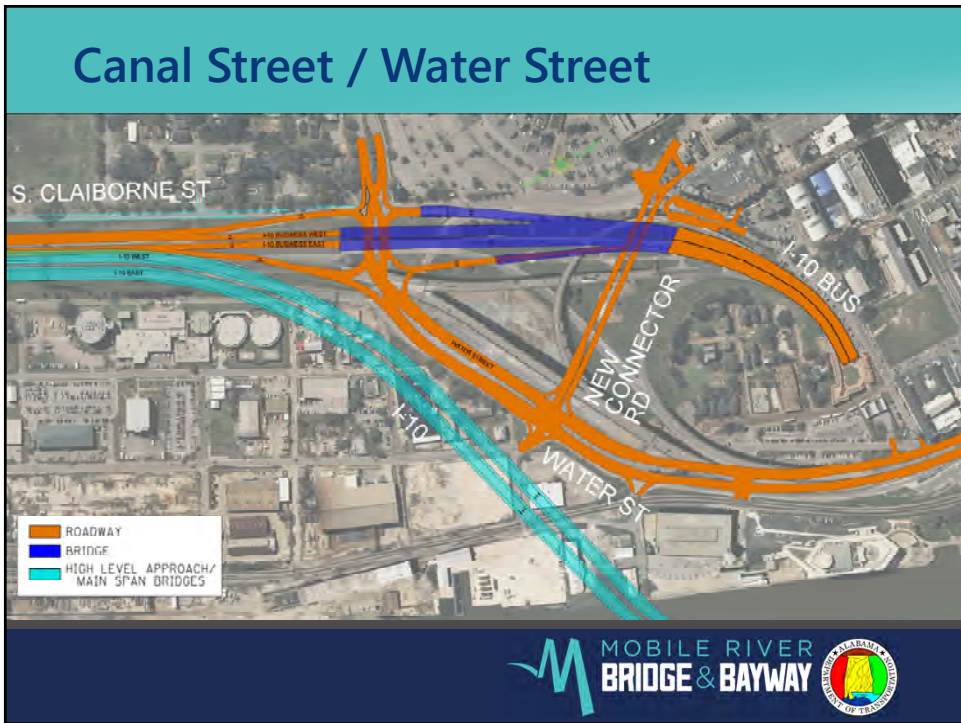
The map illustrates the project route from Africatown in Mobile to Daphne. Key features include the Wallace Tunnel, Canal St./Water St. interchange, East tunnel interchange, Midbay interchange, Eastern Shore interchange, Mobile River Bridge, Texas St. interchange, Virginia St. interchange, Broad St. interchange, Bayway bridges, and the Mobile Bay area. Major roads shown include US-90, US-98, and I-10. The map also indicates the boundaries of Mobile County and Baldwin County.



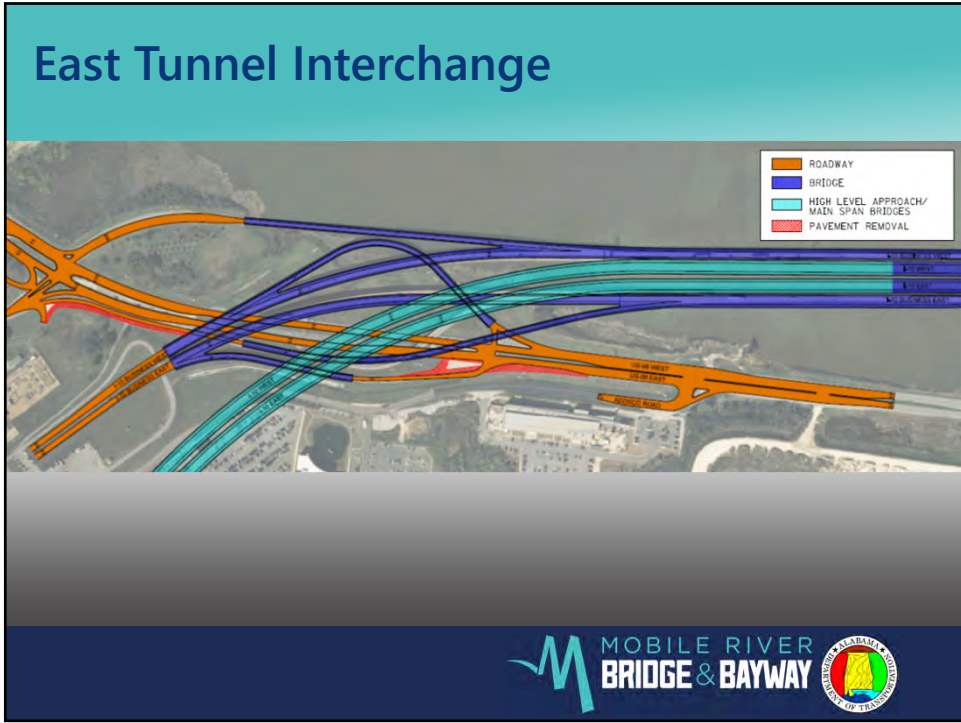
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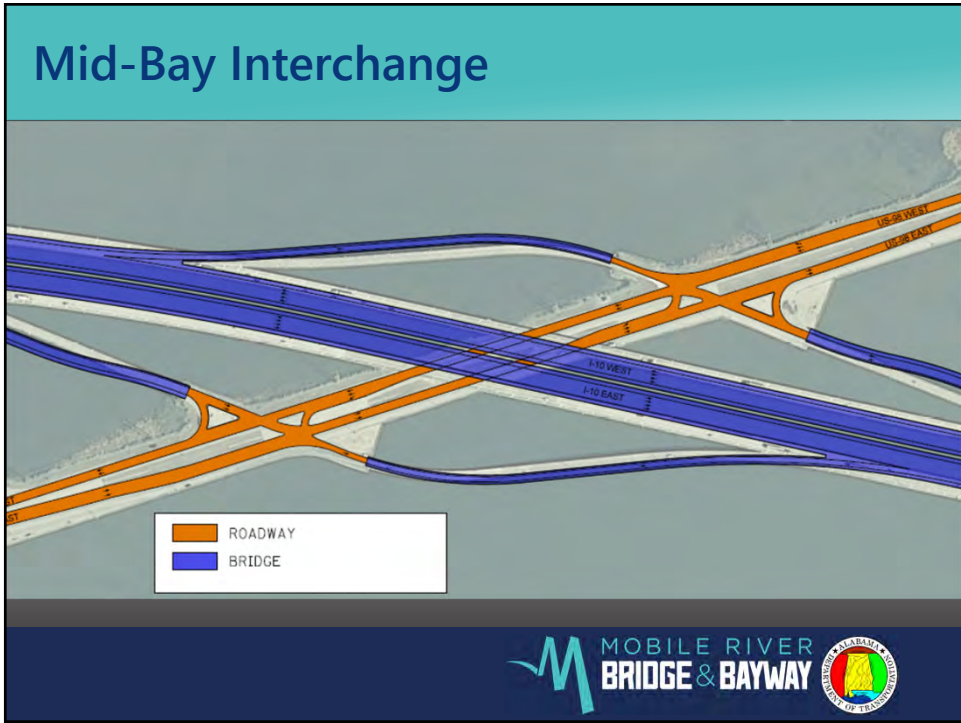
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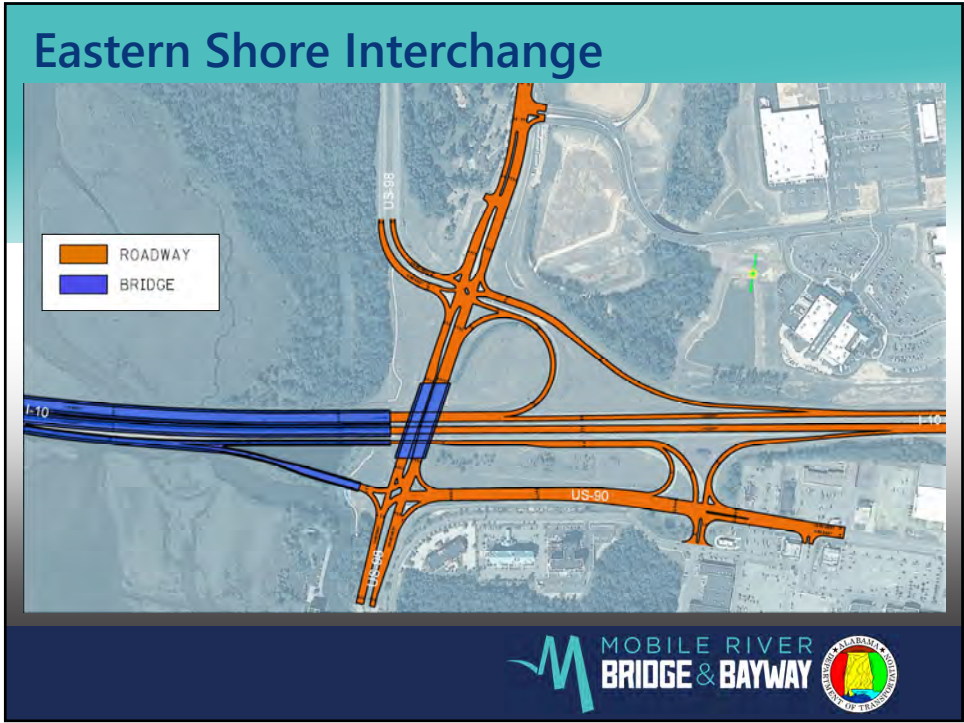


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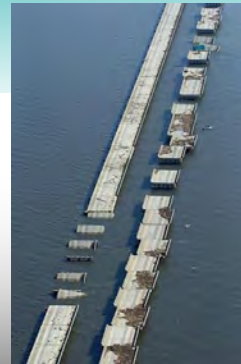
## Storm Surge Bridge Impacts



Hwy 90 Pass Christian, MS



Hwy 90 Biloxi, MS

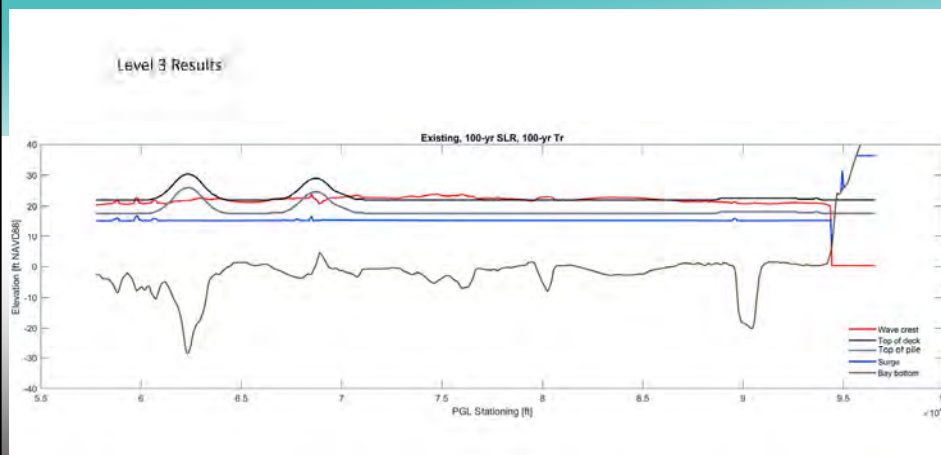


I-10 Twin Spans, LA



27

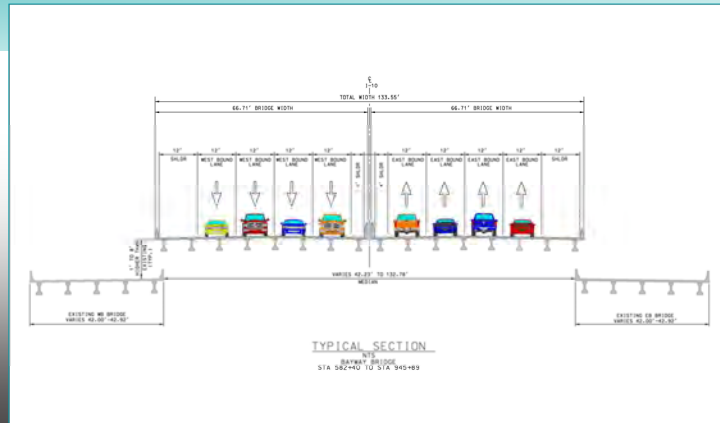
## Storm Surge Loads



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## Bayway

- Replace Bayway at higher elevation with 8 lanes



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## Bicycle and Pedestrian Facilities

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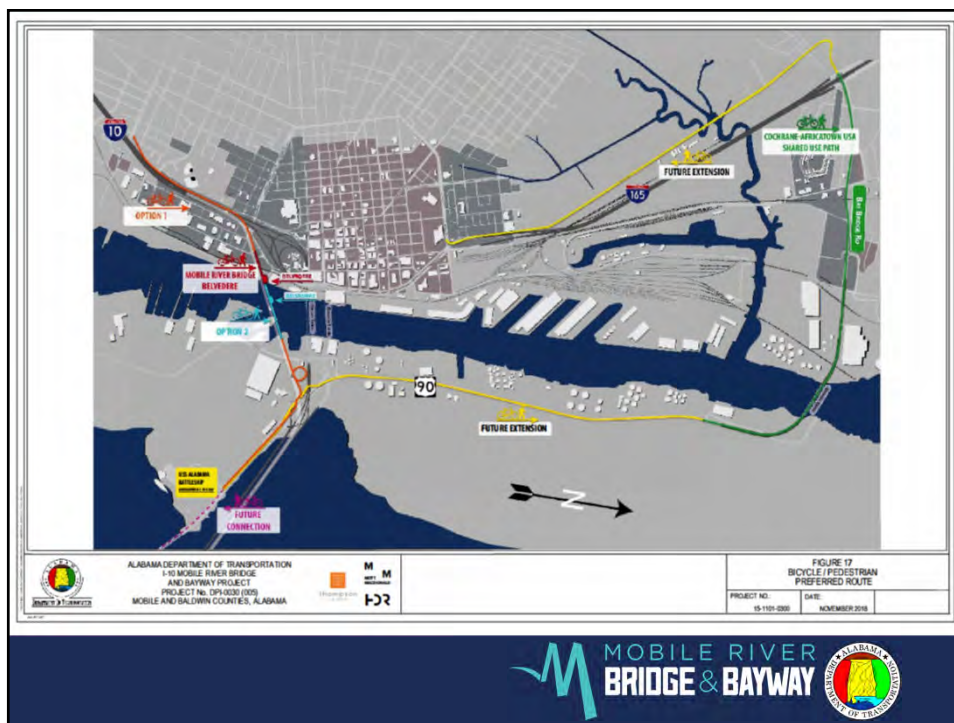
30

## Bicycle and Pedestrian Alternatives

- DEIS committed to bike/ped route across the Mobile River
- Bike/Ped Workshop: October 27, 2016
  - Bankhead Tunnel Alternative
  - Cochrane Bridge Alternative
  - New Mobile River Bridge Alternatives



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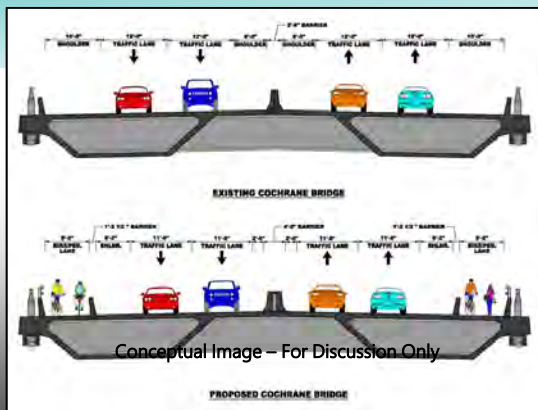


32



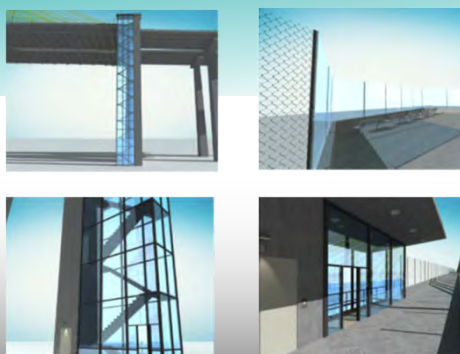
## ALDOT Commitment: Cochrane-Africatown USA Bridge Shared Used Path

- Path begins at I-165 ramp at Bay Bridge Road
- Path ends at east service road (US 98) at Cochrane Bridge approach
- Two protected bicycle and pedestrian lanes will be provided across Cochrane Bridge
- Future extensions: Downtown to Cochrane Path and Cochrane Path to Battleship



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## ALDOT Commitment: Belvedere



Conceptual - for discussion purposes only



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## Possible Added Options

**1**

**Full Bike and Ped Path on MRB**

Include a 12-foot bike and pedestrian path along the length of Mobile River Bridge and high level approaches.

**2**

**East Elevator and Stairs**

Provide elevator and stair access on the east side of the Mobile River with a path that connecting to the Belvedere and west elevator/stairs



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## Alternative Delivery Method



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## Alternative Delivery Method

"A Public Private Partnership (P3) describes a **contractual arrangement** between a Department (**public authority**) and a Developer (**private entity**) or connection with the design, build, financing, operations and maintenance (DBFOM) of an asset that will be used by or is otherwise a valuable project."

- Concession period: 55 years = 5 years construction + 50 years maintenance and operation



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## Why a P3 is being used

- Limited funding capacity on one of largest transportation projects in U.S.
- Leverages private sector expertise and range of financial resources
- Encouraged by USDOT to promote creativity, efficiency, and capital to address complex transportation problems facing state and local governments
- Risk Transfer – market (tolling) revenues, construction costs, schedule, operations and maintenance costs, liability




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## Technical Provisions


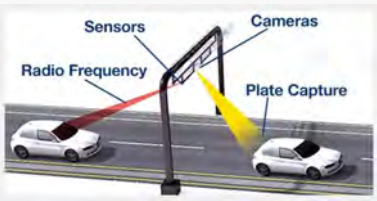
- Part of the contract
- Describe the scope of the work and related standards, performance requirements, conditions, procedures, specifications, and other provisions for the project
- Environmental commitments will be incorporated into technical provisions




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## Tolling Overview

- Max rate between \$3 and \$6 (2020 dollars)
- All-electronic tolling
- Payment options:
  - Transponders
  - Pay-by-plate
  - Walk-in centers
  - Call center located in Alabama
- Toll-free route: US 90/98 (Causeway), Bankhead, and Cochrane Bridge
- Frequent user discount



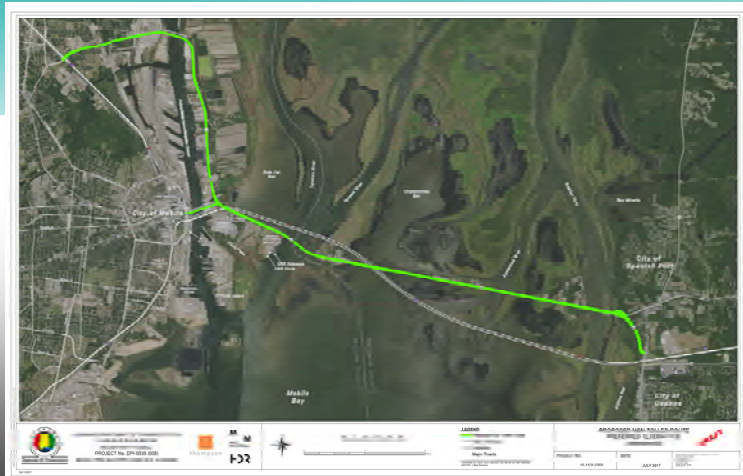
40

## Tolled Route




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## Toll-Free Route



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## NEPA Status and Section 106 Consultation





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### Status of NEPA Documentation

- Notice of Intent to prepare Supplemental Draft EIS: June 2017
- Supplemental Draft EIS in preparation
- Public Hearings after SDEIS approval
- Combined Final EIS/Record of Decision after Public Hearings



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## Previous Section 106 Meetings

- December 8, 2003: Scoping Meeting; Discussed purpose and need, alternatives, and the NEPA/Section 106 process
- July 14, 2006: Discussed and identified limits of area of potential effect
- August 31, 2010: Discussed project alternatives, potential effects, and results of Viewshed Impact Assessment



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## Previous Section 106 Meetings

- September 1, 2010: Conducted field review with Consulting Parties to look at alternatives and discuss potential effects
- July 26, 2012: Discussed potential effects, shared results of updated cultural resources survey reports
- September 23, 2014: Provided a project update, discussed potential effects
- May 8, 2018: Provided a project update, discussed draft MOA stipulations



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## Recent Written Consultation

- May 28, 2014: Comments requested on determination of effects
- February 24, 2016: Draft MOA for review and comment
- July 27, 2016: Comments on MOA circulated to Consulting Parties
- April 6, 2018: Disposition of comments on MOA and revised Draft MOA transmitted
- February 12, 2019: Disposition of comments and revised Draft MOA transmitted



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## Adverse Effect Determination

- May 18, 2015: FHWA issued letter to Alabama Historical Commission stating project may have adverse visual effect on Church Street East Historic District and Lower Dauphin Historic District
- June 11, 2015: Alabama Historical Commission concurred with adverse visual effect determination
- Determination included in Draft MOA



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# Expanded Area of Potential Effect (APE) and New Consulting Parties

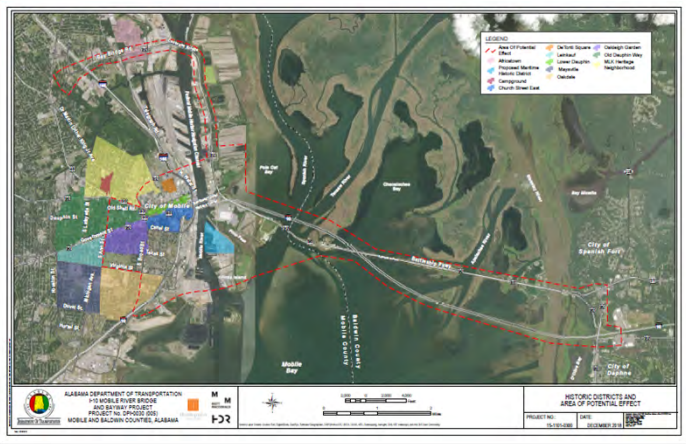



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## Expanded Area of Potential Effect

- Africatown Historic District and US-90/US-98 Causeway:





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## Expanded Area of Potential Effect

- Africatown Historic District and US-90/US-98 Causeway:
  - No adverse effect, SHPO concurrence dated February 8, 2019
  - ALDOT committed to installing interpretive/historical signage along Bay Bridge Road/Cochrane-Africatown USA Shared Use Path



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## New Section 106 Consulting Parties

- National Park Service
- Mobile County Training School
- Robert L. Hope Community Center
- Africatown C.H.E.S.S.
- Ossia Edwards



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## Draft MOA Topics



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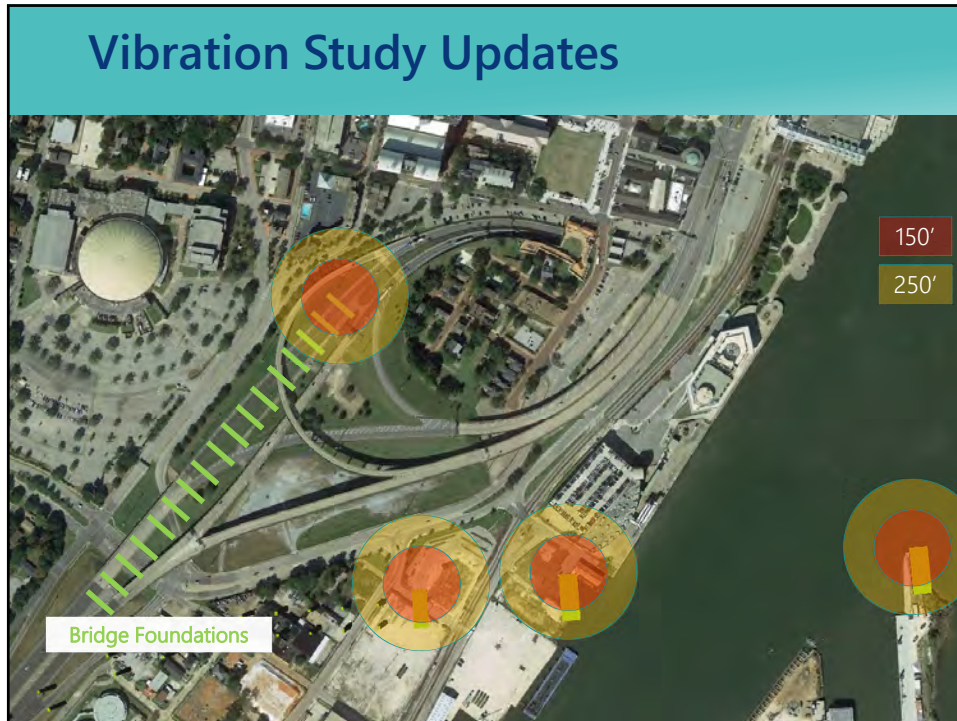
53

## Vibration Study: Findings and Recommendations

- For the largest displacement pile, vibrations were 0.15 PPV (in/sec) 150 ft. from the pile - little to no risk to adjacent structures
- Survey / monitoring for potential damage
  - 150 ft. for modern structures
  - 250 ft. for sensitive structures
  - Christ Church Cathedral and Old City Hall (History Museum of Mobile)
  - Wallace and Bankhead Tunnels
  - Conde-Charlotte Museum House
  - Phoenix Fire Museum

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## Vibration Study Updates

- Contract provisions address how vibration monitoring and mitigation will be handled during construction, including:
  - Vibration Monitoring Plan
  - Condition Assessment (Pre and Post Construction)
  - Stop work and mitigate if thresholds exceeded

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## Aesthetics

- Aesthetic Steering Committee
  - Framework for committee is included in draft MOA
  - Five meetings have been held to date
- Aesthetic Guidelines:
  - Architectural Themes
  - Land Use
  - Streetscape
  - Landscape
  - Materials and Finishes
  - Structures
  - Bicycle/Pedestrian Amenities
  - Lighting



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## Lighting

- Minimum roadway and bridge lighting criteria for safety
- Defer selection of light fixtures until late in design to use latest technology available
- Measures to minimize light spill
- Aesthetic lighting will be included in aesthetic packages from teams and reviewed by Aesthetic Steering Committee



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# Conceptual Rendering



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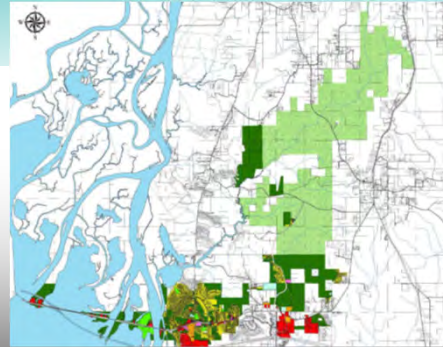
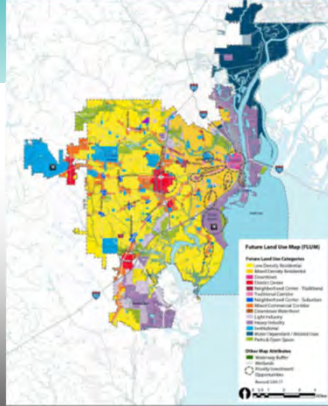
# Conceptual Rendering



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# Landscaping

- Emphasis on compatibility with land use plans



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# Landscaping

- Landscape and Management Plan for areas within ALDOT's right-of-way to be developed by Concessionaire
  - Landscaping requirements defined and reviewed by Registered Landscape Architect
  - Aesthetic Steering Committee input
  - Technical Provisions



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## Tree Canopy

- Maintain and improve tree canopy in and around downtown Mobile
- ALDOT partnership with City of Mobile in *Right Tree, Right Place* program for areas outside of ALDOT's right-of-way



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## USS ALABAMA Memorial Battleship Park

- Meetings on:
  - March 11, 2016
  - April 21, 2017
- Preliminary Signage Plan
- Access Considerations
- Continued commitment to consult with Battleship through design, construction, and post-construction



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## Supplemental Signage

USS ALABAMA Memorial Park  
2 1/2 MILES

USS ALABAMA Memorial Park  
←  
Vietnam & Korean  
Veterans Memorial

USS ALABAMA Memorial Park  
EXIT 30

USS ALABAMA Memorial Park  
2 MILES

MOBILE RIVER  
BRIDGE & BAYWAY

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## Historic Structures

- Union Hall
- Government Street/US 90 Historic Structures Survey

MOBILE RIVER  
BRIDGE & BAYWAY

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## Archaeology

- Surveys conducted to date have been sent to consulting parties and tribes
- Currently performing surveys on sites that were previously inaccessible
  - Consulted with SHPO to identify appropriate levels of archaeological surveys to be performed




67

{REDACTED}



68


{REDACTED}



The logo for Mobile River Bridge & Bayway features a stylized blue 'M' with a white wave-like base. To its right, the text 'MOBILE RIVER BRIDGE & BAYWAY' is displayed in a blue, sans-serif font. Further right is the official seal of the State of Alabama, which is circular and contains a yellow field with a red and blue border, and the text 'THE GREAT SEAL OF THE STATE OF ALABAMA' around the perimeter.

69


{REDACTED}



The logo for Mobile River Bridge & Bayway features a stylized blue 'M' with a white wave-like base. To its right, the text 'MOBILE RIVER BRIDGE & BAYWAY' is displayed in a blue, sans-serif font. Further right is the official seal of the State of Alabama, which is circular and contains a yellow field with a red and blue border, and the text 'THE GREAT SEAL OF THE STATE OF ALABAMA' around the perimeter.

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{REDACTED}



MOBILE RIVER  
BRIDGE & BAYWAY

ALABAMA  
DEPARTMENT OF TRANSPORTATION

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MOBILE RIVER  
BRIDGE & BAYWAY

# Next Steps




ALABAMA  
DEPARTMENT OF TRANSPORTATION

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## Future Section 106 Activities

- Provide Comments on Draft MOA
- ALDOT/FHWA Update Draft MOA to Address Comments
- Continue Phase 1 and Phase 2 Archaeological Surveys
- Finalize MOA – Signed by ACHP, AHC, ALDOT, and FHWA
- Continue 106 Consultation through Design and Construction



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## Anticipated Project Milestones

- Industry Review Period (through mid-2019)
- SDEIS Signed – March 2019
- Public Hearings – May 2019
- FEIS/ROD Approval – August 2019
- Proposer Selection – March 2020
- Construction Start – 2020



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Follow our progress  
MobileRiverBridge.com  
clayn@dot.state.al.us

MOBILE RIVER BRIDGE  
AND BAYWAY  
BUILDING OUR CONNECTION

Get updates

Disclaimer: This presentation is intended to provide an overview of the project status and studies performed by the project team as of the presentation date. All data and schedules shown in the presentation are preliminary and subject to change. The National Environmental Policy Act (NEPA) review of the preferred project and project alternatives is ongoing and subject to final review and approval by the Federal Highway Administration.

MOBILE RIVER  
BRIDGE & BAYWAY



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**APPENDIX L-9:**  
**Viewshed Renderings from DEIS**

### **Viewshed Renderings from DEIS**

A Viewshed Impact Assessment was completed for the project, and the full report is included in Appendix J of the DEIS. In a letter dated June 30, 2014, the Alabama Historical Commission voiced concerns about “potential visual impacts of the proposed bridge to certain historic areas in downtown Mobile, specifically at locations 26, 27, 32, 44, 45, 46, 48, 49, and 51.” Viewshed renderings at these locations are contained in this appendix for reference.



**Resource 26: Intersection of St. Emanuel and Conti Streets**



View to the northwest from the intersection of St. Emanuel and Conti Streets to the southeast toward the proposed I-10 Mobile River Bridge showing Alternative A.



View to the northwest from the intersection of St. Emanuel and Conti Streets to the southeast toward the proposed I-10 Mobile River Bridge showing Alternative B. There would be no discernable difference in the view of Alternative B'.



View to the northwest from the intersection of St. Emanuel and Conti Streets to the southeast toward the proposed I-10 Mobile River Bridge showing Alternative C.

**Resource 27: Intersection of Government and Conception Streets**



View from the intersection of Government and Conception Streets to the east-southeast viewshed showing Alternative A, Dr. LeVert's Office (small brick building), and Mobile County Probate Court.



View from the intersection of Government and Conception Streets to the southeast viewshed showing Alternative B' and a mixture of historic and non-historic structures. Alternative B would be similarly visible. Alternative C would not be visible.



**Resource #32: Admiral Semmes Hotel**



View from the rear of Admiral Semmes Hotel on Church Street looking toward Alternative A.



View from the rear of Admiral Semmes Hotel on Church Street looking toward Alternative B. Alternative B' would be similarly visible. Alternative C would not be visible looking in this direction.



View from the rear of Admiral Semmes Hotel on Church Street looking toward Alternative A.



View from the rear of Admiral Semmes Hotel on Church Street looking toward Alternative B. Alternative B' would be similarly visible.





View from the rear of Admiral Semmes Hotel on Church Street looking toward Alternative C.



View to the south viewshed from a 12th-floor room in Admiral Semmes Hotel looking toward Alternative A.



View to the south viewed from a 12th-floor room in Admiral Semmes Hotel looking toward Alternative B. Alternative B' would be similarly visible.



View to the south viewed from a 12th-floor room in Admiral Semmes Hotel looking toward Alternative C.



**Resource #44: Intersection of Lawrence Street and Eslava Street**



View to the southeast looking toward Alternative A from the intersection of S. Lawrence and Eslava Streets.



View to the southeast looking toward Alternative B from the intersection of S. Lawrence and Eslava Streets. Alternative B' would be similarly visible.



View to the southeast looking toward Alternative C from the intersection of S. Lawrence and Eslava Streets.

**Resource #45: Phoenix Fire Museum**



View to the south from the front of Phoenix Fire Museum showing Alternative A from the intersection of S. Claiborne Street, S. Jackson Street, and Civic Center Drive.



View to the south from the front of Phoenix Fire Museum showing Alternative B from the intersection of S. Claiborne Street, S. Jackson Street, and Civic Center Drive. Alternative B' would be similarly visible.





View to the south from the front of Phoenix Fire Museum showing Alternative C from the intersection of S. Claiborne Street, S. Jackson Street, and Civic Center Drive.

**Resource #46: Christ Episcopal Church**



View to the east from the front door of Christ Episcopal Church showing Alternative A. Old Southern Market and City Hall (now Museum of Mobile) is also shown.



View to the east from the front door of Christ Episcopal Church showing Alternative B. Old Southern Market and City Hall (now Museum of Mobile) is also shown. Alternate B' would be similarly visible. Alternative C would not be visible in this direction.





View to the southeast from the front door of Christ Episcopal Church showing Alternative A.



View to the southeast from the front door of Christ Episcopal Church showing Alternative B. Alternative B' would be similarly visible.



View to the southeast from the front door of Christ Episcopal Church showing Alternative C.



**Resource #48: Fort Condé Village**



View to the east from the four historic Victorian cottages on S. Royal Street on the southeast edge of Fort Condé Village looking toward Alternative A. The existing I-10 ramps shown would be removed with the proposed project.



View to the east from the four historic Victorian cottages on S. Royal Street on the southeast edge of Fort Condé Village looking toward Alternative B. Alternative B' would be similarly visible. The existing I-10 ramps shown would be removed with the proposed project.

**Resource #49: Condé-Charlotte Museum House**



View to the south from the second-story front balcony of Condé-Charlotte Museum House showing Alternative A.



View to the south from the second-story front balcony of Condé-Charlotte Museum House showing Alternative B. Alternative B' would be similarly visible.



View to the south from the second-story front balcony of Condé-Charlotte Museum House showing Alternative C.



**Resource #51: Old Southern Market and City Hall**



View to southeast from Government Street showing the Old Southern Market and City Hall (now Museum of Mobile) showing Alternative A.



View to southeast from Government Street showing the Old Southern Market and City Hall (now Museum of Mobile) showing Alternative B. Alternate B' would be similarly visible from this location.



View to south from S. Royal Street showing Old Southern Market and City Hall and Alternative A.



View to south from S. Royal Street showing Alternative B. Alternative B' would be similarly visible.



View to south from S. Royal Street showing Old Southern Market and City Hall and Alternative C.

# **APPENDIX L-10:**

**Draft Section 106 MOA**



**Draft Section 106 Memorandum of Agreement (MOA)**

MEMORANDUM OF AGREEMENT  
AMONG THE FEDERAL HIGHWAY ADMINISTRATION  
THE ALABAMA STATE HISTORIC PRESERVATION OFFICE  
THE ALABAMA DEPARTMENT OF TRANSPORTATION  
AND THE ADVISORY COUNCIL ON HISTORIC PRESERVATION  
REGARDING THE  
I-10 MOBILE RIVER BRIDGE AND BAYWAY  
MOBILE AND BALDWIN COUNTIES, ALABAMA  
FEDERAL-AID PROJECT DPI-0030 (005)

WHEREAS the Alabama Department of Transportation (ALDOT) has requested funding for Project DPI-0030(005) (undertaking) in Mobile and Baldwin Counties with Federal-Aid funds from the Federal Highway Administration (FHWA) pursuant to 23 USC 101 et seq. Federal-Aid Highways; and

WHEREAS, the undertaking, consists of constructing a new 6-lane bridge on I-10 across the Mobile River and replacing the existing I-10 bridges across Mobile Bay with 8 lanes of new bridges in Mobile and Baldwin Counties; and

WHEREAS, FHWA and ALDOT have defined the undertaking's Area of Potential Effect (APE) as an area range starting at the I-10 and Broad Street Interchange, moving northward to Virginia Street then west to Ann Street, north to Springhill Avenue, east to Beauregard Street and then crossing the Federal Mobile Harbor Navigation Channel approximately 500' north of US-90 and approximately 500' south of I-10 to the Eastern Shore. An addition to the APE includes a 1,000' corridor to the north centered on US-90 then Bay Bridge Road past I-165 to Velma St. in Prichard. (*See Attachment #1*); and

WHEREAS, FHWA and ALDOT have determined that the undertaking may have an adverse visual effect on the Church Street East Historic District and the Lower Dauphin Street Historic District (*See Attachment #1*), which are listed in the *National Register of Historic Places*; FHWA and ALDOT have also determined that the undertaking may have an adverse effect on archaeological sites (*See Attachment #2*); FHWA and ALDOT have consulted with the Alabama State Historic Preservation Officer (SHPO) pursuant to 36 CFR 800, the regulations implementing Section 106 of the *National Historic Preservation Act* (54 USC 306108) and

WHEREAS, FHWA and ALDOT have documented that consultation in the *Draft Environmental Impact Statement (DEIS) signed July 22, 2014* and the *Supplemental DEIS signed on March 26, 2019*; and

WHEREAS, FHWA and ALDOT have consulted with the Section 106 Consulting Parties and Federally-recognized tribes with historical ties to Alabama listed in *Attachment #3* regarding the effects of the undertaking on historic properties; and

WHEREAS, in accordance with 36 CFR 800.6(a)(1), FHWA has notified the Advisory Council on Historic Preservation (ACHP) of its adverse effect determination with specified documentation, and the ACHP has chosen to participate in the consultation pursuant to 36CFR 800.6(a)(1)(iii);

NOW, THEREFORE, FHWA, the SHPO, the ALDOT and the ACHP agree that the undertaking shall be implemented in accordance with the following stipulations in order to take into account the effect of the undertaking on historic properties.

**I. STIPULATIONS**

FHWA and ALDOT shall ensure that the following measures are carried out:

- A. Lighting:** Lighting associated with the bridge approaches, bridges, ramps, roadway widening, and other components of the project shall be designed to meet current design criteria, while

minimizing light pollution. In order to incorporate the newest technology available at the time of construction, lighting fixtures will not be specified until later in the design process. Measures to minimize light pollution on residential areas along I-10 shall be incorporated into the project through the use of light shielding technology, fixtures, and other means as appropriate.

Measures to minimize light pollution on historic resources will be developed with input from the SHPO and Section 106 Consulting Parties through the Aesthetic Steering Committee. ALDOT will consult with FHWA, the Aesthetic Steering Committee, and the selected design team during the design phase to ensure compliance with the Section 106 Memorandum of Agreement. *Attachment #7* describes the Aesthetic Steering Committee in more detail.

To prevent or minimize collision and nesting by migratory fowl, the maximum allowable duration for strobe (beacon) lighting on the bridge tower(s) will be requested in Federal Aviation Administration (FAA) permit application(s) for the project. These lighting requirements will be coordinated with the U.S. Coast Guard (USCG) for compliance with navigational lighting requirements and the FAA for air traffic requirements as part of the permitting process.

- B. Bridge Aesthetics:** Opportunities to incorporate bridge aesthetics and contextual design of the proposed project will be developed as the design progresses with input from the SHPO and Section 106 Consulting Parties through the Aesthetic Steering Committee. ALDOT will consult with FHWA, the Aesthetic Steering Committee, and the selected design team during the design phase to ensure compliance with the Section 106 Memorandum of Agreement.
- C. Visual:** Aesthetic and landscape plans for areas within ALDOT's right-of-way, including areas beneath the bridge, will be developed and implemented. ALDOT will consult with FHWA, the Aesthetic Steering Committee, and the selected design team during the design phase to ensure compliance with the Section 106 Memorandum of Agreement.

ALDOT understands the importance of maintaining and improving the tree canopy within downtown Mobile in areas that are outside of ALDOT's right-of-way. To achieve this, ALDOT has partnered with the City of Mobile in the *Right Tree, Right Place* program. This program places appropriate trees and landscaping throughout the City of Mobile. ALDOT has committed to contribute \$50,000 to the *Right Tree, Right Place* program to help maintain and improve the tree canopy in downtown Mobile. The City of Mobile will be responsible for administering this money. The *Right Tree, Right Place* Committee will make sure that trees and landscaping are implemented within the City's right-of-way that are compatible with the setting and comply with municipal regulations.

- D. Archaeology:** Phase I archaeological surveys and limited Phase II testing have been conducted and are currently underway for the proposed project. Due to widespread disturbed historic overburden present in many areas, a program of integrated Phase I and Phase II (Phase I/II) evaluation has been employed. This approach utilizes specialized heavy machinery to remove disturbed overburden to expose, record, and sample undisturbed cultural features and zones in areas where standard Phase I techniques are inadequate. The SHPO and the tribes have been consulted on this approach. The SHPO gave their approval, and the tribes expressed no concerns.

The project's APE has been divided into survey blocks to organize and record fieldwork results. There are 17 Survey Blocks plus 5 other named areas being investigated for this project. Each Survey Block contains smaller parcels delineated by ownership tracts, ranging from 1 to 9 tracts per Survey Block. There are 61 tracts associated with the project. Some of the tracts are not yet accessible for archaeological investigation, but fieldwork will proceed when the properties become available. Survey Blocks are shown in *Attachment #2*.

No ground-disturbing activities will be allowed on any parcels containing identified or potential archaeological sites until Phase I, Phase II, and/or Phase III investigations are complete and the results have been coordinated with the SHPO and tribes.

Impacts from the undertaking will be documented as the design progresses and as additional access to the potentially affected parcels is obtained.

Efforts will be made to avoid and/or minimize impacts on archaeological sites listed on, eligible for, or potentially eligible for listing on the NRHP. For sites where impacts cannot be avoided, mitigation will be performed in the form of Phase III Data Recovery or other approved alternative mitigation plans, as coordinated with the SHPO and tribes. Where required, Phase III Data Recovery investigations will be performed at affected parcels once specific impact locations are known and prior to commencement of ground-disturbing activities.

*Attachment #4 Post-Review Discovery Plan* outlines procedures that shall be followed in the event intact archaeological deposits are uncovered during the course of the undertaking.

*Attachment #5* and *Attachment #6* contain *The Alabama Burial Act* and *The Advisory Council on Historic Preservation's Policy Statement Regarding Treatment of Burial Sites, Human Remains, and Funerary Objects*, respectively.

- E. Historic Battleship Park:** ALDOT and FHWA met with the Battleship USS ALABAMA Memorial Park Commission on April 21, 2017 to discuss the Commission's concerns about access to the Park and potential impacts that could occur as result of this project. ALDOT evaluated several options to provide more direct access to the Park. Concepts providing direct access to the Park via a new ramp or relocating the Park's entrance could not meet design criteria for safe roadway conditions; therefore, they were not advanced for further construction. Existing access to the Battleship USS ALABAMA Memorial Park would not be altered in the final condition of this project.

In order to improve signage directing travelers to the Park, ALDOT has developed a preliminary signage plan for the USS ALABAMA Battleship Memorial Park including proposed locations and types of signs. The plan was developed with input from the SHPO and the USS ALABAMA Battleship Memorial Park Commission. New signs are proposed to supplement the existing signs along the I-10 corridor. The signs will direct travelers from I-10 to the Park. ALDOT met with the USS ALABAMA Battleship Memorial Park Commission on August 10, 2018, to give them an update on the project and the latest signage plan. ALDOT will meet with the USS ALABAMA Battleship Memorial Park Commission to finalize the signage plan prior to approving the final signage plan before construction begins.

Access to the USS ALABAMA Battleship Memorial Park will be maintained before, during, and after construction.

- F. Vibrations:** ALDOT conducted a study to evaluate potential vibration impacts for pile driving and to help identify construction methodologies that would avoid vibration impacts to properties in proximity of the project (*Attachment #8*). Based on the study, ALDOT has committed to:
1. Limit vibration to a maximum level of 0.5 inch per second for modern structures and 0.1 inch per second for historic structures at the location of the structure.
  2. Survey and monitor for potential vibration damage for all modern structures within 150 feet of vibration-causing construction operations and all historic structures within 250 feet of vibration-causing construction operations. In addition, due to concerns raised by the Section 106 Consulting Parties, vibrations will also be monitored at Christ Church

Cathedral, Old City Hall (History Museum of Mobile), Condé-Charlotte Museum House, Phoenix Fire Museum, Austal, the Wallace Tunnel, and the Bankhead Tunnel. Vibrations will also be limited to a maximum level of 0.5 inch per second for modern structures and 0.1 inch per second for historic structures. These structures are well beyond the distance where vibration levels of 0.5 and 0.1 inch per second were projected to occur based on the vibration study and, therefore, represent conservative survey distances to ensure adjacent structures are not damaged.

3. Require the Concessionaire to obtain the services of a competent vibration or seismologist consultant to conduct vibration surveys and monitor and record ground vibrations during the entire demolition and construction phase operations. If at any time the maximum vibration level is exceeded, the Concessionaire will be required to make appropriate changes to reduce vibration to acceptable levels prior to continuing operations.
4. Prior to acceptance of the project, the Concessionaire will be required to submit a vibration report covering the life of the project. Photographic, video and other surveys of surrounding structures and utilities (pre-construction and post-construction) will be made as part of the documentation record.
5. Any damage to historic structures due to vibration levels above the maximum will be repaired/restored in accordance with ALDOT Specification 107.12, 107.14 and 107.15 *Protection and Restoration of Property, Landscape and Utility Facilities*, 36CFR 800.12 *Emergency Situations* and 36 CFR 68 *The Secretary of Interior's Standards for the Treatment of Historic Properties*.

**G. Public Involvement:** Public Hearings will be held prior to finalizing the Environmental Impact Statement and issuing a Record of Decision. The public, local agencies, and Section 106 Consulting Parties will be given the opportunity to provide input regarding available design information as part of the public involvement process. The Section 106 Consulting Parties will be notified in writing (via letter and/or e-mail) of all future public involvement activities.

## **II. DURATION**

This MOA will expire if its terms are not carried out within 10 years from the date of its execution. Prior to such time, FHWA and ALDOT may consult with the other signatories to reconsider the terms of the MOA and amend it in accordance with Stipulation VI below.

## **III. POST-REVIEW DISCOVERIES**

If properties are discovered that may be historically significant or unanticipated effects on historic properties found, the FHWA shall implement the discovery plan included as *Attachment #4*, Post Review Discoveries Plan of this MOA.

## **IV. MONITORING AND REPORTING**

Each year following the execution of this MOA until it expires or is terminated, ALDOT shall provide all parties to this MOA and the ACHP, a summary report detailing work undertaken pursuant to its terms. Such report shall include any scheduling changes proposed, any problems encountered, and any disputes and objections received in FHWA's efforts to carry out the terms of this MOA.

## **V. DISPUTE RESOLUTION**

Should any signatory to this MOA object at any time to any actions proposed or the manner in which the terms of this MOA are implemented, FHWA shall consult with such party to resolve the objection. If FHWA determines that such objection cannot be resolved, FHWA will:

- A. Forward all documentation relevant to the dispute, including the FHWA's proposed resolution, to the ACHP. The ACHP shall provide FHWA with its advice on the resolution of the objection within thirty (30) days of receiving adequate documentation. Prior to reaching a final decision on

the dispute, FHWA shall prepare a written response that takes into account any timely advice or comments, regarding the dispute from the ACHP, signatories and provide them with a copy of this written response. FHWA will then proceed according to its final decision.

- B.** If the ACHP does not provide its advice regarding the dispute within the thirty (30) day time period, FHWA may make a final decision on the dispute and proceed accordingly. Prior to reaching such a final decision, FHWA shall prepare a written response that takes into account any timely comments regarding the dispute from the signatories to the MOA, and provide them and the ACHP with a copy of such written response.
- C.** FHWA's responsibility to carry out all other actions subject to the terms of this MOA that are not the subject of the dispute remain unchanged.

#### **VI. AMENDMENTS**

This MOA may be amended when such an amendment is agreed to in writing by all signatories. The amendment will be effective on the date a copy signed by all of the signatories is filed with the ACHP.

#### **VII. TERMINATION**

If any signatory or concurring party to this MOA determines that its terms will not or cannot be carried out, that party shall immediately consult with the other signatories to attempt to develop an amendment per Stipulation VI, above. If within thirty (30) days an amendment cannot be reached, any signatory may terminate the MOA upon written notification to the other signatories.

Once the MOA is terminated, and prior to work continuing on the undertaking, FHWA must either (a) execute an MOA pursuant to 36CFR 800.6 or (b) request, take into account, and respond to the comments of the ACHP under 36CFR 800.7. FHWA shall notify the signatories as to the course of action it will pursue.

Execution of this MOA by the FHWA and SHPO and the ACHP, and implementation of its terms evidence that FHWA has taken into account the effects of this undertaking on historic properties.

**SIGNATORIES:**

FEDERAL HIGHWAY ADMINISTRATION

By: \_\_\_\_\_  
Mark Bartlett, Division Administrator Date

ADVISORY COUNCIL ON HISTORIC PRESERVATION

By: \_\_\_\_\_  
John M. Fowler, Executive Director Date

ALABAMA STATE HISTORIC PRESERVATION OFFICE

By: \_\_\_\_\_  
Lee Anne Wofford, Deputy SHPO Date

**CONCUR:**

ALABAMA DEPARTMENT OF TRANSPORTATION

By: \_\_\_\_\_  
John R. Cooper, Transportation Director Date

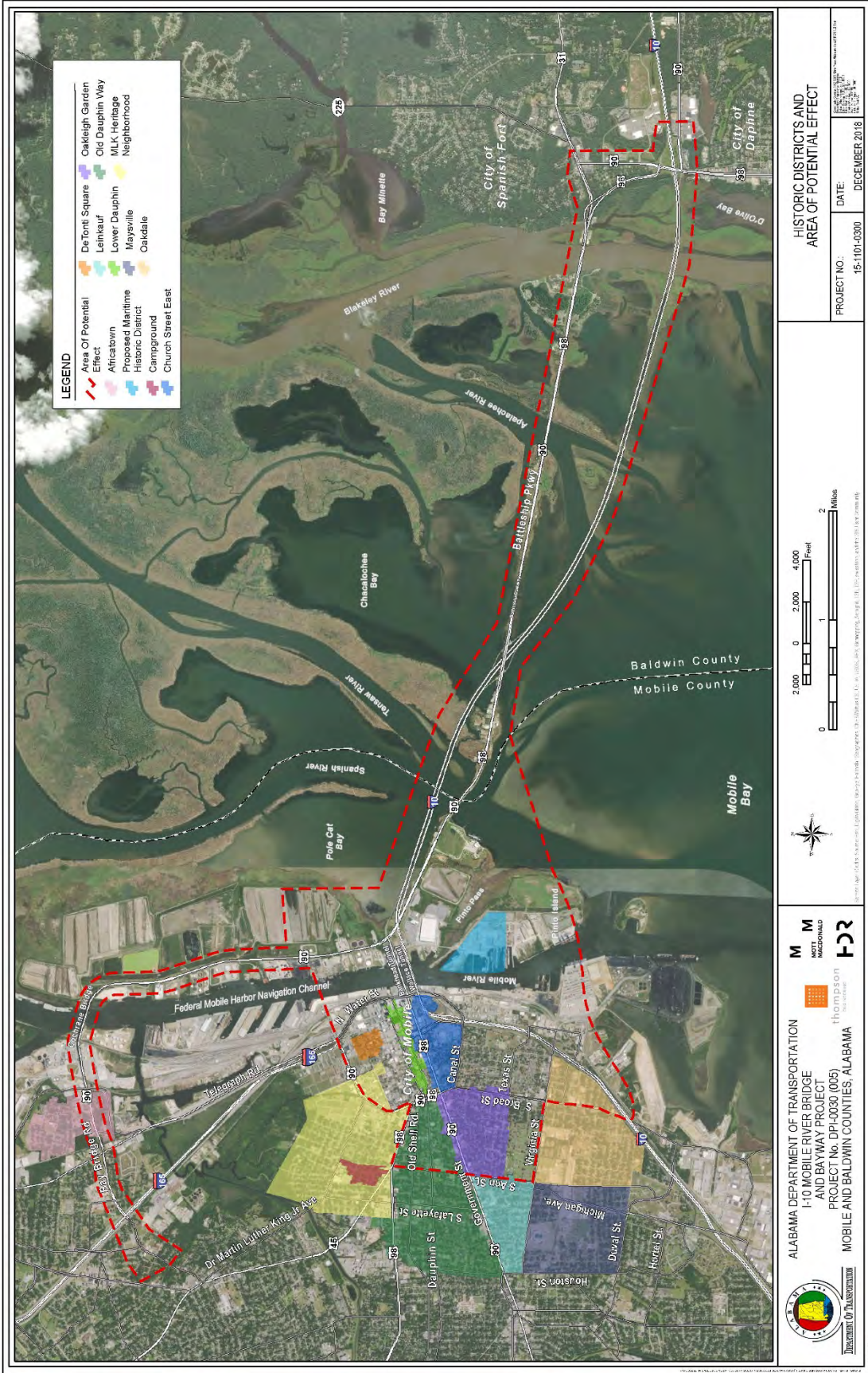


**Attachment #1. Area of Potential Effect (APE)**

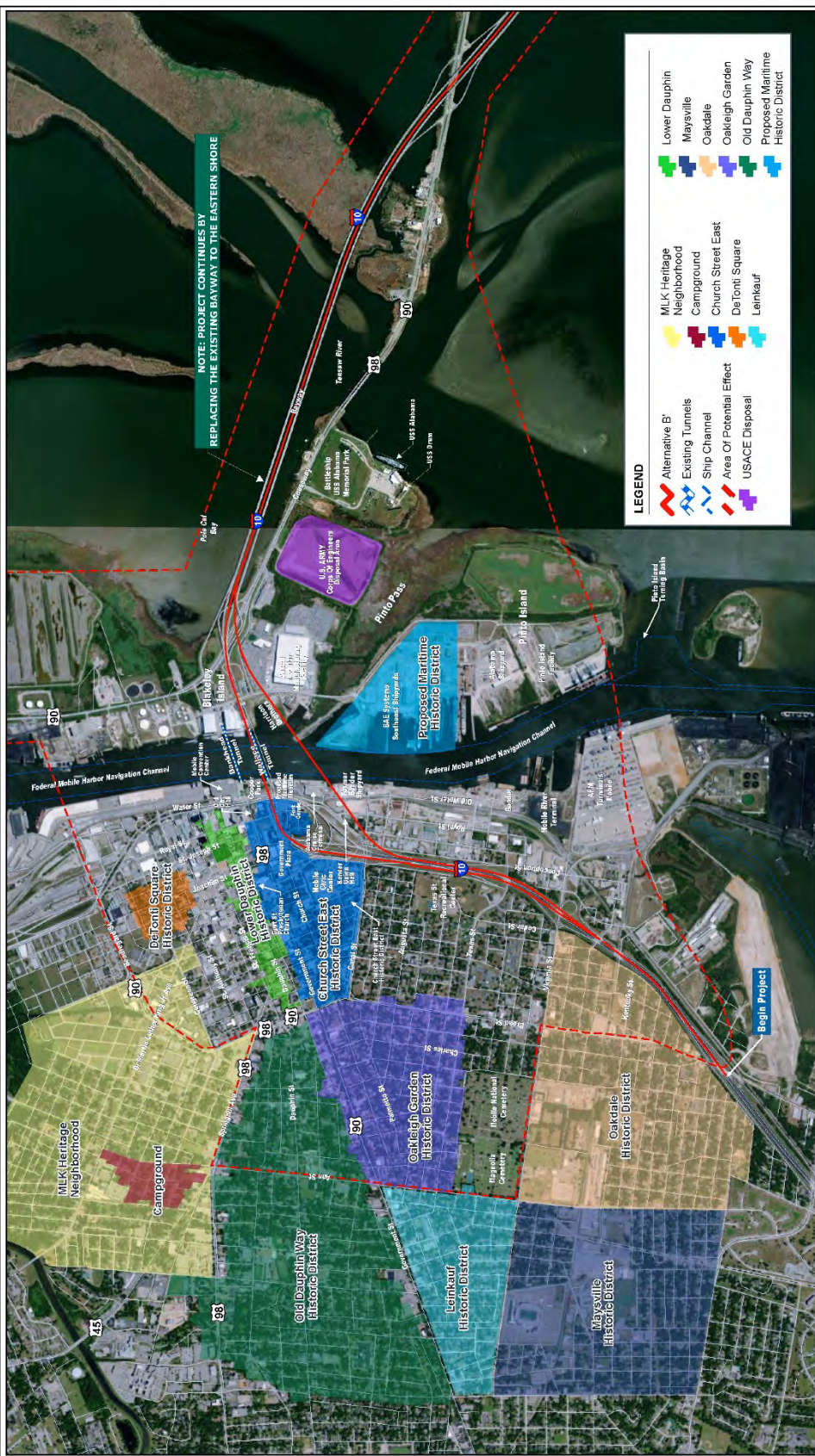
The Area of Potential Effect (APE), for the I-10 Mobile River Bridge project was established in consultation with the State Historic Preservation Officer (SHPO) and other Section 106 Consulting Parties (*see Attachment #3*).

The FHWA has defined the undertaking's APE as a range starting at I-10 and Broad Street in the Oakdale Historic District, moving northward to Virginia Street then west to Ann Street. The Ann Street border goes north to Springhill Avenue then east to Beauregard Street. The APE follows Beauregard Street and then crosses the Federal Mobile Harbor Navigation Channel approximately 500' north of US-90 and approximately 500' south of I-10 to the Eastern Shore. An addition to the APE includes a 1,000' corridor to the north, centered on US-90 then Bay Bridge Road past I-165 to Velma Street in Prichard.

*National Register* listed Historic Districts included in the APE are: The Church Street East Historic District, the Oakleigh Garden Historic District, Lower Dauphin Historic District, DeTonti Square Historic District, Oakdale Historic District, Maysville Historic District, and the Africatown Historic District.





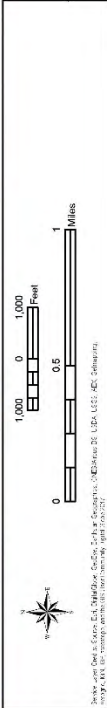


- LEGEND**
- Alternative B'
  - Existing Tunnels
  - Ship Channel
  - Area Of Potential Effect
  - USACE Disposal
  - MLK Heritage Neighborhood
  - Campground
  - Church Street East
  - DeToni Square
  - Leinkauf
  - Lower Dauphin
  - Maysville
  - Oakdale
  - Oakeigh Garden
  - Old Dauphin Way
  - Proposed Maritime Historic District
  - Proposed Maritime Historic District

**HISTORIC DISTRICTS AND AREA OF POTENTIAL EFFECT**

PROJECT NO.: 15-1101-0300

DATE: DECEMBER 2018



ALABAMA DEPARTMENT OF TRANSPORTATION  
 I-10 MOBILE RIVER BRIDGE AND BAYWAY PROJECT  
 PROJECT NO. DP-1030 (005)

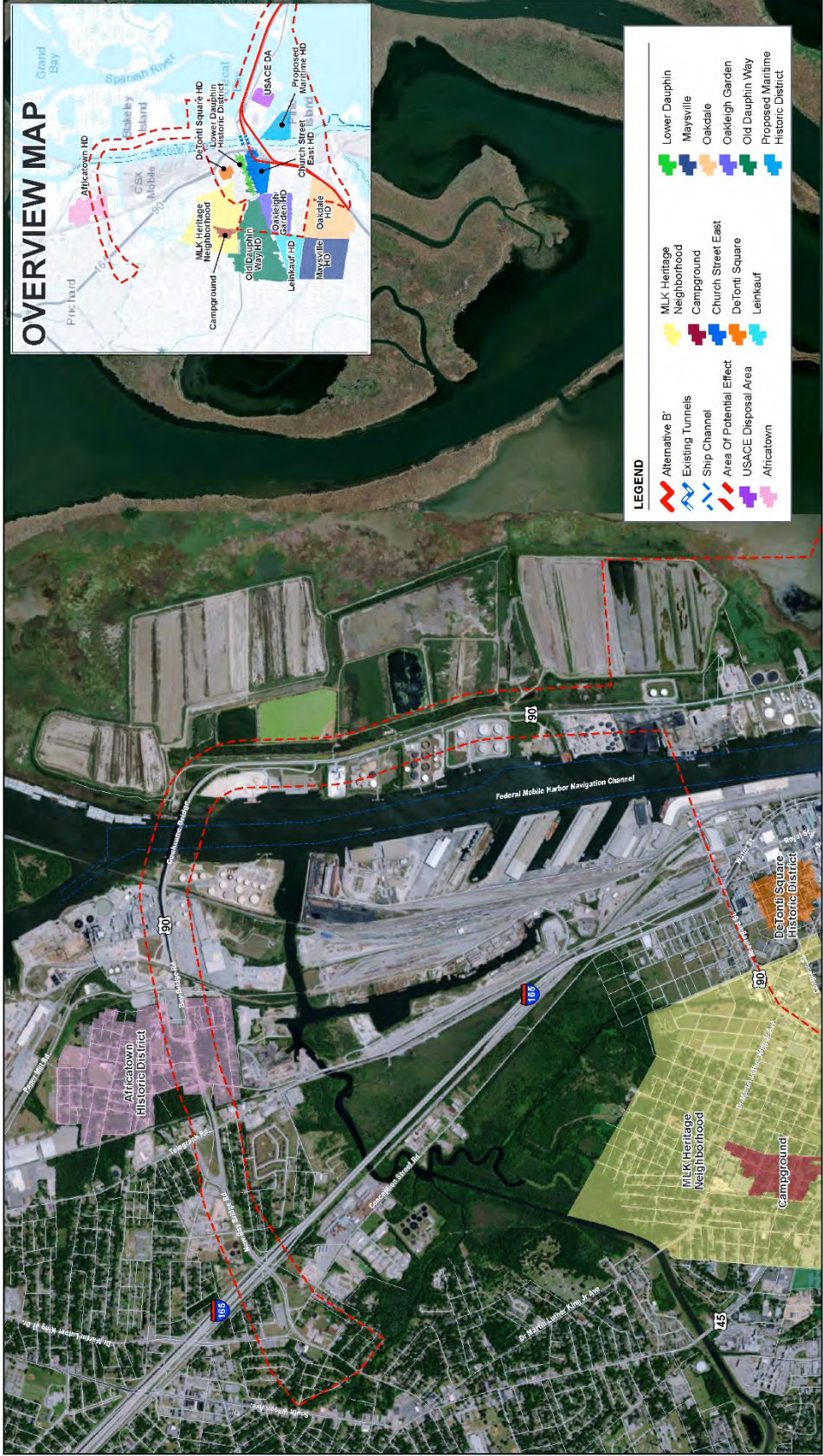
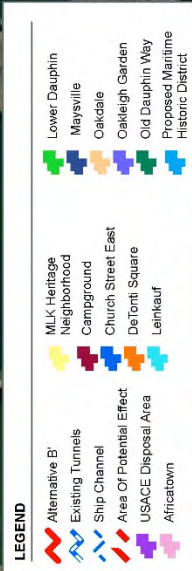
MOBILE AND BALDWIN COUNTIES, ALABAMA

**FTR**  
 the nation's transportation trust funds

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# OVERVIEW MAP



**ALABAMA DEPARTMENT OF TRANSPORTATION**  
**I-10 MOBILE RIVER BRIDGE AND BAYWAY PROJECT**  
**PROJECT No. DPI-0030 (005)**  
**MOBILE AND BALDWIN COUNTIES, ALABAMA**

**M M**  
**MOTT MACDONALD**

**FJR**  
**thompson**  
**consultants**

**HISTORIC DISTRICTS AND AREA OF POTENTIAL EFFECT**

PROJECT NO.: 15-1101-0390      DATE: DECEMBER 2018

*Attachment #2. Archaeological Investigation Maps*

{REDACTED}

**Attachment #3. Section 106 Consulting Parties and Tribal Contact Information**

**Section 106 Consulting Party Contact Information**

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The Honorable Sandy Stimpson  
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Commissioner Connie Hudson  
President  
Mobile County Commission  
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The Honorable Dane Haygood  
Mayor, City of Daphne  
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The Honorable Michael McMillan  
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**Attachment #4. Post-Review Discovery Plan**

- A. When notified by the Concessionaire or other outside party, ALDOT shall notify FHWA immediately if it appears that a FHWA funded undertaking has affected a previously unidentified property that may be eligible for the *National Register* or affected a known historic property in an unanticipated manner.
  - 1. ALDOT shall require the Concessionaire to stop construction activities in the vicinity of the discovery and shall require the Concessionaire to take all reasonable measures to avoid or minimize harm to the property until FHWA concludes consultation with SHPO or THPO or Tribes.
  - 2. FHWA shall notify SHPO or THPO and Tribes at the earliest possible time, but no later than 72 hours, and consult to develop actions that will take into account the effects of the undertaking.
- B. When notified by a Concessionaire, ALDOT shall notify FHWA at the earliest possible time, but no later than 72 hours, if intact archaeological deposits are uncovered in the course of any undertaking.
  - 1. ALDOT shall require the Concessionaire to stop all work immediately in the vicinity of the discovery and take reasonable measures to avoid or minimize harm to the finds. The site and all archaeological findings shall be secured and access to the APE of the individual project restricted.
  - 2. The Concessionaire shall inform FHWA immediately and FHWA shall consult with SHPO or THPO and Tribes.
  - 3. Work in the APE of the project cannot resume until consultation is completed or until an archeologist who meets the *Professional Qualifications* determines the extent of the archeological deposit. Work may then resume in unaffected areas of the APE outside of the delineated deposit.
- C. If an unmarked grave, indications of a burial, or human remains are present, compliance with the Alabama Cemetery and Human Remains Protection Act is required.
  - 1. ALDOT shall require the Concessionaire to stop work immediately in the vicinity of the discovery and secure the area. ALDOT shall immediately notify FHWA and the law enforcement agencies of the discovery.
  - 2. Within twenty-four hours of notification by ALDOT, FHWA shall notify and coordinate with the Tribes. The local law enforcement officials, in concert with a professional bioarchaeologist, shall assess the nature and age of the human skeletal remains. FHWA shall notify the Alabama Historical Commission at the earliest possible time after the discovery. If the coroner, bioarchaeologist, and/or appropriate local official determines that the human skeletal remains are older than 50 years of age, the Alabama Historical Commission has jurisdiction over the remains until final determinations of origin are made.
  - 3. In all cases, FHWA shall follow guidelines set forth by the ACHP in its "Human Remains Policy."
- D. In cases where the human remains are determined to be American Indian:
  - 1. FHWA shall take the lead in working with Tribes and the Alabama Historical Commission and consulting parties to ensure compliance with the Alabama Cemetery and Human Remains Protection Act and other applicable laws. In addition, FHWA shall follow guidelines set forth by the ACHP in its "Human Remains Policy."

2. FHWA shall hold a consultation meeting about the remains with Tribes and representatives of the Alabama Historical Commission as necessary. Such a consultation meeting may include a site visit to review the situation.
  3. In all cases, the preferred action is to avoid further disturbance of the remains, unless there is no alternative to further disturbance.
- E. FHWA shall also notify SHPO or THPO and Tribes of any time constraints, and FHWA and SHPO or THPO and Tribes shall mutually agree upon timeframes for this consultation. ALDOT and the Concessionaire may participate in this consultation. FHWA shall provide SHPO or THPO and/or Tribes with written recommendations that take into account the effect of the undertaking. If SHPO or THPO and Tribes do not object to FHWA's recommendations within the agreed upon timeframe, FHWA shall require the Concessionaire to modify the scope of work as necessary to implement the recommendations.

DRAFT

## ALABAMA HISTORICAL COMMISSION

*The State Historic Preservation Office*

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### **Desecration, defacement, etc., of memorial of dead; invasion or mutilation of corpse.**

#### **Code of Alabama 1975, §13A-7-23.1, as amended**

(a) Any person who willfully or maliciously injures, defaces, removes, or destroys any tomb, monument, gravestone, burial mound, earthen or shell monument containing human skeletal remains or associated burial artifacts, or other structure or thing placed or designed for a memorial of the dead, or any fence, railing, curb, or any enclosure for the protection or ornamentation of any tomb, monument, gravestone, burial mound, earthen or shell monument containing human skeletal remains or associated burial artifacts, or other structure before mentioned, or for any enclosure for the burial of the dead, or any person who willfully and wrongfully or maliciously destroys, removes, cuts, breaks, or injures any tree, shrub, plant, flower, decoration, or other real or personal property within any cemetery or graveyard shall be guilty of a Class A misdemeanor.

(b) Any person who willfully or maliciously desecrates, injures, defaces, removes, or destroys any tomb, monument, structure, or container of human remains, burial mound, earthen or shell monument containing human skeletal remains or associated burial artifacts, and invades or mutilates the human corpse or remains shall be guilty of a Class C felony and upon conviction the person shall be punished as provided by law.

(c) The provisions of subsections (a) and (b) shall not apply to any person holding a permit issued by the Alabama Historical Commission pursuant to subsection (d), to anyone operating a cemetery under standard rules and regulations and maintenance procedures, or to any person otherwise authorized by law to remove or disturb a tomb, monument, grave marker, burial mound, earthen or shell monument, or similar structure, or its contents, as described in subsections (a) and (b), nor shall the provisions of subsections (a) and (b) apply to any person authorized to take any action on municipal property.

(d) The Alabama Historical Commission, to provide for the lawful preservation, investigation, restoration, or relocation of human burial remains, human skeletal remains, or funerary objects, shall promulgate rules and regulations for the issuance of a permit and may issue a permit to persons or companies who seek to restore, preserve, or relocate human burial remains, human skeletal remains, funerary objects, or otherwise disturb a place of burial."

(Acts 1980, No. 80-706, p. 1424; Acts 1993, No. 93-770, §1; Acts 1993, 1st Ex. Sess., No. 93-905, p. 201, §1;

**Act 2010-723).**

See also Administrative Code, Chapter 460-X-10.01

*This paper is for reference purposes only and does not constitute legal advice.*

**Attachment #6. Advisory Council on Historic Preservation- Policy Statement Regarding Treatment of Burial Sites, Human Remains and Funerary Objects**





*Preserving America's Heritage*

## ADVISORY COUNCIL ON HISTORIC PRESERVATION

### POLICY STATEMENT REGARDING

### TREATMENT OF BURIAL SITES, HUMAN REMAINS AND FUNERARY OBJECTS

**Preamble:** This policy offers leadership in resolving how to treat burial sites, human remains, and funerary objects in a respectful and sensitive manner while acknowledging public interest in the past. As such, this policy is designed to guide federal agencies in making decisions about the identification and treatment of burial sites, human remains, and funerary objects encountered in the Section 106 process, in those instances where federal or state law **does not prescribe a course of action**.

This policy applies to all federal agencies with undertakings that are subject to review under Section 106 of the National Historic Preservation Act (NHPA; 16 U.S.C. § 470f), and its implementing regulations (36 CFR Part 800). To be considered under Section 106, the burial site must be or be a part of a historic property, meaning that it is listed, or eligible for listing, in the National Register of Historic Places.

The Advisory Council on Historic Preservation (ACHP) encourages federal agencies to apply this policy throughout the Section 106 process, including during the identification of those historic properties. In order to identify historic properties, federal agencies must assess the historic significance of burial sites and apply the National Register criteria to determine whether a property is eligible. Burial sites may have several possible areas of significance, such as those that relate to religious and cultural significance, as well as those that relate to scientific significance that can provide important information about the past. This policy does not proscribe any area of significance for burial sites and recognizes that the assessment must be completed on a case-by-case basis through consultation.

The policy is not bound by geography, ethnicity, nationality, or religious belief, but applies to the treatment of all burial sites, human remains, and funerary objects encountered in the Section 106 process, as the treatment and disposition of these sites, remains, and objects are a human rights concern shared by all.

This policy also recognizes the unique legal relationship between the federal government and tribal governments as set forth in the Constitution of the United States, treaties, statutes and court decisions, and acknowledges that, frequently, the remains encountered in Section 106 review are of significance to Indian tribes.

Section 106 requires agencies to seek agreement with consulting parties on measures to avoid, minimize, or mitigate adverse effects to historic properties. Accordingly, and consistent with Section 106, this policy does not recommend a specific outcome from the consultation process. Rather, it focuses on issues and perspectives that federal agencies ought to consider when making their Section 106 decisions. In many cases, federal agencies will be bound by other applicable federal, tribal, state, or local laws that do



prescribe a specific outcome, such as the Native American Graves Protection and Repatriation Act (NAGPRA). The federal agency must identify and follow applicable laws and implement any prescribed outcomes.

For undertakings on federal and tribal land that encounter Native American or Native Hawaiian human remains and funerary objects, NAGPRA applies. NHPA and NAGPRA are separate and distinct laws, with separate and distinct implementing regulations and categories of parties that must be consulted.<sup>1</sup> Compliance with one of these laws does not mean or equal compliance with the other. Implementation of this policy and its principles does not, in any way, change, modify, detract or add to NAGPRA or other applicable laws.

**Principles:** When burial sites, human remains, or funerary objects will be or are likely to be encountered in the course of Section 106 review, a federal agency should adhere to the following principles:

**Principle 1:** Participants in the Section 106 process should treat all burial sites, human remains and funerary objects with dignity and respect.

**Principle 2:** Only through consultation, which is the early and meaningful exchange of information, can a federal agency make an informed and defensible decision about the treatment of burial sites, human remains, and funerary objects.

**Principle 3:** Native Americans are descendants of original occupants of this country. Accordingly, in making decisions, federal agencies should be informed by and utilize the special expertise of Indian tribes and Native Hawaiian organizations in the documentation and treatment of their ancestors.

**Principle 4:** Burial sites, human remains and funerary objects should not be knowingly disturbed unless absolutely necessary, and only after the federal agency has consulted and fully considered avoidance of impact and whether it is feasible to preserve them in place.

**Principle 5:** When human remains or funerary objects must be disinterred, they should be removed carefully, respectfully, and in a manner developed in consultation.

**Principle 6:** The federal agency is ultimately responsible for making decisions regarding avoidance of impact to or treatment of burial sites, human remains, and funerary objects. In reaching its decisions, the federal agency must comply with applicable federal, tribal, state, or local laws.

**Principle 7:** Through consultation, federal agencies should develop and implement plans for the treatment of burial sites, human remains, and funerary objects that may be inadvertently discovered.

**Principle 8:** In cases where the disposition of human remains and funerary objects is not legally prescribed, federal agencies should proceed following a hierarchy that begins with the rights of lineal descendants, and if none, then the descendant community, which may include Indian tribes and Native Hawaiian organizations.

<sup>1</sup> The ACHP's publication *Consulting with Indian Tribes in the Section 106 Process* and the National Association of Tribal Historic Preservation Officers' publication *Tribal Consultation: Best Practices in Historic Preservation* provide additional guidance on this matter.

**DISCUSSION:**

**Principle 1: Participants in the Section 106 process should treat all burial sites, human remains and funerary objects with dignity and respect.**

Because the presence of human remains and funerary objects gives a historic property special importance as a burial site or cemetery, federal agencies need to consider fully the values associated with such sites. When working with human remains, the federal agency should maintain an appropriate deference for the dead and the funerary objects associated with them, and demonstrate respect for the customs and beliefs of those who may be descended from them.

Through consultation with descendants, culturally affiliated groups, descendant communities, and other parties, federal agencies should discuss and reach agreement on what constitutes respectful treatment.

**Principle 2: Only through consultation, which is the early and meaningful exchange of information, can a federal agency make an informed and defensible decision about the treatment of burial sites, human remains, and funerary objects.**

Consultation is the hallmark of the Section 106 process. Federal agencies must make a “reasonable and good faith” effort to identify consulting parties and begin consultation early in project planning, after the federal agency determines it has an undertaking and prior to making decisions about project design, location, or scope.

The NHPA, the ACHP’s regulations, and Presidential Executive Orders set out basic steps, standards, and criteria in the consultation process, including:

- Federal agencies have an obligation to seek out all consulting parties [36 CFR § 800.2(a)(4)], including the State Historic Preservation Officer (SHPO)/Tribal Historic Preservation Officer (THPO) [36 CFR § 800.3(c)].
- Federal agencies must acknowledge the sovereign status of Indian tribes [36 CFR § 800.2(c)(2)(ii)]. Federal agencies are required to consult with Indian tribes on a government-to-government basis in recognition of the unique legal relationship between federal and tribal governments, as set forth in the Constitution of the United States, treaties, statutes, court decisions, and executive orders and memoranda.
- Consultation on a government-to-government level with Indian tribes cannot be delegated to non-federal entities, such as applicants and contractors.
- Federal agencies should solicit tribal views in a manner that is sensitive to the governmental structures of the tribes, recognizing their desire to keep certain kinds of information confidential, and that tribal lines of communication may argue for federal agencies to provide extra time for the exchange of information.

- Properties of traditional religious and cultural importance to an Indian tribe or Native Hawaiian organization may be determined eligible for inclusion on the National Register [16 U.S.C. § 470a(d)(6)(A)], and federal agencies must consult with any Indian tribe or Native Hawaiian organization that attaches religious and cultural significance to such historic properties [16 U.S.C. § 470a(d)(6)(B) and 36 CFR § 800.2(c)(2)(ii)(D)].

**Principle 3: Native Americans are descendants of original occupants of this country. Accordingly, in making decisions, federal agencies should be informed by and utilize the special expertise of Indian tribes and Native Hawaiian organizations in the documentation and treatment of their ancestors.**

This principle reiterates existing legal requirements found in federal law, regulation and executive orders, and is consistent with positions that the ACHP has taken over the years to facilitate enfranchisement and promote broad participation in the Section 106 process. Federal agencies must consult with Indian tribes on a government-to-government basis because they are sovereign nations.

Indian tribes and Native Hawaiian organizations bring a special perspective on how a property possesses religious and cultural significance to them. Accordingly, federal agencies should utilize their expertise about, and religious and cultural connection to, burial sites, human remains, and associated funerary objects to inform decision-making in the Section 106 process.

**Principle 4: Burial sites, human remains and funerary objects should not be knowingly disturbed unless absolutely necessary, and only after the federal agency has consulted and fully considered avoidance of impact and whether it is feasible to preserve them in place.**

As a matter of practice, federal agencies should avoid impacting burial sites, human remains, and funerary objects as they carry out their undertakings. If impact to the burial site can be avoided, this policy does not compel federal agencies to remove human remains or funerary objects just so they can be documented.

As this policy advocates, federal agencies should always plan to avoid burial sites, human remains, and funerary objects altogether. When a federal agency determines, based on consultation with Section 106 participants, that avoidance of impact is not appropriate, the agency should minimize disturbance to such sites, remains, and objects. Accordingly, removal of human remains or funerary objects should occur only when other alternatives have been considered and rejected.

When a federal agency determines, based on consultation with Section 106 participants, that avoidance of impact is not appropriate, the agency should then consider any active steps it may take to preserve the burial site in place, perhaps through the intentional covering of the affected area, placement of markers, or granting of restrictive or other legal protections. In many cases, preservation in place may mean that, to the extent allowed by law, the locations of burial sites, human remains, and funerary objects should not be disclosed publicly. Alternatively and consistent with the Section 106 regulations [36 CFR § 800.5(a)(2)(vi)], natural deterioration of the remains may be the acceptable or preferred outcome of the consultation process.

**Principle 5: When human remains or funerary objects must be disinterred, they should be removed carefully, respectfully, and in a manner developed in consultation.**

When the federal agency decides that human remains or funerary objects must be disturbed, they should be removed respectfully and dealt with according to the plan developed by the federal agency in consultation. "Careful" disinterment means that those doing the work should have, or be supervised by people having, appropriate expertise in techniques for recognizing and disinterring human remains.

This policy does not endorse any specific treatment. However, federal agencies must make a reasonable and good faith effort to seek agreement through consultation before making its decision about how human remains and/or funerary objects shall be treated.

The plan for the disinterment and treatment of human remains and/or funerary objects should be negotiated by the federal agency during consultation on a case-by-case basis. However, the plan should provide for an accurate accounting of federal implementation. Depending on agreements reached through the Section 106 consultation process, disinterment may or may not include field recordation. In some instances, such recordation may be so abhorrent to consulting parties that the federal agency may decide it is inappropriate to carry it out. When dealing with Indian tribes, the federal agency must comply with its legal responsibilities regarding tribal consultation, including government-to-government and trust responsibilities, before concluding that human remains or funerary objects must be disinterred.

**Principle 6: The federal agency is ultimately responsible for making decisions regarding avoidance of impact to or treatment of burial sites, human remains, and funerary objects. In reaching its decisions, the federal agency must comply with applicable federal, tribal, state, or local laws.**

Federal agencies are responsible for making final decisions in the Section 106 process [36 CFR § 800.2(a)]. The consultation and documentation that are appropriate and necessary to inform and support federal agency decisions in the Section 106 process are set forth in the ACHP's regulations [36 CFR Part 800].

Other laws, however, may affect federal decision-making regarding the treatment of burial sites human remains, and funerary objects. Undertakings located on federal or tribal lands, for example, are subject to the provisions of NAGPRA and the Archaeological Resources Protection Act (ARPA). When burial sites, human remains, or funerary objects are encountered on state and private lands, federal agencies must identify and follow state law when it applies. Section 106 agreement documents should take into account the requirements of any of these applicable laws.

**Principle 7: Through consultation, federal agencies should develop and implement plans for the treatment of burial sites, human remains, and funerary objects that may be inadvertently discovered.**

Encountering burial sites, human remains, or funerary objects during the initial efforts to identify historic properties is not unheard of. Accordingly, the federal agency must determine the scope of the identification effort in consultation with the SHPO/THPO, Indian tribes and Native Hawaiian

organizations, and others before any archaeological testing has begun [36 CFR § 800.4(a)] to ensure the full consideration of avoidance of impact to burial sites, human remains, and funerary objects.

The ACHP's regulations provide federal agencies with the preferred option of reaching an agreement ahead of time to govern the actions to be taken when historic properties are discovered during the implementation of an undertaking. In the absence of prior planning, when the undertaking has been approved and construction has begun, the ACHP's post-review discovery provision [36 CFR § 800.13] requires the federal agency to carry out several actions:

- (1) make reasonable efforts to avoid, minimize, or mitigate adverse effects to such discovered historic properties;
- (2) notify consulting parties (including Indian tribes and Native Hawaiian organizations that might attach religious and cultural significance to the affected property) and the ACHP within 48 hours of the agency's proposed course of action;
- (3) take into account the recommendations received; and then
- (4) carry out appropriate actions.

NAGPRA prescribes a specific course of action when Native American and Native Hawaiian human remains and funerary objects are discovered on federal or tribal lands in the absence of a plan—cessation of the activity, protection of the material, notification of various parties, consultation on a course of action and its implementation, and then continuation of the activity. However, adherence to the plan under Principle 5 would cause new discoveries to be considered “intentional excavations” under NAGPRA because a plan has already been developed, and can be immediately implemented. Agencies then could avoid the otherwise mandated 30 day cessation of work for “inadvertent discoveries.”

**Principle 8: In cases where the disposition of human remains and funerary objects is not legally prescribed, federal agencies should proceed following a hierarchy that begins with the rights of lineal descendants, and if none, then the descendant community, which may include Indian tribes and Native Hawaiian organizations.**

Under the ACHP's regulations, “descendants” are not identified as consulting parties by right. However, federal agencies shall consult with Indian tribes and Native Hawaiian organizations that attach religious and cultural significance to burial sites, human remains and associated funerary objects, and be cognizant of their expertise in, and religious and cultural connection to, them. In addition, federal agencies should recognize a biological or cultural relationship and invite that individual or community to be a consulting party [36 CFR § 800.3(f)(3)].

When federal or state law does not direct disposition of human remains or funerary objects, or when there is disagreement among claimants, the process set out in NAGPRA may be instructive. In NAGPRA, the “ownership or control” of human remains and associated funerary objects lies with the following in descending order: specific lineal descendants; then tribe on whose tribal lands the items were discovered; then tribe with the closest cultural affiliation; and then tribe aboriginally occupying the land, or with the closest “cultural relationship” to the material.

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## Definitions Used for the Principles

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- **Burial Site:** Any natural or prepared physical location, whether originally below, on, or above the surface of the earth, into which as a part of the death rite or ceremony of a culture, individual human remains are deposited [25 U.S.C. 3001.2(1)].
- **Consultation:** The process of seeking, discussing, and considering the views of other participants, and, where feasible, seeking agreement with them regarding matters arising in the Section 106 review process [36 CFR § 800.16(f)].
- **Consulting parties:** Persons or groups the federal agency consults with during the Section 106 process. They may include the State Historic Preservation Officer; the Tribal Historic Preservation Officer; Indian tribes and Native Hawaiian organizations; representatives of local governments; applicants for federal assistance, permits, licenses, and other approvals; and/or any additional consulting parties [based on 36 CFR § 800.2(c)]. Additional consulting parties may include individuals and organizations with a demonstrated interest in the undertaking due to the nature of their legal or economic relation to the undertaking or affected properties, or their concern with the undertaking's effects on historic properties [36 CFR § 800.2(c)(6)].
- **Disturbance:** Disturbance of burial sites that are listed in or eligible for listing in the National Register of Historic Places will constitute an adverse effect under Section 106. An adverse effect occurs when "an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, setting, materials, workmanship, feeling, or association" [36 CFR § 800.5(a)(1)].
- **Federal land:** Lands under a federal agency's control. Mere federal funding or permitting of a project does not turn an otherwise non-federal land into federal land (see *Abenaki Nation of Mississquoi v. Hughes*, 805 F. Supp. 234 (D. Vt. 1992), aff'd, 990 F. 2d 729 (2d Cir. 1993) (where the court found that a Clean Water Act permit issued by the US Army Corps of Engineers did not place the relevant land under federal "control" for NAGPRA purposes).
- **Funerary objects:** "items that, as part of the death rite or ceremony of a culture, are reasonably believed to have been placed intentionally at the time of death or later with or near individual human remains" [25 U.S.C. 3001(3)(B)].
- **Historic property:** "Any prehistoric or historic district, site, building, structure, or object included in, or eligible for inclusion in, the National Register of Historic Places maintained by the Secretary of the Interior. It includes artifacts, records, and remains that are related to and located within such properties, and it includes properties of traditional religious and cultural importance to an Indian tribe or Native Hawaiian organization and that meet the National Register of Historic Places criteria" [36 CFR § 800.16(1)].
- **Human remains:** The physical remains of a human body. The term does not include remains or portions of remains that may reasonably be determined to have been freely given or naturally shed by the individual from whose body they were obtained, such as hair made into ropes or nets [see 43 CFR § 10.2(d)(1)].
- **Indian Tribe:** "An Indian tribe, band, nation, or other organized group or community, including a Native village, Regional Corporation or Village Corporation, as those terms are defined in Section 3 of the Alaska Native Claims Settlement Act [43 U.S.C. 1602], which is recognized as eligible for the special programs and services provided by the United States to Indians because of their status as Indians" [36 CFR § 800.16(m)].
- **Native American:** Of, or relating to, a tribe, people, or culture that is indigenous to the United States [25 U.S.C. 3001 (9)]. Of, or relating to, a tribe, people, or culture indigenous to the United States, including Alaska and Hawaii [43 CFR 10.2(d)].



- **Native Hawaiian:** Any individual who is a descendant of the aboriginal people who, prior to 1778, occupied and exercised sovereignty in the area that now constitutes the state of Hawaii [36 CFR § 800.16(s)(2)].
- **Native Hawaiian Organization:** Any organization which serves and represents the interests of Native Hawaiians; has as a primary and stated purpose the provision of services to Native Hawaiians; and has demonstrated expertise in aspects of historic preservation that are significant to Native Hawaiians [36 CFR § 800.16(s)].
- **Policy statement:** A formal statement, endorsed by the full ACHP membership, representing the membership's collective thinking about what to consider in reaching decisions about select issues, in this case, human remains and funerary objects encountered in undertakings on federal, tribal, state, or private lands. Such statements do not have the binding force of law.
- **Preservation in place:** Taking active steps to ensure the preservation of a property.
- **Protection of Historic Properties:** Regulations [36 CFR Part 800] implementing Section 106 of the National Historic Preservation Act.
- **Section 106:** That part of the National Historic Preservation Act which establishes a federal responsibility to take into account the effects of undertakings on historic properties and to provide the Advisory Council on Historic Preservation a reasonable opportunity to comment with regard to such action.
- **State Historic Preservation Officer:** The official appointed or designated pursuant to Section 101(b)(1) of NHPA to administer the state historic preservation program.
- **Tribal Historic Preservation Officer:** The official appointed by the tribe's chief governing authority or designated by a tribal ordinance or preservation program who has assumed the responsibilities of the SHPO for purposes of Section 106 compliance on tribal lands in accordance with Section 101(d)(2) of NHPA.
- **Treatment:** Under Section 106, "treatments" are measures developed and implemented through Section 106 agreement documents to avoid, minimize, or mitigate adverse effects to historic properties.

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#### **Acronyms Used for the Policy Statement**

- **ACHP:** Advisory Council on Historic Preservation.
- **ARPA:** Archaeological Resources Protection Act [16 U.S.C. 470aa-mm].
- **NHPA:** National Historic Preservation Act [16 U.S.C. § 470f].
- **NAGPRA:** The Native American Graves Protection and Repatriation Act [25 U.S.C. 3001 et seq].
- **SHPO:** State Historic Preservation Officer
- **THPO:** Tribal Historic Preservation Officer

*[The members of the Advisory Council on Historic Preservation unanimously adopted this policy on February 23, 2007]*

## Attachment #7. Aesthetic Steering Committee Framework

### **A. Purpose of Aesthetic Steering Committee**

Major infrastructure projects around the U.S. have increasingly included an Aesthetics Steering Committee to assist in engaging communities with the aesthetic design. For this project, ALDOT will develop an Aesthetic Steering Committee to provide input on preferences regarding the aesthetics of the project. The Aesthetic Steering Committee will serve on behalf of the community and Section 106 Consulting Parties to provide input on likes, dislikes, and preferences related to aesthetics so that ALDOT can communicate those preferences to the proposing teams and ensure that commitments related to aesthetics are upheld as the project is designed and constructed. To encourage open and honest feedback on aesthetic preferences, the members of the committee will not be released to the proposing teams or the public until after a team is selected to design, build, finance, operate, and maintain the project. During the pre-proposal phase, the proposing teams will receive input from the Committee through ALDOT. After a team is selected, the winning team will work directly with the Aesthetic Steering Committee and ALDOT to finalize the aesthetic components of the project.

### **B. Members of Aesthetic Steering Committee**

The Aesthetic Steering Committee will be comprised of members from Mobile and Baldwin Counties. The following organizations will be invited to participate as members of the Aesthetic Steering Committee:

- {REDACTED}
- {REDACTED}
- {REDACTED}
- {REDACTED}
- {REDACTED}
- {REDACTED}
- {REDACTED}
- {REDACTED}
- {REDACTED}

Eight of the nine organizations invited to serve on the Committee are Section 106 Consulting Parties. The invitees consist of individuals and organizations with interests in historic resources, as well as the region as a whole. They have a diverse background in terms of training and education, which will allow them to provide a variety of perspectives as part of this process.

### **C. Roles and Responsibilities of Committee**

The involvement of the Aesthetic Steering Committee will be a collaborative process that occurs through in-person meetings. The Aesthetic Steering Committee will meet with ALDOT as needed to develop Aesthetic Guidelines for the project and to provide feedback on the Aesthetic and Landscape Plans submitted by the proposing teams. The Committee will also work with the selected team during the final design and construction phase(s) of the project.

The Aesthetic Steering Committee will be responsible for assisting ALDOT in the development of Aesthetic Guidelines to address commitments and preferences related to the following aesthetic elements:

- Land use compatibility,
- Aesthetics,
- Landscaping,
- Form commonality,
- Materials and finishes,
- Barriers,
- Retaining walls,
- Overhead gantries and sign structures,
- Bridge structures,
- Interchange areas,
- Straddle bents,
- High level approaches to main span of bridge,
- Bicycle/pedestrian amenities and connectivity,

- Treatment of areas beneath the Mobile River Bridge and its approach structures,
- Roadway and bridge lighting, and
- Aesthetic lighting.

The Aesthetic Steering Committee will also provide input on appropriate themes and regional context that should be used by the teams to create a project that reflects the culture and history of the project area and complements its setting.

The following table provides a list of activities in which the Aesthetic Steering Committee will participate:

<b><i>Activity</i></b>	<b><i>Purpose/Focus</i></b>
Initial Meeting	<ul style="list-style-type: none"> <li>• Learn about the proposed project through available design information, including typical sections, maps, and a visualization/animation.</li> <li>• Discuss various aesthetic components to be included in the project.</li> <li>• Review photographs and drawings of bridges and other project components (such as ramps, interchanges, roadways, lighting, etc.) from projects around the world to identify likes and dislikes.</li> <li>• Discuss what makes the Mobile and Baldwin County region unique and what aspects should be incorporated into themes for the project.</li> </ul>
Meeting on Precedent Images	<ul style="list-style-type: none"> <li>• Review precedent images showing different aesthetic components (bridge railings, retaining walls, roadway lighting, bridge lighting, aesthetic lighting, landscaping, colors, materials, etc.) to identify likes and dislikes.</li> </ul>
Meeting on Draft Aesthetic Guidelines	<ul style="list-style-type: none"> <li>• Review Draft Aesthetic Guidelines developed based on input received from Committee during previous meetings.</li> </ul>
Meeting to Review Pre-Proposal Preliminary Aesthetic and Landscape Plans – Submittal #1	<ul style="list-style-type: none"> <li>• Review pre-proposal preliminary Aesthetic and Landscape Plans submitted by proposing teams. The primary aesthetic elements contained in this initial submittal from the proposing teams are expected to include the following: overall design approach and theme, main span bridge and structures, retaining walls, aesthetic lighting, landscape and urban design, and pedestrian access.</li> <li>• Provide comments on submittals, focusing on likes, dislikes, and how well the package reflects the preferences set forth in the Aesthetic Guidelines.</li> <li>• ALDOT to share the feedback from the Committee with the proposing teams.</li> </ul>
Meeting to Review Pre-Proposal Preliminary Aesthetic and Landscape Plans – Submittal #2	<ul style="list-style-type: none"> <li>• Review revised preliminary Aesthetic and Landscape Plans submitted by proposing teams. The primary aesthetic elements contained in this initial submittal from the proposing teams are expected to include the following: overall design approach and theme, main span bridge and structures, retaining walls, aesthetic lighting, landscape and urban design, and pedestrian access.</li> <li>• Provide comments on submittals, focusing on likes, dislikes, and how well the package reflects the preferences set forth in the Aesthetic Guidelines.</li> <li>• ALDOT to share the feedback from the Committee with the proposing teams.</li> </ul>
Meeting to Review Pre-Proposal Preliminary Aesthetic and Landscape Plans – Submittal #3	<ul style="list-style-type: none"> <li>• Review revised preliminary Aesthetic and Landscape Plans submitted by proposing teams. The primary aesthetic elements contained in this initial submittal from the proposing teams are expected to include the following: overall design approach and theme, main span bridge and structures, retaining walls, aesthetic lighting, landscape and urban design, and pedestrian access.</li> <li>• Provide comments on submittals, focusing on likes, dislikes, and how well the package reflects the preferences set forth in the Aesthetic Guidelines.</li> <li>• ALDOT to share the feedback from the Committee with the proposing teams.</li> </ul>

<i>Activity</i>	<i>Purpose/Focus</i>
Meeting to Review Aesthetic and Landscape Plans Submitted with Proposals	<ul style="list-style-type: none"> <li>• Review Aesthetic and Landscape Plans submitted as part of each team's proposal.</li> <li>• Provide comments on submittals.</li> <li>• ALDOT to use the feedback from the Committee to evaluate the Aesthetic and Landscape Plans in each team's proposal.</li> </ul>
Meeting(s) with Selected Team during Design and Construction Phase(s)	<ul style="list-style-type: none"> <li>• Meet directly with the selected team and ALDOT to finalize the details of Aesthetic and Landscape Plans.</li> <li>• Provide input on more detailed components of the project, such as light fixtures, colors, types of materials, signage, aesthetic lighting, barriers, and other elements.</li> <li>• May require multiple meetings.</li> </ul>

**D. Updates to Section 106 Consulting Parties regarding Aesthetic Steering Committee Activities**

ALDOT will provide summaries of Aesthetic Steering Committee activities to Section 106 Consulting Parties after the meetings occur.

DRAFT

# Final Report on Vibrations Due to Pile Driving at the Mobile River Bridge Site

Research Project 930-839R

*INVESTIGATION OF PILE SETUP (FREEZE) IN ALABAMA*

*Development of a Setup Prediction Method and Implementation into LRFD Driven Pile Design*

*Addendum: Pile Driving Vibration Monitoring of the Future Mobile River Bridge Project*



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## **DISCLAIMER**

The contents of this report reflect the views of the authors who are responsible for the facts and accuracy of the data presented herein. The contents do not necessarily reflect the official views or policies of the Alabama DOT or the University of South Alabama. This report does not constitute a standard, specification, or regulation. Comments contained in this paper related to specific testing equipment and materials should not be considered an endorsement of any commercial product or service; no such endorsement is intended or implied.



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## ABSTRACT

All projects have some amount of inherent risk; one such risk associated with construction projects is the potential for ground vibrations that could damage nearby structures. Research has been conducted on the effects of vibrations on structures; however, the expected levels of vibration are dependent on several factors including the soil conditions at the construction site. Therefore, site-specific investigations are often recommended.

After concerns were raised by the Alabama Department of Transportation (ALDOT) about damage potential at a project site in South Alabama, an addendum was added to a research project related to investigating pile setup in Alabama soils. The purpose of the addendum was to investigate ground vibrations from pile driving at a project site near the Mobile River in Mobile, Alabama.

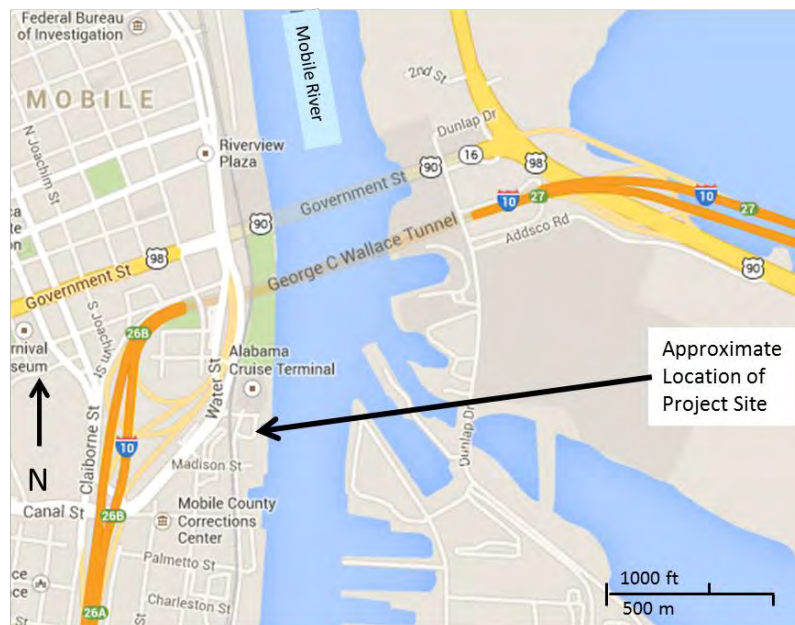
An investigation and vibration monitoring program was developed for four pile sizes that are often used by the Alabama Department of Transportation (ALDOT). The piles included thirty-six inch square and twenty-four inch square concrete piles, as well as, two steel H-Piles. The piles were driven using typical installation techniques and the vibration levels at various distances from the piles were monitored.

The investigation found that the largest vibrations were observed while driving the thirty-six inch concrete pile. The maximum vibrations observed had a magnitude of 0.82 inches per second at fifty feet from the pile. The vibrations at 150 feet from the pile had dissipated to 0.15 inches per second. The results of the monitoring program and a literature review determined that an allowable vibration level of 0.5 inches per second for modern structures and 0.1 inches per second for potentially sensitive structures should be established for construction activity at or near the location of the project site. Additionally, a survey distance of 150 feet for modern structures and 250 feet for potentially sensitive structures is recommended.

# INTRODUCTION

## Background

The following report contains the analysis of ground vibrations generated during a pile driving research study located at the Mobile River Bridge Project Site. The project site, owned by the Alabama Department of Transportation (ALDOT), is located on the Mobile River just south of the Alabama Cruise Terminal, Figure 1. The study consisted of monitoring ground vibrations during the installation of four driven piles; two precast concrete piles and two steel H-piles. The study was conducted in response to concerns raised by ALDOT related to possible damage of nearby structures from ground-borne vibrations. The primary objective of this project was to determine the distance that pile driving operations can be conducted with minimal risk to nearby structures. To accomplish this, the vibration levels at various distances from the driven piles were determined and a prediction equation for other distances was developed. This study was conducted by researchers from the Department of Civil Engineering at the University of South Alabama between August 15, 2013 and August 27, 2013.



**Figure 1:** Location of project site, Mobile, AL (Google 2013)

## Objective

This project consisted of several objectives. The first was to determine the vibration levels from typical piles used by ALDOT. The second objective was to develop a methodology to predict vibrations at any distance from the pile. The third and final objective of the project was to develop guidelines on allowable vibrations for the project site.

## **Scope**

The scope of this report is limited to the vibrations portion of the larger project: *Investigation of Pile Setup (Freeze) In Alabama: Development of a Setup Prediction Method and Implementation into LRFD Driven Pile Design; Addendum: Pile Driving Vibration Monitoring of the Future Mobile River Bridge Project* (Research Project 930-839R).

The vibrations portion of the project was limited to the aforementioned location near the Mobile River. The project included monitoring vibrations during pile installation and restrikes, analysis of vibration data, development of vibration prediction methodology, and vibration limit recommendations.

## **Report Organization**

The report is organized into five main sections: Introduction, Literature Review, Experimental Design, Results, and Conclusions. Each section contains sub sections as needed.

## LITERATURE REVIEW

### Construction Vibrations

Ground vibrations are commonly generated from several sources including roadway traffic, railroad traffic, and construction activity. Vibrations can be measured and quantified using several different parameters including: displacement, velocity, and acceleration. Ground vibrations are typically measured by the velocity of the ground surface and reported as Peak Particle Velocity or PPV. Typical units of PPV are inches per second (in/sec) in the US system or millimeters per second (mm/sec) in the SI system of units. Typical construction activity that generates vibrations includes: pile driving, heavy equipment operation, concrete breaking (jackhammers), and truck/equipment traffic. Although the level of vibrations generated from these sources can vary widely, some typical vibration levels have been included in Table 1.

**Table 1:** Typical ground vibrations from construction equipment (Hanson, Towes and Lance 2006)

<b>Equipment</b>		<b>PPV (in/sec)</b> (Distance = 25 ft.)
Pile Driver (impact)	upper range	1.518
	typical	0.644
Pile Driver (vibratory)	upper range	0.734
	typical	0.170
Bulldozer	large	0.089
	small	0.003
Caisson Drilling		0.089
Loaded Trucks		0.076
Jackhammer		0.035

Table 1 shows that under typical conditions, pile driving has the potential to create large vibration levels, relative to other construction activity. The pile installation method, however, can affect the level of vibrations. High displacement piles are typically driven using an impact hammer and low displacement piles are sometimes driven using a vibratory hammer. Research has shown that the vibration magnitudes from vibratory hammers are typically smaller than from impact hammers. Additionally, installation techniques such as pre-boring and jetting can reduce vibration levels from impact pile driving (Woods 1997).

The mechanism of vibration formation is the transfer of energy from the pile driving hammer to the pile and then to the surrounding soil. The transfer of energy comes from two main sources. The first is the skin friction that is developed along the surface of the pile and the second is the displacement of the soil at the pile tip. For high displacement piles, the main source of energy transfer is at the pile tip. Several factors can affect the magnitude of vibrations including pile size, pile type, soil type, and the hammer energy. The most important factor in determining vibration levels is the distance from the pile, since vibrations will mitigate or dampen with distance from the source (Dowding 1996).

## Damage Thresholds

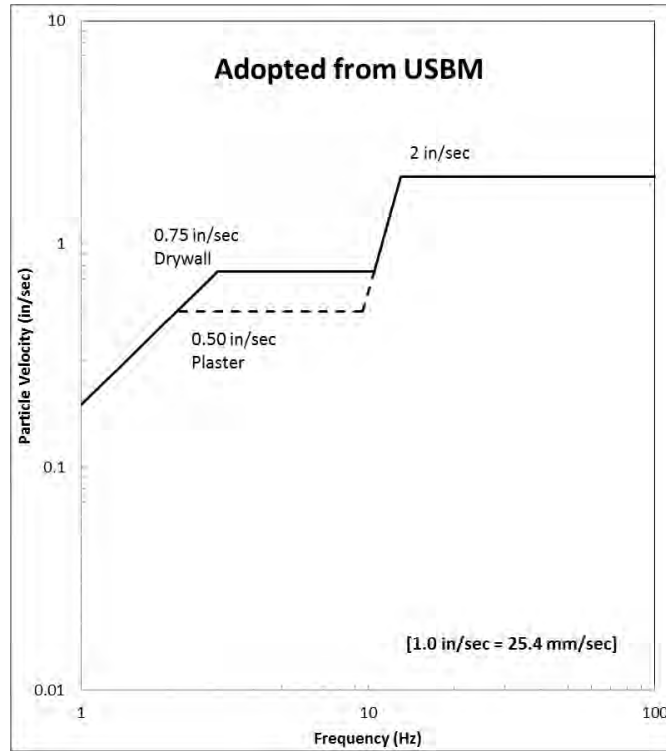
Vibrations generated from construction activity can cause several concerns at adjacent structures that range from annoyance to structural damage. Several studies have been conducted to determine the relationship between vibration levels, human perception, and structural damage. Table 2 contains a summary of a study reported by Hendriks (2002) for continuous vibrations. The study concluded that vibration levels that are large enough to “annoy people” are at threshold levels for architectural damage to structures that contain plaster walls or ceilings. Since these levels are below levels of even minor structural damage, the perception of building occupants can sometimes lead to discrepancies in the effects of vibrations. The values listed in Table 2 are generally conservative when compared to pile driving vibrations since they were developed for continuous vibrations. Pile driving operations develop discontinuous vibrations that can reduce the damage potential (Hendriks 2002).

**Table 2:** Continuous vibration levels and effects (Hendriks 2002)

<b>Vibration Level (Peak Particle Velocity)</b>	<b>Human Reaction</b>	<b>Building Effects</b>
0.006-0.019 in/sec	Threshold of perception;	Vibrations unlikely to cause damage
0.08 in/sec	Vibration readily perceptible	Recommended upper level for ruins and ancient monuments
0.1 in/sec	Continuous vibrations begin to annoy people	Virtually no risk of “architectural” damage to normal buildings
0.2 in/sec	Vibrations annoying to people in buildings	Threshold at which there is a risk of “architectural” damage to normal dwelling- houses with plaster wall and ceilings
0.4-0.6 in/sec	Vibrations considered unpleasant by people subjected to continuous vibrations	Vibrations at a greater level than normally expected from traffic, but would cause “architectural” damage and possible minor structural damage

In addition to the many studies to determine the effect of vibrations on structures, several State and Federal Agencies, as well as, International Organizations have developed guidelines on permissible vibration levels due to construction activity. Much of the early work related to vibrations was performed by the United States Bureau of Mines (USBM) in the 1970’s and 80’s (Siskind, et al. 1980). This research focused on vibrations from blasting operations. Figure 2 shows the recommended vibration limits for blasting as a function of frequency. The limits range from 0.2 to 2.0 inches per second (in/sec).





**Figure 2:** Vibration limits from the USBM (Siskind, et al. 1980)

The American Association of State Highway and Transportation Officials (AASHTO) and the Federal Transit Administration (FTA) have developed guidelines for vibration limits that range from 0.1 to 1.5 in/sec depending on the structure type as shown in Table 3.

**Table 3:** AASHTO and FTA criteria for construction vibrations

<b>Organization/Jurisdiction</b>	<b>Comments</b>	<b>PPV (in/sec)</b>
American Association of State Highway and Transportation Officials (AASHTO 1990)	Residential buildings, plastered walls	0.2-0.3
	Residential buildings in good repair with gypsum board walls	0.4-0.5
	Engineered structures, without plaster	1.0-1.5
	Historic sites or other critical locations	0.1
Federal Transit Administration (FTA 2006)	Reinforced-concrete, steel or timber	0.5
	Engineered concrete and masonry	0.3
	Non-engineered timber and masonry	0.2
	Buildings extremely susceptible to vibration damage	0.12

The vibration criteria developed by the various states also have a wide range of values as shown in Table 4. If the table is carefully analyzed, the vibration limits can be divided into several categories including: modern structures, sensitive structures, and miscellaneous structures. The range of vibration limits for modern structures is from 0.4 to 1.0 in/sec and sensitive structures have a range of 0.08 to 0.2 in/sec. These vibration limits correlate well to the AASHTO and FTA limits. A thorough review of construction vibration limits can be found in several reports including: (Tao and Zhang 2012), (Wilson Ihrig & Associates 2012), and (Cleary 2013).

**Table 4:** State criteria for construction vibrations

<b>Organization/Jurisdiction</b>	<b>Comments</b>	<b>PPV (in/sec)</b>
California Department of Transportation (Caltrans 2002)	Upper level for possible damage	0.4-0.6
	Threshold for damage to plaster	0.20
	Ruins and ancient monuments	0.08
Florida DOT (FDOT 2010)	All construction	0.5
	Fresh concrete	1.5
Iowa DOT (Iowa DOT n.d.)	Project specific specification	0.2
Louisiana Department of Transportation and Development (Tao and Zhang 2012)	General scenario	
	- New requirements	0.5
	- Old requirements	0.2
	Historic structures or loose sandy soil	0.1
New Hampshire DOT (NHDOT 2010)	Modern Homes	0.75
	Older Homes	0.50
New York City DOT (New York City DOT 2009)	Piles driven adjacent to subway structures (may be lowered)	0.5
Rhode Island DOT (RIDOT 2010)	Lower limits may be applied by engineer	1.0

### Dynamic Settlement

In addition to structural damage and human perception, dynamic settlement can occur due to construction vibrations. Research has shown that if loose cohesionless soils (loose sands) are present, relatively low vibration levels can cause densification (Dowding 1996). This densification can lead to settlement related damage in adjacent structures. Loose sands are typically defined as having a relative density less than 40% (Tao and Zhang 2012). Dynamic settlement has occurred in some soils at vibration levels as low as 0.1 in/sec. If loose sands are located on or near a project site, then special considerations for construction vibrations need to be considered.

## Vibration Prediction

Since it is typically unrealistic for most construction projects to conduct full scale testing to determine the expected levels of vibrations and since only a discrete number of locations are measured during testing, several methods have been developed to predict vibration levels. The first prediction equations were developed as early as 1912 by Golitsin who developed a simple equation to predict the peak particle displacement of ground vibrations from earthquakes. The equation, as reported by (Bayraktar, et al. 2013) is as follows,

$$A_2 = A_1 \sqrt{r_1/r_2} e^{-\gamma(r_2-r_1)}, \quad (1)$$

where  $A_1$  is the peak particle displacement of ground vibrations at a distance  $r_1$  from the source,  $A_2$  is the peak particle displacement of ground vibrations at a distance  $r_2$  from the source, and  $\gamma$  is a vibration attenuation coefficient.

More recently, several methods have been developed to predict the peak particle velocity (PPV) from construction activity, pile driving in particular. Hendriks (2002) reported several equations to predict the propagation of construction vibrations. The first equation presented by Hendriks was first reported by Richart, et.al. (1970), who cited Bornitz (1931),

$$V = V_o(D_o/D)^{0.5} e^{\alpha(D_o-D)} \quad (2)$$

where  $V$  is the peak particle velocity at distance  $D$ ,  $V_o$  is the peak particle velocity at reference distance  $D_o$ , and  $\alpha$  is a vibration attenuation parameter that must be determined experimentally.

Hendriks (2002) also reported a simplified equation for pile driving vibrations that is similar to an equation reported by Woods (1997) as follows,

$$V = V_o(D_o/D)^k \quad (3)$$

where  $V$  is the peak particle velocity at distance  $D$ ,  $V_o$  is the peak particle velocity at reference distance  $D_o$ , and  $k$  is a vibration attenuation parameter that must be determined experimentally.

Several researchers have found that a better correlation with predicted and measured vibrations could be determined by including the energy of the pile driving hammer in the equation. This approach is often referred to as the “scaled-distance” approach. One commonly used equation was developed by Wiss and reported by Bayraktar, et al. (2013),

$$v = k[D/\sqrt{W_t}]^{-n} \quad (4)$$

where  $W_t$  is the energy of the source,  $v$  is the peak particle velocity at distance  $D$ ,  $k$  is the intercept value of the peak particle velocity at a scaled distance of  $D/(W_t)^{1/2}$  equal to one, and  $n$  is a vibration attenuation parameter that must be determined experimentally.

The previous equations are relatively accurate at predicting ground vibrations when compared to experimental data, however, they all require testing to determine the soil parameters. Jones & Stokes (2004) performed an extensive literature review and determined that the following equation, with the assumed values shown, could be used to predict pile driving vibrations without experimental evaluations:

$$PPV_{Impact\ Pile\ Driver} = PPV_{Ref}(25/D)^n(E_{equip}/E_{ref})^{0.5} \quad (5)$$

where  $PPV_{Impact\ Pile\ Driver}$  is the peak particle velocity at distance D in feet,  $PPV_{Ref}$  is equal to 0.65 in/sec for a reference pile driver at 25 feet,  $E_{ref}$  is equal to 36,000 ft-lb (rated energy of reference pile driver),  $E_{equip}$  is the rated energy of impact pile driver in foot-pounds, and n is a vibration attenuation parameter with a recommended value of 1.1.

Jones and Stokes also provided a table, Table 5, with suggested “n” values based on the soil type.

**Table 5:** Suggested “n” values based on soil class: Adopted from (Jones & Stokes 2004)

<b>Soil Class</b>	<b>Description of Soil</b>	<b>Suggested Value of “n”</b>
I	Weak or soft soils: loose soils, dry or partially saturated peat and muck, mud, loose beach sand, and dune sand, recently plowed ground, soft spongy forest or jungle floor, organic soils, top soil. (shovel penetrates easily)	1.4
II	Competent soils: most sands, sandy clays, silty clays, gravel, silts, weathered rock. (can dig with shovel)	1.3
III	Hard soils: dense compacted sand, dry consolidated clay, consolidated glacial till, some exposed rock. (cannot dig with shovel, need pick to break up)	1.1
IV	Hard, competent rock: bedrock, freshly exposed hard rock. (difficult to break with hammer)	1.0

## EXPERIMENTAL DESIGN

### Overview

The main objective of this research was to determine the distance from nearby structures that pile driving operations can be conducted with minimal risk to those structures. It is important to note that these guidelines were developed for typical piles used by ALDOT at the project site. The project was divided into two phases, collecting data during pile driving and analyzing the data. The information related to the project site, the test piles, the pile driving equipment, and the data collection equipment is located below.

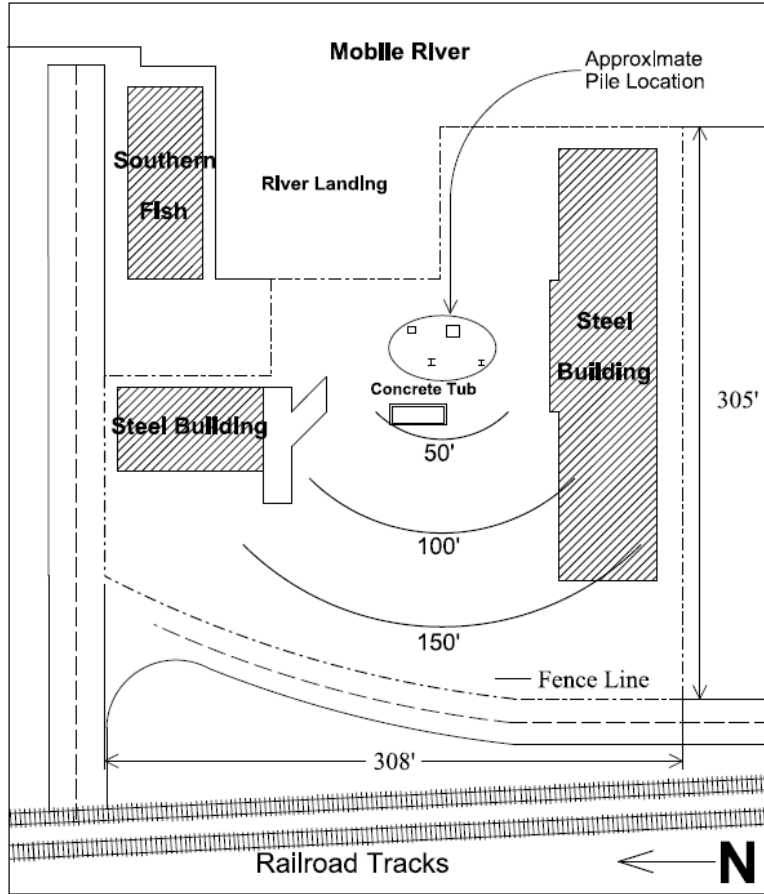
### Project Site

The project site is located on the west bank of the Mobile River, just south of the Alabama Cruise Terminal. The soil profile at the site consists primarily of sandy soils to a depth of 90 feet below the ground surface with a clay layer located at an approximate depth of 90 to 110 feet. Table 6 contains a summary of the soil layers that were defined by a standard penetration test (SPT) conducted at the project site. Appendix A contains the details of the soil investigations conducted by an ALDOT drill crew and Southern Earth Sciences.

**Table 6:** Soil profile at site location

<b>Depth (ft.)</b>	<b>Basic Material</b>	<b>Average Blow Count</b>	<b>Consistency</b>
0-23.5	Sand	12	Loose to Medium
23.5-89.5	Sand	31	Medium to Dense
89.5-108.5	Clay	28	Stiff to Very Stiff
108.5-115	Sand	27	Medium

Figure 3 contains a plan view of the project site. The dashed line in the figure represents the approximate property boundary. Note that the pile locations are approximate and the drawing is not to scale. The arc lines shown in the drawing represent the approximate distance from the piles to where the monitoring equipment was located.



**Figure 3:** Plan view of Mobile River Bridge Project Site

Four test piles were driven for this project, two prestressed precast concrete piles (PPC) and two steel H-Piles. Table 7 contains descriptions of the piles and Appendix B contains the details of the two pile driving hammers utilized on this project. The piles were installed using typical techniques including pile jetting or vibration followed by driving with a diesel hammer. The concrete piles were jetted to a depth of approximately 30 feet and driven to the final elevation using a Delmag Model D-62-22 diesel hammer. A vibratory driver was used to drive the steel HP 14 to 55 feet and the HP 12 to 15 feet. The steel piles were then driven to the final elevation using an APE Model D30-42 diesel hammer.

**Table 7:** Pile descriptions

<b>Pile</b>	<b>Cross Section</b>	<b>Material</b>	<b>Length</b>
#1	24" Square	Precast Concrete	81 ft
#2	36" Square	Precast Concrete	89 ft
#3	HP14x117	Steel	106 ft
#4	HP12x53	Steel	70 ft

## **Vibration Monitoring**

Data collectors were placed at various locations throughout the pile installation and testing process. The data collectors utilized for this project were Minimate Plus tri-axial geophones manufactured by Instatel. Each tri-axial geophone unit contains three geophones oriented on three mutually perpendicular axes. The units come with software allowing data collection and analysis in several configurations. For this research, the units were configured to collect histogram data during two-second intervals. When configured in this way the data collector measures all vibrations over the interval, but only records the maximum PPV and the frequency that it occurred at for each geophone over the two second interval.

The geophones were placed at predetermined distances from each pile during installation. Three of the data collectors were located at approximately 50, 100, and 150 feet. A fourth data collector, which had two geophone units attached to it, was located at various distances throughout testing to collect additional information. Table 8 contains a detailed account of the location of each data collector during testing.

During the initial driving of the 36-inch PPC pile, geophone number three was located at the edge of the project site near Southern Fish and Oyster, an adjacent property owner. The fourth data collector had one geophone unit placed at 100 feet from the pile and the other geophone unit was attached to the brick façade of a building that was located on the project site. Please note that the 30-day restrike was at 32-days for the 36-inch concrete pile and 31-days for the 24-inch concrete pile.



**Table 8:** Geophone location during testing

<b>Initial Drive</b>	<b>Pile Type</b>	<b>Geophone Unit</b>					<b>#4b</b>
		<b>#1</b>	<b>#2</b>	<b>#3</b>	<b>#4a</b>		
Aug. 19, 2013	36" PCP	50 ft	150 ft	69 ft	100 ft	Building	
Aug. 20, 2013	24" PCP	99.5 ft	142 ft	n/a	n/a	n/a	
Aug. 21, 2013	HP 12	53 ft	101 ft	144 ft	n/a	n/a	
Aug. 21, 2013	HP 14	58 ft	106 ft	146 ft	n/a	n/a	
<b>24 Hour Restrike</b>							
Aug. 22, 2013	HP 12	50 ft	150 ft	100 ft	n/a	n/a	
Aug. 22, 2013	HP 14	50 ft	150 ft	100 ft	n/a	n/a	
<b>3-Day Restrike</b>							
Aug. 22, 2013	36" PCP	50 ft	n/a	100 ft	n/a	n/a	
Aug. 23, 2013	24" PCP	50 ft	150 ft	100 ft	n/a	n/a	
<b>7-Day Restrike</b>							
Aug. 26, 2013	36" PCP	50 ft	150 ft	100 ft	75 ft	125 ft	
Aug. 27, 2013	24" PCP	50 ft	150 ft	100 ft	75 ft	125 ft	
<b>30-Day Restrike</b>							
Sept. 20, 2013	36" PCP	50 ft	150 ft	100 ft	n/a	n/a	
Sept. 20, 2013	24" PCP	55 ft	155 ft	105 ft	n/a	n/a	
Sept. 20, 2013	HP 12	50 ft	150 ft	100 ft	n/a	n/a	
Sept. 20, 2013	HP 14	50 ft	150 ft	100 ft	n/a	n/a	

## RESULTS

### Vibration Levels

Vibrations were monitored during installation and restrikes on the 36-inch concrete pile at three, seven, and thirty days. A communication error occurred between the ALDOT personnel, the pile driving contractor, and the research team during the installation of the 24-inch concrete pile which resulted in the start of driving prior to the installation of the vibration monitors. Due to this error, the 24-inch concrete pile only had vibrations monitored during the final stage of driving and at all restrikes. The steel piles were monitored during installation and during the one day and thirty day restrikes.

Baseline vibration data was collected at the project site by monitoring vibration levels due to railroad activity from a pair of railroad tracks located adjacent to the project site, Figure 3. The approximate distance from the tracks to the data collectors was determined and the vibration levels from train activity were evaluated. Due to the relatively low vibration levels recorded during train activity, baseline data was not collected for truck traffic.

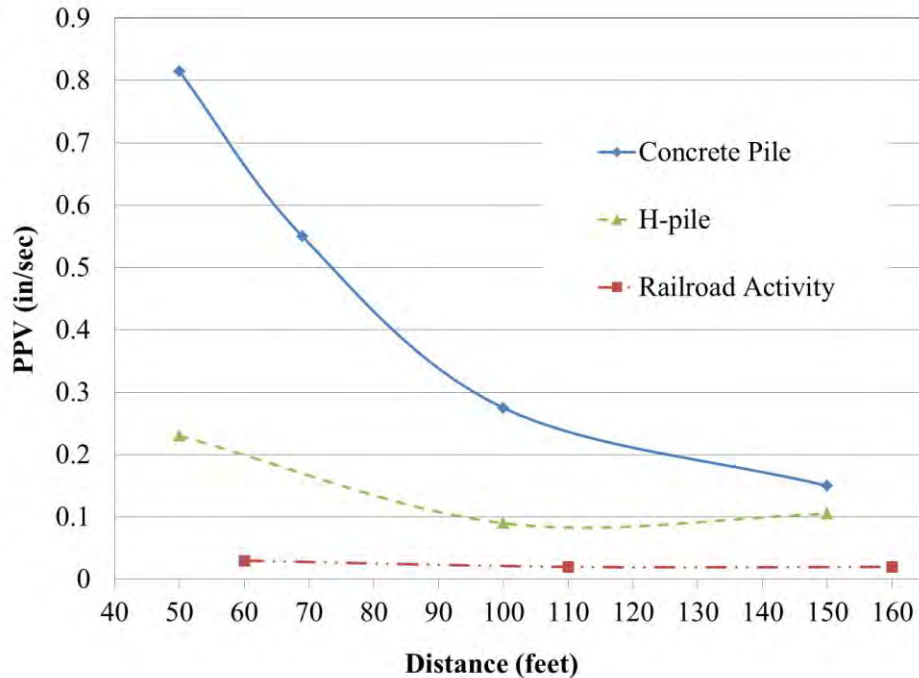
The vibration data collected from the project site was analyzed and the peak particle velocity (PPV) from each pile was recorded. Table 9 contains a summary of the results. The largest recorded vibration during this study occurred while driving the 36-inch concrete pile and resulted in a PPV of 0.82 inches per second at a distance of 50 feet.

**Table 9:** Maximum PPV (in/sec) during pile driving operations

Vibration Source	Horizontal Distance from Pile		
	50 feet	100 feet	150 feet
36" Concrete Pile	0.82	0.28	0.15
HP14x117	0.18	0.09	0.11
HP12x53	0.23	0.07	0.08
Railroad Activity	0.03 <sup>1</sup>	0.02 <sup>1</sup>	0.02 <sup>1</sup>

<sup>1</sup>The approximate distances were 60, 110, and 160 feet

Figure 4 shows the maximum PPV for the 36-inch concrete pile, the H-Piles, and railroad activity observed during testing. Since the maximum vibrations occurred during the beginning of the driving process, the 24-inch concrete pile was not included in this figure. The figure confirms that the largest vibrations recorded were associated with the installation of the 36-inch concrete pile.



**Figure 4:** Maximum recorded vibration levels during pile installation

During the driving of the 36-inch concrete pile, one of the geophones was attached to the brick façade of a building that was located on the project site. The building was located to the south of the piles, Figure 3, and was approximately 90 feet from the 36-inch concrete pile. The brick façade was located on the west end of the building and was approximately 140 feet from the pile. The data from this geophone was analyzed and it was determined that the vibration levels were below the threshold for detection, 0.005 in/sec. This indicates that the ground vibrations did not have enough energy to cause vibrations in the building. Additionally, crack width monitors were installed on the outside wall of the building. The crack widths and lengths were monitored throughout the project and it was determined that there were no changes in any of the cracks.

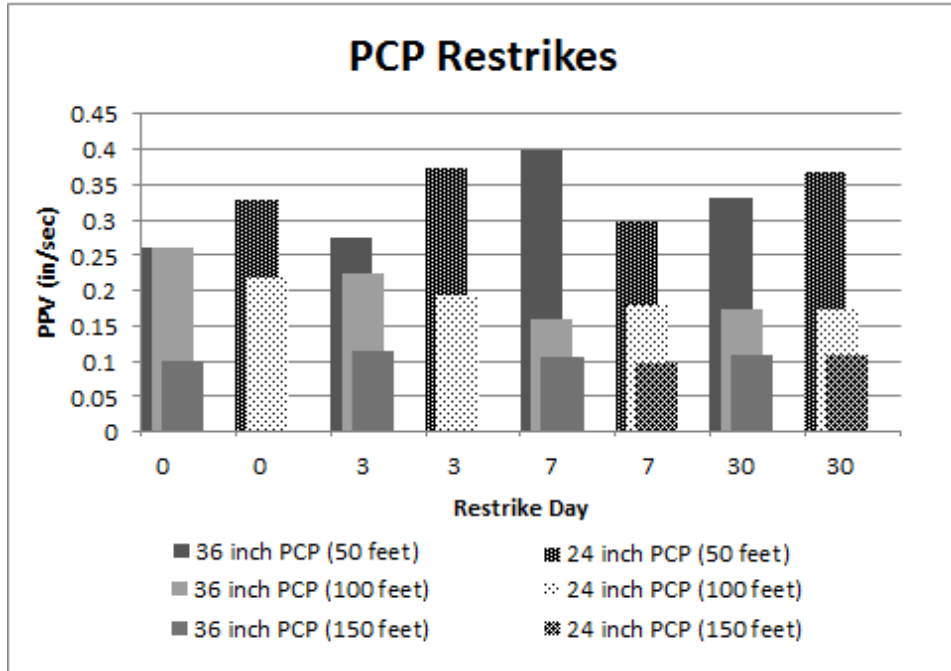


Figure 5: Bar chart of restrikes on precast concrete piles (PCP)

An analysis was performed to compare the vibrations between the 24- and 36-inch concrete piles since data was not collected throughout the driving of the 24-inch pile. Figure 5 shows a bar chart of the vibration levels for each of the concrete piles during the restrikes, note that day zero is at the end of drive. Figure 6 shows the same data in the form of a data plot. The data indicates that the vibration levels for the 24- and 36-inch concrete piles are similar and that the maximum vibrations, near the start of driving, would be expected to be approximately equal for each concrete pile.

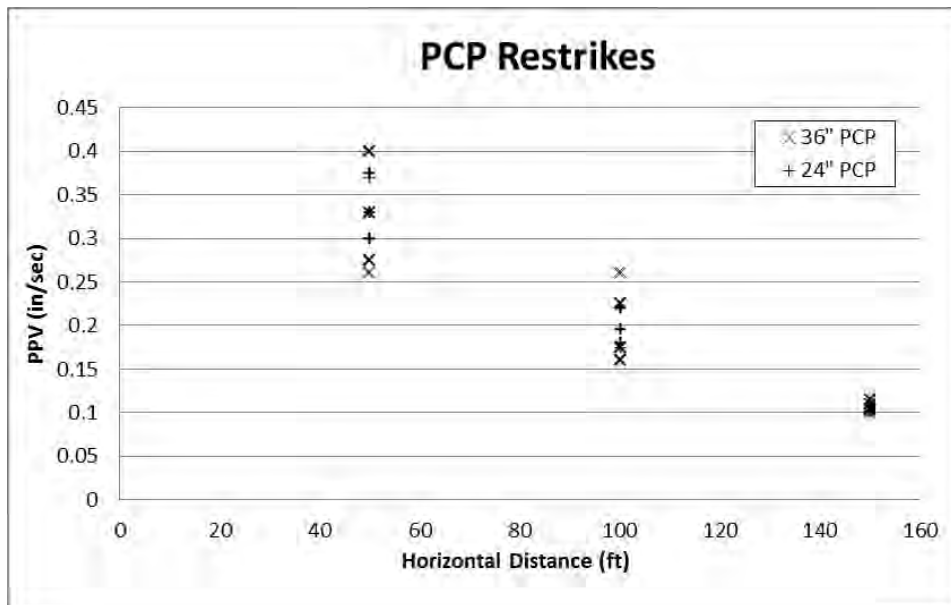


Figure 6: Data plot of restrikes on precast concrete piles (PCP)

## Prediction Equation

The second major objective of this project was to develop a methodology to predict the vibration level at various distances from the pile location. Since the primary use of this research is for determining the vibration levels for piles typically used by ALDOT located at or near the project site, two prediction equations were developed. The equations are based on the maximum peak particle velocities while driving the 36-inch concrete pile and the H-piles. Both equations are based on Equation 3, as presented by Hendriks (2002), where the vibration attenuation parameter ( $k$ ) was determined with the experimental data. Equation 6 was developed to predict vibrations for 36 inch concrete pile,

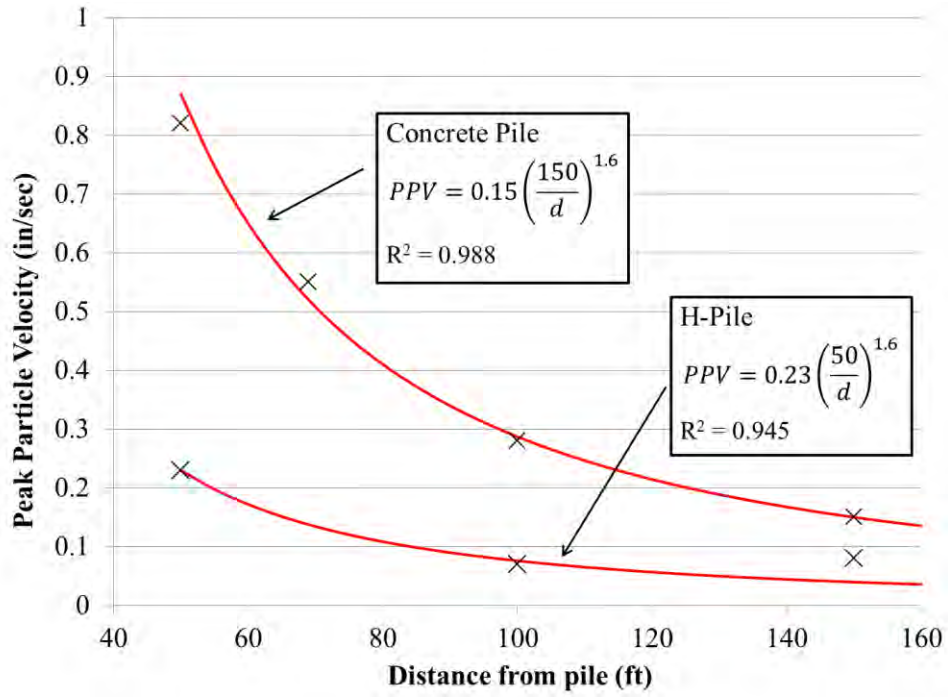
$$PPV = 0.15 \left( \frac{150}{d} \right)^{1.6}, \quad (6)$$

and Equation 7 was developed to predict vibrations for the H-piles,

$$PPV = 0.23 \left( \frac{50}{d} \right)^{1.6}, \quad (7)$$

where, in both equations,  $PPV$  is the peak particle velocity at distance ( $d$ ) in inches per second and  $d$  is the distance from the pile in feet.

Figure 7 shows a plot of the experimental data and the peak particle velocities based on the prediction equation. The results indicate that the prediction equation model fit the experimental data well. However, due to the unusual increase in vibration magnitude at 150 feet for the H-piles, the prediction equation under-predicts the vibration magnitude at 150 feet. It was also noted that the soil attenuation parameter ( $k$ ) for both equations was determined to be 1.6. This was expected since the parameter is primarily dependent on the soil properties and less dependent on the pile type or hammer energy.



**Figure 7:** Peak particle velocity versus distance

## CONCLUSIONS

The experimental data shows that the largest vibrations occurred during the installation of the 36-inch concrete pile, which was recorded as 0.82 inches per second. According to the research presented in Table 2 (Hendriks 2002), a vibration level of 0.82 inches per second has the potential to cause structural damage to an adjacent structure. However, this vibration was recorded at a distance of 50 feet from the pile; the vibration level at 100 feet from the pile was reduced to 0.275 inches per second. This vibration level could cause potential architectural damage to buildings constructed with plaster, but would not likely cause structural damage. At 150 feet the vibration levels were reduced to 0.15 inches per second, a level that would have little to no risk of damage to adjacent structures.

Based on the experimental data and a thorough review of the literature, it is recommend that a maximum vibration level of 0.5 inches per second for modern structures and 0.1 inches per second for potentially sensitive structures be allowed for construction activity at or near the location of the project site. These vibration levels are the allowable levels at the location of the structure. To determine if any structures should be surveyed and monitored for potential vibration damage, a survey distance of 150 feet for modern structures and 250 feet for potentially sensitive structures should be established. The monitoring distances should be measured from the source of the vibration. The ground vibration prediction equation that was developed would estimate a peak particle velocity of 0.15 inches per second at 150 feet and 0.07 inches per second at 250 feet. The survey distances are well beyond the distance where the prediction equation would estimate vibration levels of 0.5 and 0.1 inches per second and therefore would represent conservative survey distances to ensure adjacent structures are not damaged.

### **Recommendations for Future Research**

The research presented in this report contains detailed analysis for a particular location in the state of Alabama; however, data has not been collected and analyzed for other regions of the state with differing soil conditions. A state wide research project should be initiated to determine vibration propagation and attenuation criteria for soil conditions located throughout the state. This data could be used to develop prediction equations that could be used in project planning. Additionally, the results of this research could be used to develop model vibration specifications for the state of Alabama.

In addition to the research mentioned above, it is recommended that a vibration monitoring program be developed for any large scale construction projects in urban environments. These programs could be used not only to ensure the construction activity is not damaging nearby structures, but to ensure the public that the DOT is proactive in preventing damage.



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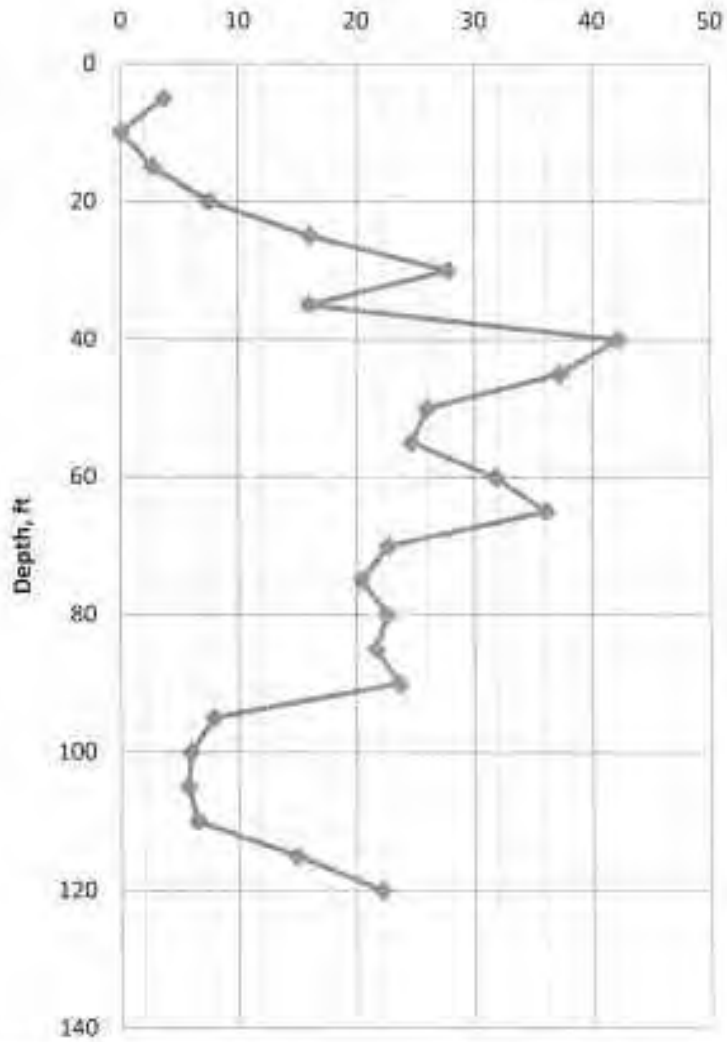
## **Appendix A: Soil Reports**

Two soil investigations were performed at the site. The first was a Standard Penetration Test (SPT), which was performed at two locations. The first location, labeled B-1 in the documents that follow, was located at a property owned by ALDOT that is several hundred feet to the west of the project site. This location was an alternate location for testing. The second location, labeled B-2, was at the project site in the vicinity of where the test piles were installed. The SPT test was performed by an ALDOT drill crew.

The second soil investigation performed was a Seismic Cone Penetration Test (SCPT). Two locations were also investigated, both on the project site. The first test was performed at the location of the test piles and the second was located at 100 to 120 feet from the test piles. The results of both investigations are included here. The SCPT was conducted by Southern Earth Sciences.

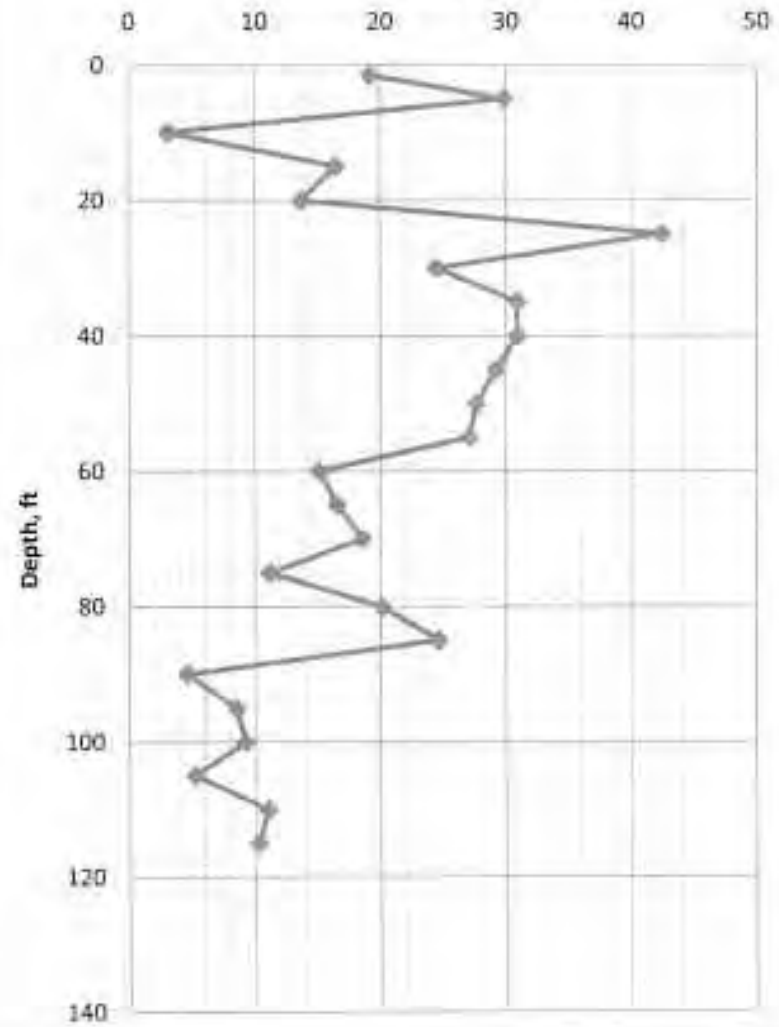
### B-1

#### $N_{160}$ Corrected Blow Count



### B-2

#### $N_{160}$ Corrected Blow Count



Project# DPI-0030 (005) Division 9<sup>th</sup>

Station \_\_\_\_\_ Offset \_\_\_\_\_ Ft \_\_\_\_\_

Ground Elev. \_\_\_\_\_ Water Elev. in Hole \_\_\_\_\_

Depth of Strata BOR# B-1 Visual BOR Loc. \_\_\_\_\_

From	To	Consistency or Density	Approx. Moisture	Color	Basic Matl.
0.0	0.3	Asphalt			
0.3	18.5	U. loose	Moist	Br	br sand
18.5	23.5	Loose	Moist	Br	SAND
23.5	38.5	Med	Moist	TAN	SAND
38.5	63.5	Dense	Moist	TAN	sand
63.5	68.5	Very Dense	Moist	TAN	Sand
68.5	93.5	Dense	Moist	Tan	Sand
93.5	108.5	stiff	Moist	Gray	CLAY
108.5	118.5	HARD	Moist	Gray	Clay
118.5	120.0	DENSE	Moist	Gray	SAND

Remarks by Driller Installed well monitor  
 GPS Cord. 42' 1"  
 LAT. Water -10' 3" 24Hrs  
 LONG. \_\_\_\_\_

County Mobile Date 8-2-12

C/L Driller Young/Evans

Type Drill Used SE 9050 Total Hole Depth 120.0

Identification CME 5504 2.25 Hollow Stem

Other Pertinent Components	Sample No.	Penetration or Sample Elev.		N Blow			N Value
		From	To	5	1.0	1.5	
	* 1-A	3.5	5.0	1	1	1	2
W/ clay	* 1-B	8.5	10.0	W	0	H	UOH
	* 1-C	13.5	15.0	W	1	1	2
	1-D	18.5	20.0	1	2	4	6
	1-E	23.5	25.0	5	5	9	14
	1-F	28.5	30.0	10	12	14	26
	1-G	33.5	35.0	9	7	9	16
W/Sand	* 1-H	38.5	40.0	26	23	22	45
W/Sand	* 1-I	43.5	45.5	23	23	19	42
large moll	1-J	48.5	50.0	11	14	11	21
	1-K	53.5	55.0	9	16	15	31
	1-L	58.5	60.0	18	20	22	42
	* 1-M	63.5	65.0	20	23	27	50
	* 1-N	68.5	70.0	14	16	17	33
	1-O	73.5	75.0	7	15	16	31

\* Sample 11 JMS

Project# DPT-0030 (005) Division 9th

Station \_\_\_\_\_ Offset \_\_\_\_\_ Ft \_\_\_\_\_

Ground Elev. \_\_\_\_\_ Water Elev. in Hole \_\_\_\_\_

Depth of Strata BOR# B-1 Visual BOR Loc. \_\_\_\_\_

From	To	Consistency or Density	Approx. Moisture	Color	Basic Matl.

Remarks by Driller \_\_\_\_\_

GPS Cord. \_\_\_\_\_

LAT. \_\_\_\_\_

LONG. \_\_\_\_\_

County Mobile Date 8-6-12

CL Driller Young/Evans

Type Drill Used SE 9050 Total Hole Depth 120.0

Identification CME 550X 2.25 1/16 in

Other Pertinent Components	Sample No.	Penetration or Sample Elev.		"N" Blow			"N" Value
		From	To	5	1.0	1.5	
	1-P	78.5	80.0	12	17	19	36
	1-Q	83.5	85.0	12	18	18	36
	1-R	88.5	90.0	17	22	19	41
	* 1-S	93.5	95.0	3	6	8	14
	1-T	98.5	100.0	5	5	6	11
	1-U	103.5	105.0	4	5	6	11
	* 1-V	108.5	110.0	3	6	7	13
	* 1-W	113.5	115.0	6	15	16	31
	* 1-X	118.5	120.0	13	21	27	48

\* 1-S HAS 4 JAR samples  
at C/A

Project# DP1-0030 (005) Division 9+2

Station \_\_\_\_\_ Offset \_\_\_\_\_ Ft \_\_\_\_\_

Ground Elev. \_\_\_\_\_ Water Elev. in Hole \_\_\_\_\_

Depth of Strata BOR# B-2 Visual BOR Loc. \_\_\_\_\_

From	To	Consistency or Density	Approx. Moisture	Color	Basic Mat.
0.0	0.2	Topsoil	—		
0.2	3.5	Loose	Moist	Br	Sand
3.5	8.5	Med	Moist	Br	Sand
8.5	13.5	Loose	Moist	Br	Sand
13.5	23.5	Med	Moist	Gray	Sand
23.5	28.5	Dense	Moist	Tan	Sand
28.5	33.5	Med	Moist	Tan	Sand
33.5	58.5	Dense	Moist	Tan	Sand
58.5	78.5	Med	Moist	Tan	Sand
78.5	89.5	Dense	Moist	Tan	Sand
89.5	93.5	Stiff	Moist	Gray	Clay
93.5	103.5	Stiff	Moist	Gray	Clay
103.5	108.5	Stiff	Moist	Gray	Clay
108.5	115.0	Med	Moist	Gray	Sand

Remarks by Driller \_\_\_\_\_

GPS Cord. \_\_\_\_\_

LAT. \_\_\_\_\_

LONG. \_\_\_\_\_

County Mobile Date 8-9-12

C/L Driller Turner/Evan

Type Drill Used SE 9050 Total Hole Depth 115'

Identification CME 550X 2.25 Hollow Stem

Other Pertinent Components	Sample No.	Penetration or Sample Elev.		'N' Blow			'N' Value
		From	To	5	1.0	1.5	
	*2-A	0.0	1.5	9	4	4	8
w/Gravel	*2-B	3.5	5.0	5	7	9	16
w/ORG MAT	*2-C	8.5	10.0	1	1	1	2
	*2-D	13.5	15.0	2	5	7	12
	2-E	18.5	20.0	5	4	7	11
	*2-F	23.5	25.0	16	19	19	37
	2-G	28.5	30.0	10	11	12	23
	*2-H	33.5	35.0	7	15	16	31
	2-I	38.5	40.0	7	13	20	33
	2-J	43.5	45.0	9	14	19	33
w/Sand	2-K	48.5	50.0	7	15	18	33
ORG MAT	2-L	53.5	55.0	10	16	18	34
	*2-M	58.5	60.0	10	10	10	20
	2-N	63.5	65.0	6	12	11	23
	2-O	68.5	70.0	12	17	10	27

\* JAC Sample



Project# \_\_\_\_\_ Division \_\_\_\_\_

Station \_\_\_\_\_ Offset \_\_\_\_\_ Ft \_\_\_\_\_

Ground Elev. \_\_\_\_\_ Water Elev. in Hole \_\_\_\_\_

Depth of Strata BOR# B-2 Visual BOR Loc. \_\_\_\_\_

From	To	Consistency or Density	Approx. Moisture	Color	Basic Matl.

Remarks by Driller \_\_\_\_\_

GPS Cord. \_\_\_\_\_

LAT. \_\_\_\_\_

LONG. \_\_\_\_\_

County \_\_\_\_\_ Date 8-9-12

C/L Driller \_\_\_\_\_

Type Drill Used \_\_\_\_\_ Total Hole Depth 115'

Identification

Other Pertinent Components	Sample No.	Penetration or Sample Elev.		'N' Blow			'N' Value
		From	To	5	10	15	
	2-1	73.5	7.5	6	7	10	17
	* 2-0	78.5	8.0	6	11	21	32
	2-R	83.5	8.50	12	18	23	41
	* 2-6	88.5	9.0	4	4	4	8
	* 2-7	93.5	9.5	6	7	8	15
	* 2-U	98.5	10.0	1	9	8	17
	* 2-V	103.5	10.5	7	4	6	10
	* 2-W	108.5	11.0	5	9	13	22
	2-X	113.5	11.5	6	9	13	22

2-7 has 3 jars

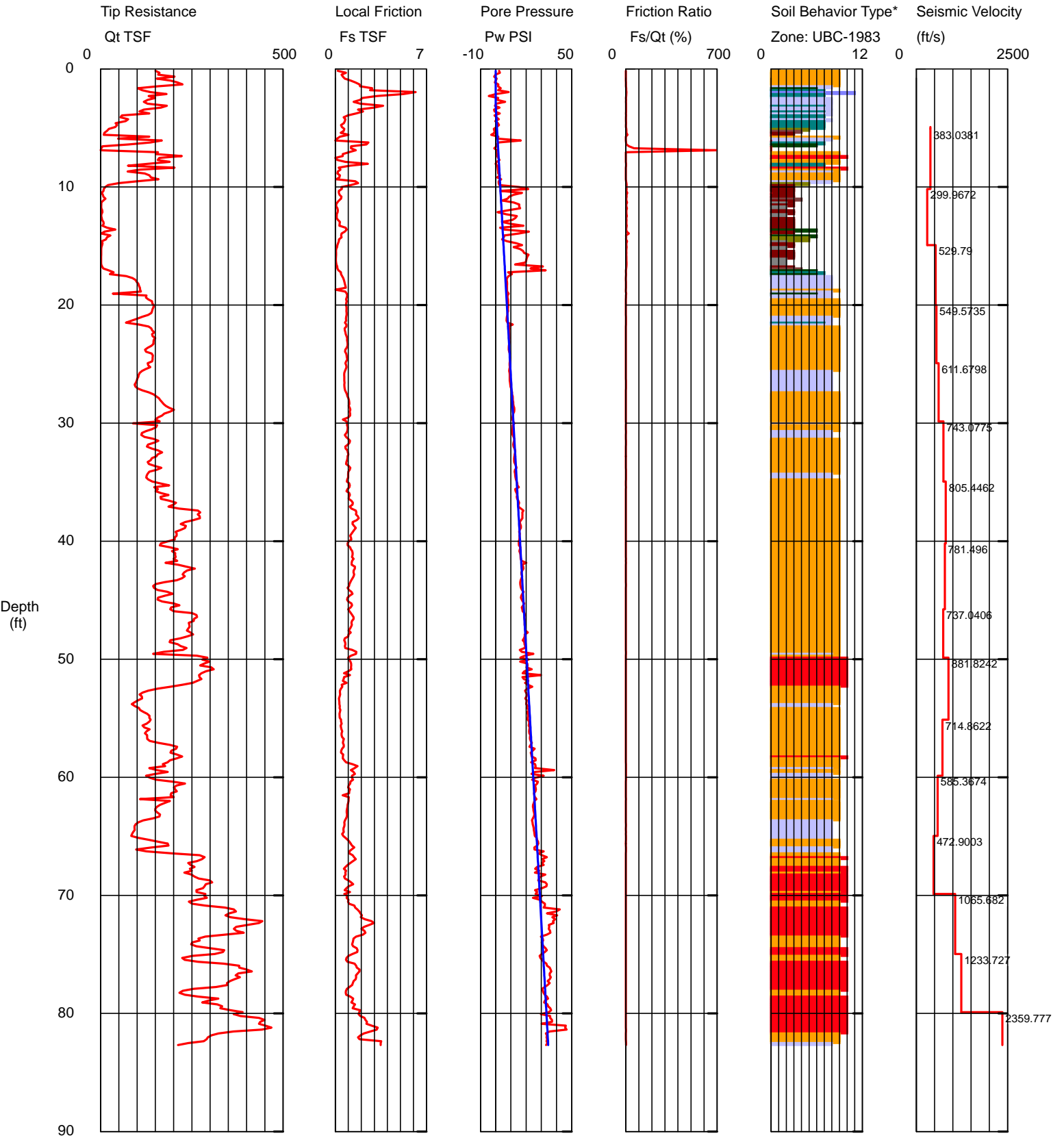
2-U has 2 jars

2-V has 3 jars

# Southern Earth Sciences

Operator: Mike Wright  
 Sounding: SCPT-1  
 Cone Used: DDG0892

CPT Date/Time: 8/14/2013 9:08:56 AM  
 Location: Test Pile Evaluation  
 Job Number: 13-000



Maximum Depth = 82.68 feet

Depth Increment = 0.164 feet

- 1 sensitive fine grained
  - 2 organic material
  - 3 clay
- Groundwater measured at 3.1'

- 4 silty clay to clay
- 5 clayey silt to silty clay
- 6 sandy silt to clayey silt

- 7 silty sand to sandy silt
- 8 sand to silty sand
- 9 sand

- 10 gravelly sand to sand
- 11 very stiff fine grained (\*)
- 12 sand to clayey sand (\*)

N30.68546 W88.03791

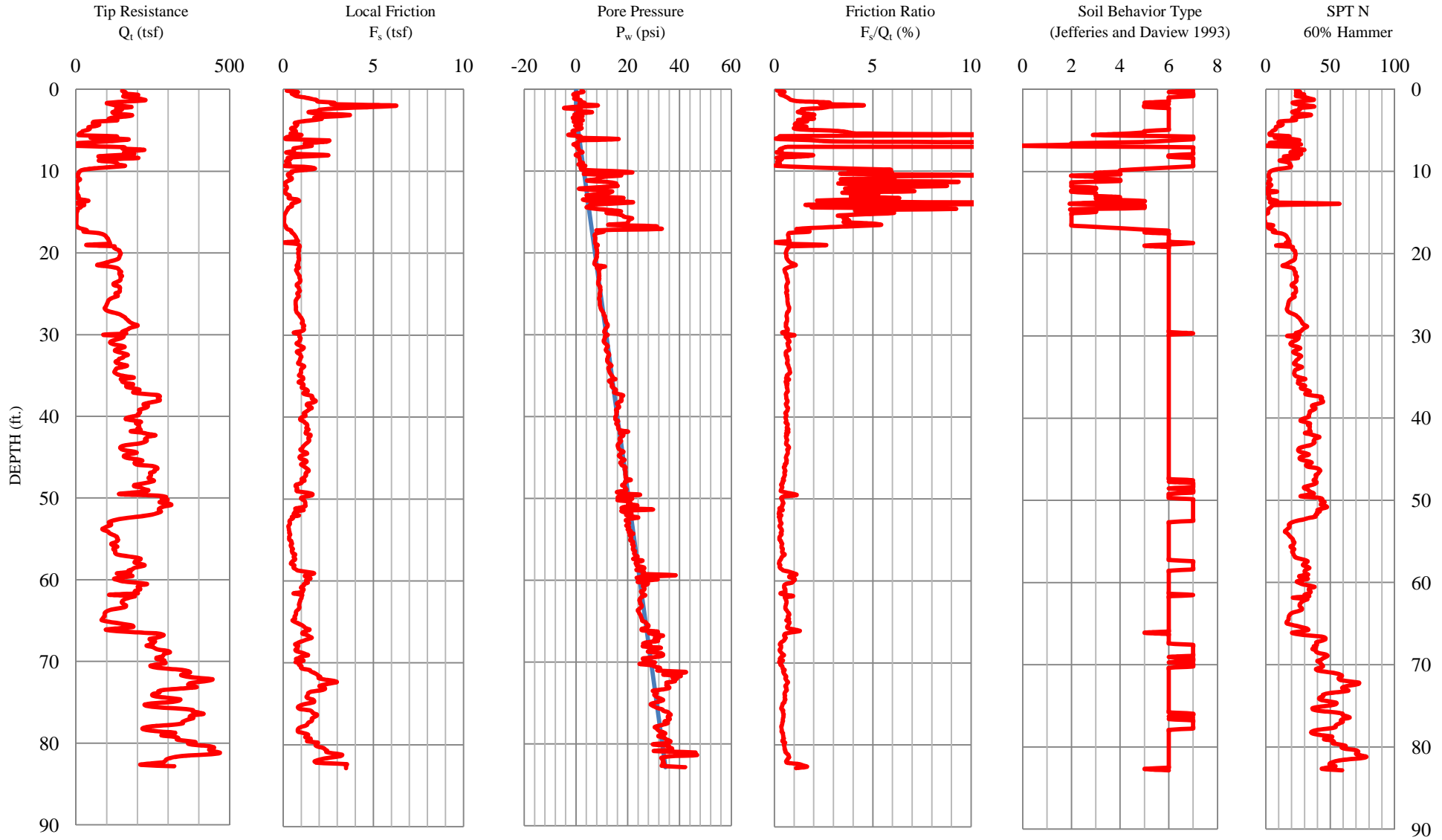
\*Soil behavior type and SPT based on data from UBC-1983

# CONE PENETRATION TEST LOG

**Project Name:** Test Pile Evaluation  
**Project No.:** 13-000  
**Sounding:** SCPT-1

**Cone Used:** DDG0892  
**Operator:** Mike Wright  
**CPT Date:** 8/14/2013

**Groundwater Level:** 3.1 feet  
**Elevation:** Unknown  
**Lat/Long:** N30.68546 W88.03791



**Baseline Data:**

	$Q_t$ (tsf)	$F_s$ (tsf)	$P_w$ (psi)
Initial Baseline:	0	0	0
Final Baseline:	-0.602	0.002	-0.172

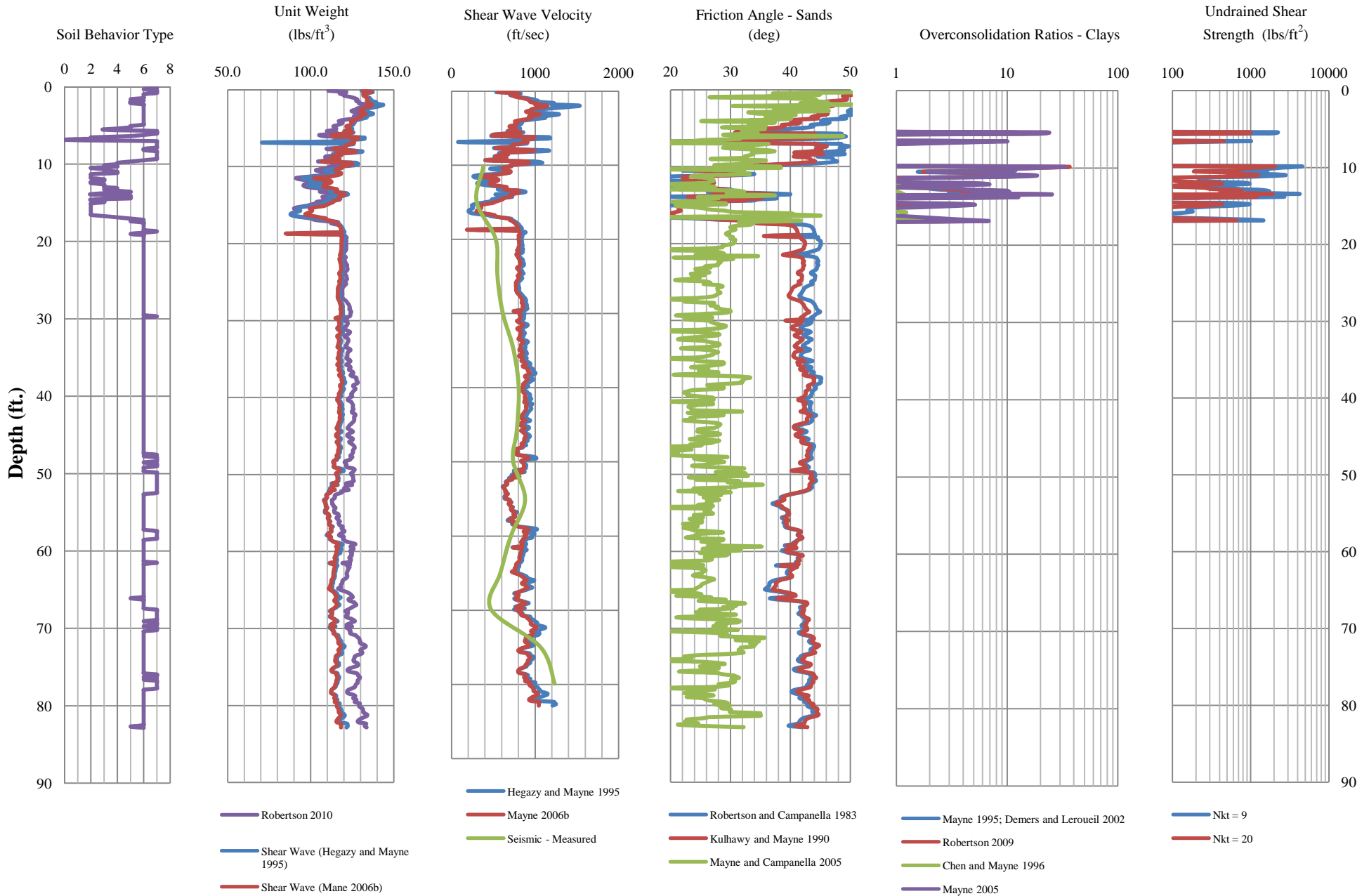
SPT N, SOIL BEHAVIOR TYPE, OR ZONE NUMBER FROM CPT CLASSIFICATION INDEX,  $I_c$   
 Organic Clay Soils = 2, Clays = 3, Silt Mixtures = 4, Sand Mixtures = 5, Sands = 6, Gravelly Sands = 7

# CONE PENETRATION TEST LOG

**Project Name:** Test Pile Evaluation  
**Project No.:** 13-000  
**Sounding:** SCPT-1

**Cone Used:** DDG0892  
**Operator:** Mike Wright  
**CPT Date:** 8/14/2013

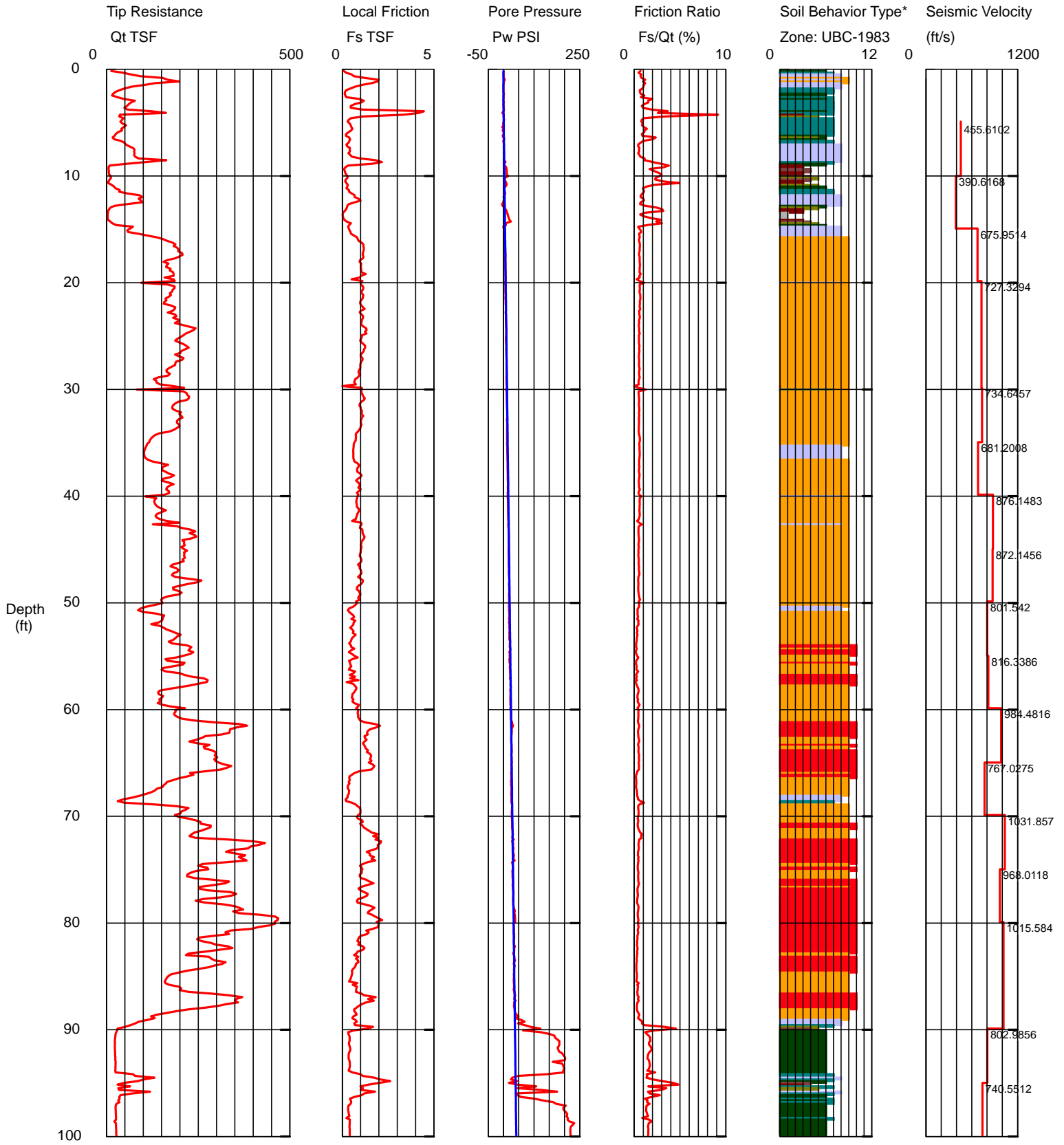
**Groundwater Level:** 3.1 feet  
**Elevation:** Unknown  
**Lat/Long:** N30.68546 W88.03791



# Southern Earth Sciences

Operator: Mike Wright  
 Sounding: SCPT-2  
 Cone Used: DDG0892

CPT Date/Time: 8/14/2013 10:35:15 AM  
 Location: Test Pile Evaluation  
 Job Number: 13-000



Maximum Depth = 99.90 feet

Depth Increment = 0.164 feet

- 1 sensitive fine grained
  - 2 organic material
  - 3 clay
- Groundwater measured at 3.2'

- 4 silty clay to clay
- 5 clayey silt to silty clay
- 6 sandy silt to clayey silt

- 7 silty sand to sandy silt
- 8 sand to silty sand
- 9 sand

- 10 gravelly sand to sand
- 11 very stiff fine grained (\*)
- 12 sand to clayey sand (\*)

N30.68541 W88.03821

\*Soil behavior type and SPT based on data from UBC-1983

# CONE PENETRATION TEST LOG



**Project Name:** Test Pile Evaluation

**Cone Used:** DDG0892

**Groundwater Level:** 3.2 feet

**Project No.:** 13-000

**Operator:** Mike Wright

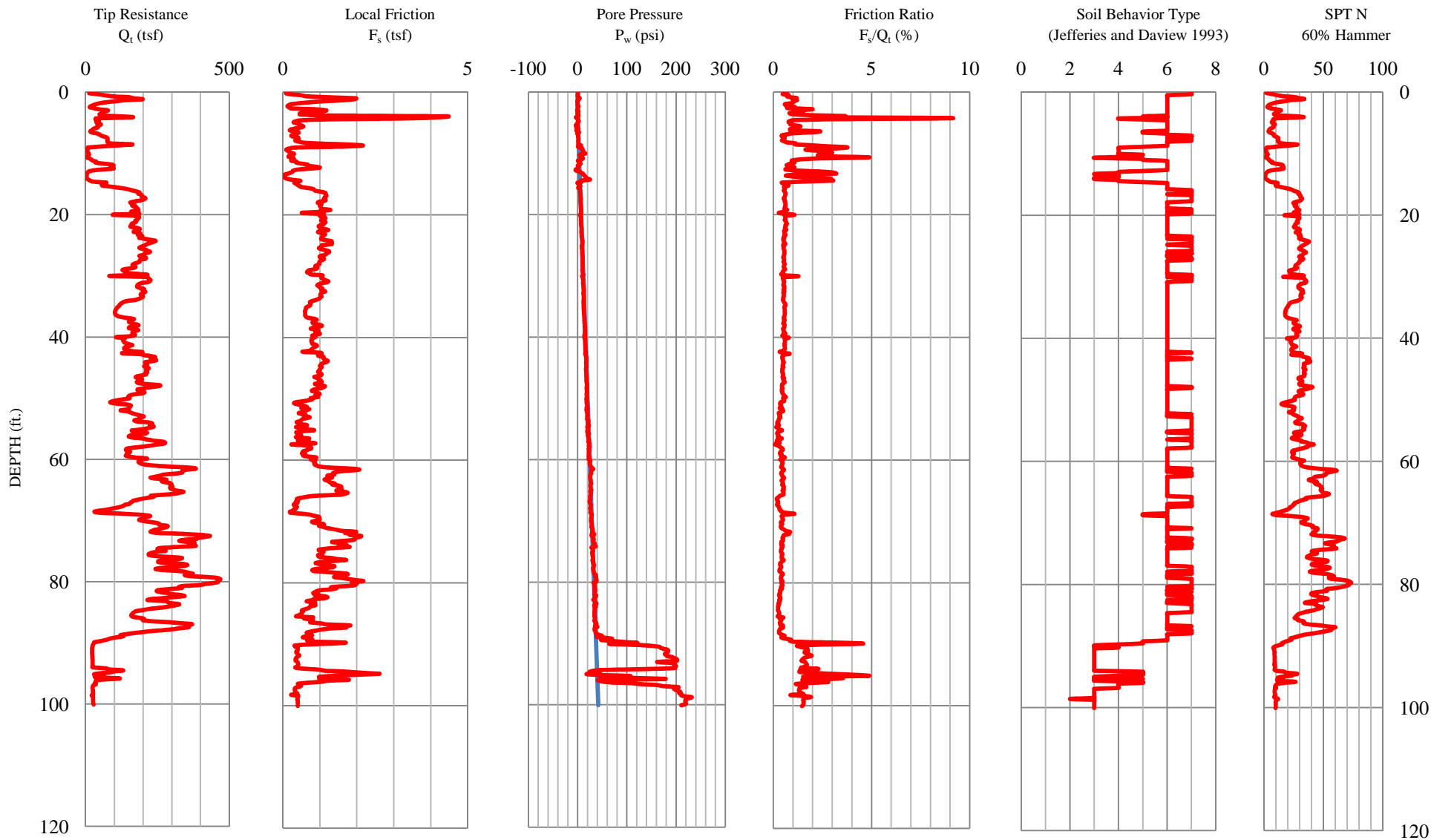
**Elevation:** Unknown

Geotechnical, Environmental & Construction Materials Testing

**Sounding:** SCPT-2

**CPT Date:** 8/14/2013

**Lat/Long:** N30.68541 W88.03821



**Baseline Data:**

	Q <sub>t</sub> (tsf)	F <sub>s</sub> (tsf)	P <sub>w</sub> (psi)
Initial Baseline:	0	0	0
Final Baseline:	0.357	0.012	0.210

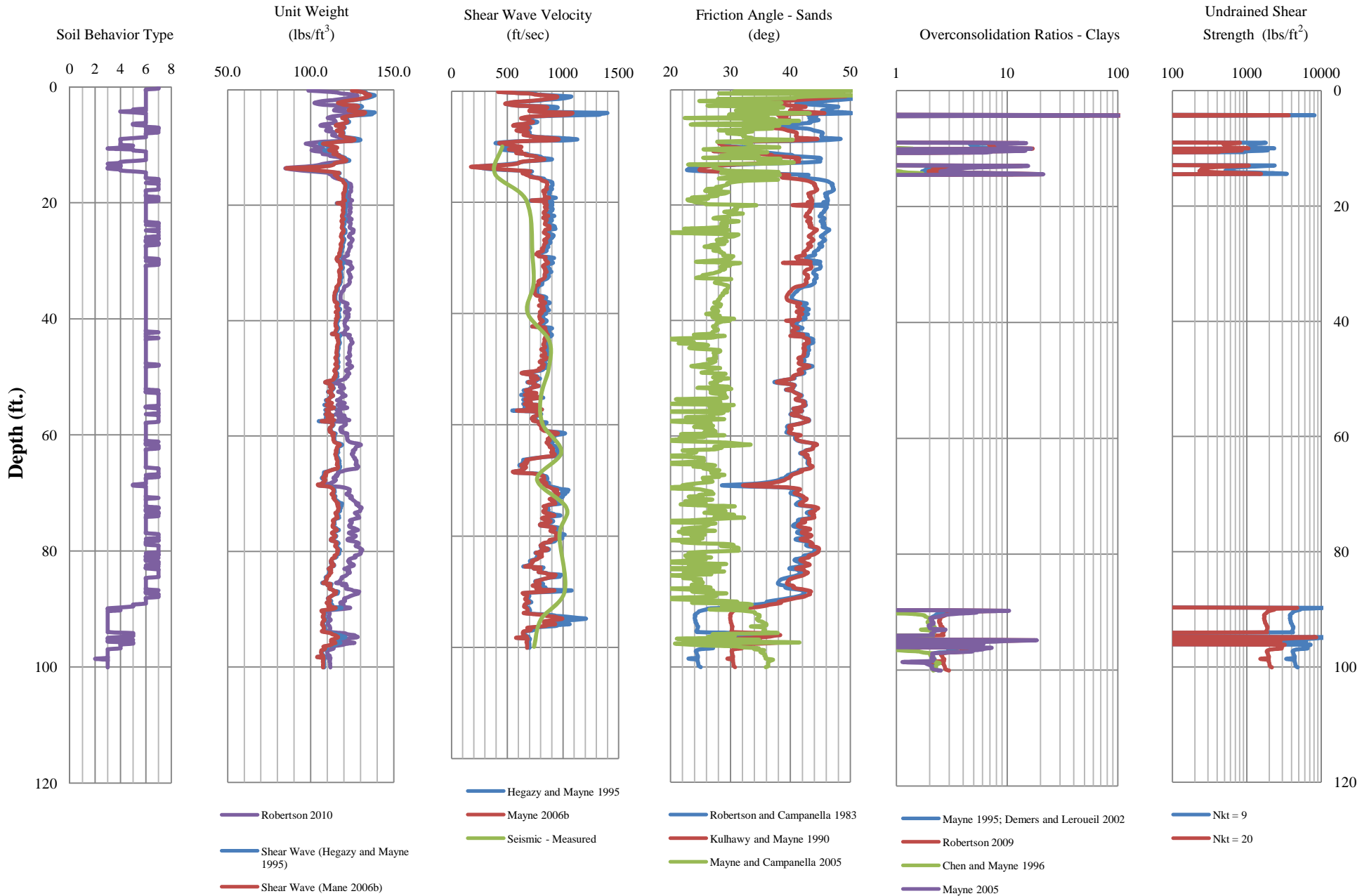
SPT N, SOIL BEHAVIOR TYPE, OR ZONE NUMBER FROM CPT CLASSIFICATION INDEX, I<sub>c</sub>  
 Organic Clay Soils = 2, Clays = 3, Silt Mixtures = 4, Sand Mixtures = 5, Sands = 6, Gravelly Sands = 7

# CONE PENETRATION TEST LOG

**Project Name:** Test Pile Evaluation  
**Project No.:** 13-000  
**Sounding:** SCPT-2

**Cone Used:** DDG0892  
**Operator:** Mike Wright  
**CPT Date:** 8/14/2013

**Groundwater Level:** 3.2 feet  
**Elevation:** Unknown  
**Lat/Long:** N30.68541 W88.03821



PARAMETERS ABOVE ARE BASED UPON EMPIRICAL CORRELATIONS AND SHOULD BE CONSIDERED APPROXIMATE. IT IS RECOMMENDED THAT CALCULATED PARAMETERS BE CORRELATED BY SPECIFIC LABORATORY DATA AND/OR LOCAL EXPERIENCE.



## Appendix B: Pile Driving Hammer Information

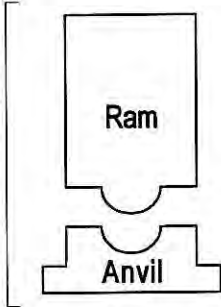
	Fuel Setting #1	Fuel Setting #2	Fuel Setting #3	Fuel Setting #4
<b>Concrete Piles used Delmag Model D-62-22 Single Acting Diesel Hammer</b>				
<b><u>36 in PCP</u></b>				
Setting Usage	Down to 43 feet	43 to 45 feet	45 to 48 feet	48 feet to end Restrikes
Rated Energy	78,960 ft. lbs.	109,725 ft. lbs.	138,960 ft. lbs.	165,000 ft. lbs
<b><u>24 in PCP</u></b>				
Setting Usage	Down to 61 feet	61 feet to end Restrikes	N/A	N/A
Rated Energy	78,960 ft. lbs.	109,725 ft. lbs.		
<b>Steel Piles used APE Model D30-42 Single Acting Diesel Hammer</b>				
<b><u>HP 14</u></b>				
Setting Usage	N/A	N/A	Entire depth Restrikes	N/A
Rated Energy			66,977 ft. lbs.	
<b><u>HP 12</u></b>				
Setting Usage	N/A	Entire depth Restrikes	N/A	N/A
Rated Energy		55,070 ft. lbs		


FORM C-14 **ALABAMA DEPARTMENT OF TRANSPORTATION**  
 Revised 08-07-95 **PILE AND DRIVING EQUIPMENT DATA FORM**

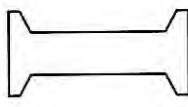
Project Number USA Test Pile & Vibration	County Mobile	Division 9th
---------------------------------------------	------------------	-----------------


Pile Driving Contractor or Subcontractor Jordan Pile Driving Inc.	Bridge Identification Number N/A
----------------------------------------------------------------------	-------------------------------------

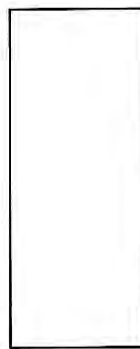
Details of access method to pile top for dynamic testing are:  Attached  Not Applicable

<b>Hammer Components</b>		<b>Hammer</b>	Manufacturer: <u>Delmag</u> Model: <u>D-62-22</u> Type: <u>S.A. Diesel</u> Serial No.: <u>238</u> Rated Energy: <u>165,000</u> (ft.-lbs.) at <u>11.3</u> (ft.) Length of Stroke Modifications: <u>Adjustable Fuel Pump</u> Pump Setting 1 <u>78,960 ft. lbs.</u> Pump Setting 2 <u>109,725 ft. lbs.</u> Pump Setting 3 <u>136,950 ft. lbs.</u> Pump Setting 4 <u>165,000 ft. lbs.</u>
--------------------------	-----------------------------------------------------------------------------------	---------------	------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

	<b>Capblock (Hammer Cushion)</b>	Material: <u>Aluminum &amp; Micarta Alternating</u> Thickness: <u>6</u> (in.) Area: <u>381</u> (in. <sup>2</sup> ) Modulus of Elasticity - E : <u>450 KSI</u> (P.S.I.) Coefficient of Restitution - e : <u>0.8</u>
-----------------------------------------------------------------------------------	----------------------------------	-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

	<b>Pile Cap</b>	<table border="1" style="display: inline-table;"> <tr> <td>Helmet</td> <td><input checked="" type="checkbox"/></td> </tr> <tr> <td>Bonnet</td> <td><input type="checkbox"/></td> </tr> <tr> <td>Anvil Block</td> <td><input type="checkbox"/></td> </tr> <tr> <td>Drivehead</td> <td><input type="checkbox"/></td> </tr> </table> Weight : <u>10,000</u> (lbs.) Note: Should include weight of striker plate.	Helmet	<input checked="" type="checkbox"/>	Bonnet	<input type="checkbox"/>	Anvil Block	<input type="checkbox"/>	Drivehead	<input type="checkbox"/>
Helmet	<input checked="" type="checkbox"/>									
Bonnet	<input type="checkbox"/>									
Anvil Block	<input type="checkbox"/>									
Drivehead	<input type="checkbox"/>									

	<b>Pile Cushion</b>	Cushion Material: <u>Plywood</u> Thickness: <u>10</u> (in.) Area: <u>576</u> (in. <sup>2</sup> ) Modulus of Elasticity - E : <u>45 KSI</u> (P.S.I.) Coefficient of Restitution - e : <u>0.5</u>
-------------------------------------------------------------------------------------	---------------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

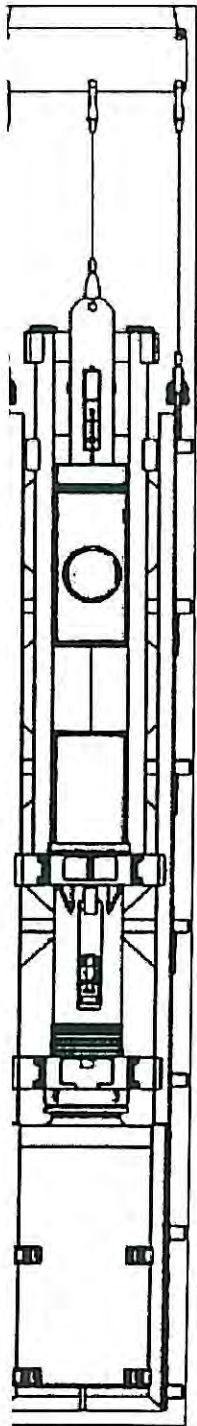
	<b>Pile</b>	Pile Type: <u>36" x 36" &amp; 24" x 24" Prestressed Concrete Test Pile</u> Length (in Leads): <u>89' &amp; 81'</u> (ft.) Weight / Ft: <u>936 &amp; 510</u> (lbs./ft.) Wall Thickness: <u>NA</u> (in.) Taper: <u>NA</u> Cross Sectional Area: <u>489 &amp; 898</u> (in <sup>2</sup> ) Design Pile Capacity: _____ (Tons) Description of Splice: <u>N/A</u> Tip Treatment Description: <u>N/A</u>
-------------------------------------------------------------------------------------	-------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

Note: If mandrel is used to drive this pile, attach separate manufacturer's detail sheet(s) including weight and dimensions.

Submitted By: Davis Daniel Date: \_\_\_\_\_

## Model D62-22 Diesel Hammer

Maximum obtainable energy	203,216 ft-lbs
Maximum obtainable stroke	178 inches
Pump setting 1: (minimum)	78,956 ft-lbs
Pump setting 2:	109,749 ft-lbs
Pump setting 3:	137,186 ft-lbs
Pump setting 4: (maximum)	164,250 ft-lbs
Stroke at rated energy	135 inches
Energy at rated stroke	165,000 ft-lbs
Speed (blows per minute)	36-50
Ram	13,700 lbs
Anvil	2,833 lbs
Hammer weight (includes trip device)	29,491 lbs
Typical operating (weight with drive cap)	32,963 lbs
Fuel tank (runs on diesel or bio-diesel)	25.86 gal
Oil tank	8.32 gal
Weight	1100 lbs
Diameter	25 inches
Thickness	8 inches
Type	Monocast MC 901
Diameter	25 inches
Thickness	2 inches
Elastic-modulus	285 kips per square inch
Coeff. of restitution	0.8
Weight (fits 8 by 26 inch leads)	1,350 lbs
Diesel or Bio-diesel fuel	5.28 gal/hr
Lubrication oil	0.84 gal/hr
**Grease twice per day	
Length overall	232.6 inches
Length over cylinder extension	272.0 inches
Impact block diameter	27.9 inches
Width over bolts	32.6 inches
Hammer width overall	31.5 inches
Width for guiding- face to face	22.0 inches
Hammer center to pump guard	19.3 inches
Hammer center to bolt center	15.0 inches
Hammer depth overall	38.2 inches
Minimum clearance for leads	19.7 inches

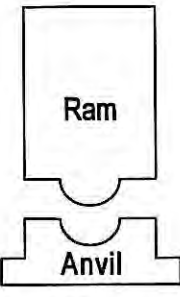



FORM C-14 **ALABAMA DEPARTMENT OF TRANSPORTATION**  
 Revised 08-07-95 **PILE AND DRIVING EQUIPMENT DATA FORM**

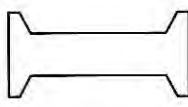
Project Number USA Test Pile & Vibration	County Mobile	Division 9th
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Pile Driving Contractor or Subcontractor Jordan Pile Driving Inc.	Bridge Identification Number N/A
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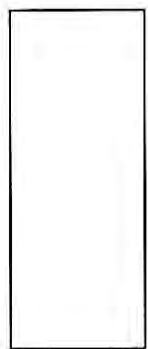
Details of access method to pile top for dynamic testing are:  Attached  Not Applicable

<b>Hammer Components</b>		<b>Hammer</b>	Manufacturer: <u>APE</u> Model: <u>D30-42</u> Type: <u>S.A. Diesel</u> Serial No.: _____ Rated Energy: <u>74,419</u> (ft.-lbs.) at <u>11.25</u> (ft.) Length of Stroke  Modifications: <u>Adjustable Fuel Pump</u> <table style="width:100%; border-collapse: collapse;"> <tr> <td style="width:70%;">Pump Setting 1</td> <td style="width:30%;">37,209 ft. lbs.</td> </tr> <tr> <td>Pump Setting 2</td> <td>55,070 ft. lbs.</td> </tr> <tr> <td>Pump Setting 3</td> <td>66,977 ft. lbs.</td> </tr> <tr> <td>Pump Setting 4</td> <td>74,419 ft. lbs.</td> </tr> </table>	Pump Setting 1	37,209 ft. lbs.	Pump Setting 2	55,070 ft. lbs.	Pump Setting 3	66,977 ft. lbs.	Pump Setting 4	74,419 ft. lbs.
	Pump Setting 1	37,209 ft. lbs.									
Pump Setting 2	55,070 ft. lbs.										
Pump Setting 3	66,977 ft. lbs.										
Pump Setting 4	74,419 ft. lbs.										

	<b>Capblock (Hammer Cushion)</b>	Material: <u>Aluminum &amp; Micarta Alternating</u> Thickness: <u>4</u> (in.) Area: <u>398</u> (in. <sup>2</sup> ) Modulus of Elasticity - E : <u>285</u> (P.S.I.) Coefficient of Restitution - e : <u>0.8</u>
-----------------------------------------------------------------------------------	----------------------------------	-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

	<b>Pile Cap</b>	Helmet <input checked="" type="checkbox"/> Bonnet <input type="checkbox"/> Anvil Block <input type="checkbox"/> Drivehead <input type="checkbox"/>	Weight : <u>1,704</u> (lbs.) Note: Should include weight of striker plate.
-------------------------------------------------------------------------------------	-----------------	-------------------------------------------------------------------------------------------------------------------------------------------------------------	-------------------------------------------------------------------------------

	<b>Pile Cushion</b>	Cushion Material: <u>N/A</u> Thickness: <u>N/A</u> (in.) Area: <u>N/A</u> (in. <sup>2</sup> ) Modulus of Elasticity - E : <u>N/A</u> (P.S.I.) Coefficient of Restitution - e : <u>N/A</u>
-------------------------------------------------------------------------------------	---------------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

	<b>Pile</b>	Pile Type: <u>HP 12 x 53 &amp; HP 14 x117</u> Length (in Leads): <u>70' &amp; 106'</u> (ft.) Weight / Ft: <u>53 &amp; 117</u> (lbs./ft.) Wall Thickness: <u>N/A</u> (in.) Taper: <u>NA</u> Cross Sectional Area: _____ (in. <sup>2</sup> ) Design Pile Capacity: _____ (Tons) Description of Splice: <u>Mechanical</u>  Tip Treatment Description: _____
-------------------------------------------------------------------------------------	-------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

Note: If mandrel is used to drive this pile, attach separate manufacturer's detail sheet(s) including weight and dimensions.

Submitted By: \_\_\_\_\_ Date: \_\_\_\_\_

Davis Daniel



# APE Model D30-42 Single Acting Diesel Impact Hammer

*D30-42 Finishing Dolphin Piles.*



## MODEL D30-42 (3.0 metric ton ram)

### SPECIFICATIONS

Stroke at maximum rated energy	135 in (343 cm)
Maximum rated energy (Setting 4)	74,419 ft-lbs (100.47 kNm)
Setting 3	66,977 ft-lbs (90.42 kNm)
Setting 2	55,070 ft-lbs (74.34 kNm)
Minimum rated energy (Setting 1)	37,209 ft-lbs (50.23 kNm)
<i>(Variable throttle allows for infinite fuel settings)</i>	

Maximum obtainable stroke	157 in (381 cm)
Maximum obtainable energy	86,546 ft-lbs (117 kNm)
Speed (blows per minute)	34-53

### WEIGHTS

Ram	6,615 lbs (3,000 kg)
Anvil	1,358 lbs (616 kg)
Anvil cross sectional area	367.94 in <sup>2</sup> (2373.80 cm <sup>2</sup> )
Hammer weight (includes trip device)	13,571 lbs (6,154 kg)
Typical operating (weight with DB26 and H-beam insert)	16,223 lbs (7,357 kg)

### CAPACITIES

Fuel tank (runs on diesel or bio-diesel)	17.4 gal (65 liters)
Oil tank	5 gal (19 liters)

### CONSUMPTION

Diesel or Bio-diesel fuel	2.6 gal/hr (9.84 liters/hr)
Lubrication	0.26 gal/hr (1 liters/hr)
Grease	8 to 10 pumps every 45 minutes of operation time.

*Optional Variable Throttle Control.*



### STRIKER PLATE FOR DB 26

Weight	628 lbs (284 kg)
Diameter	22.5 in (57.15 cm)
Area	398 in <sup>2</sup> (2567.74 cm <sup>2</sup> )
Thickness	6 in (15.24 cm)

### CUSHION MATERIAL

Type/Qty	Micarta / 2 each
Diameter-DB26	22.5 in (57.15 cm)
Thickness	1 in (25.4 mm)

*Drive Base Assembly.*



Type/Qty	Aluminum / 3 each
Thickness	1/2 in (12.7 mm)
Diameter	22.5 in (57.15 cm)
Total Combined Thickness	3.5 in (8.89 cm)
Area	398 in <sup>2</sup> (2567.74 cm <sup>2</sup> )
Elastic-modulus	285 ksi (1,965 mpa)
Coeff. of restitution	0.8

### DRIVE CAP

DB 26:	1,076 lbs (488 kg)
--------	--------------------

### INSERT WEIGHT

H-Beam insert for 12" (305 mm) and 14" (355 mm):	948 lbs (430 kg)
Large pipe insert for sizes 12" to 24" diameter:	1,830 lbs (830 kg)

### MINIMUM BOX LEAD SIZE/OPERATING LENGTH

Minimum box leader size	8 in x 26 in (20.32 cm x 66 cm)
Operating length as described above	354 in (900 cm)



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# **APPENDIX M**

## **Vibrations Study**





# Final Report on Vibrations Due to Pile Driving at the Mobile River Bridge Site

Research Project 930-839R

*INVESTIGATION OF PILE SETUP (FREEZE) IN ALABAMA*

*Development of a Setup Prediction Method and Implementation into LRFD Driven Pile Design*

*Addendum: Pile Driving Vibration Monitoring of the Future Mobile River Bridge Project*



**John Cleary, Ph.D., P.E. (P.I. for Vibration Monitoring)**

**Eric Steward, Ph.D. (Co. P.I. for Vibration Monitoring)**

**Andrew Gillis (Graduate Student)**

Department of Civil Engineering

University of South Alabama

Mobile, AL 36688

June 12, 2015

## **ACKNOWLEDGEMENT**

The Alabama Department of Transportation sponsored this project. Their support for this study is greatly appreciated.

## **DISCLAIMER**

The contents of this report reflect the views of the authors who are responsible for the facts and accuracy of the data presented herein. The contents do not necessarily reflect the official views or policies of the Alabama DOT or the University of South Alabama. This report does not constitute a standard, specification, or regulation. Comments contained in this paper related to specific testing equipment and materials should not be considered an endorsement of any commercial product or service; no such endorsement is intended or implied.

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## ABSTRACT

All projects have some amount of inherent risk; one such risk associated with construction projects is the potential for ground vibrations that could damage nearby structures. Research has been conducted on the effects of vibrations on structures; however, the expected levels of vibration are dependent on several factors including the soil conditions at the construction site. Therefore, site-specific investigations are often recommended.

After concerns were raised by the Alabama Department of Transportation (ALDOT) about damage potential at a project site in South Alabama, an addendum was added to a research project related to investigating pile setup in Alabama soils. The purpose of the addendum was to investigate ground vibrations from pile driving at a project site near the Mobile River in Mobile, Alabama.

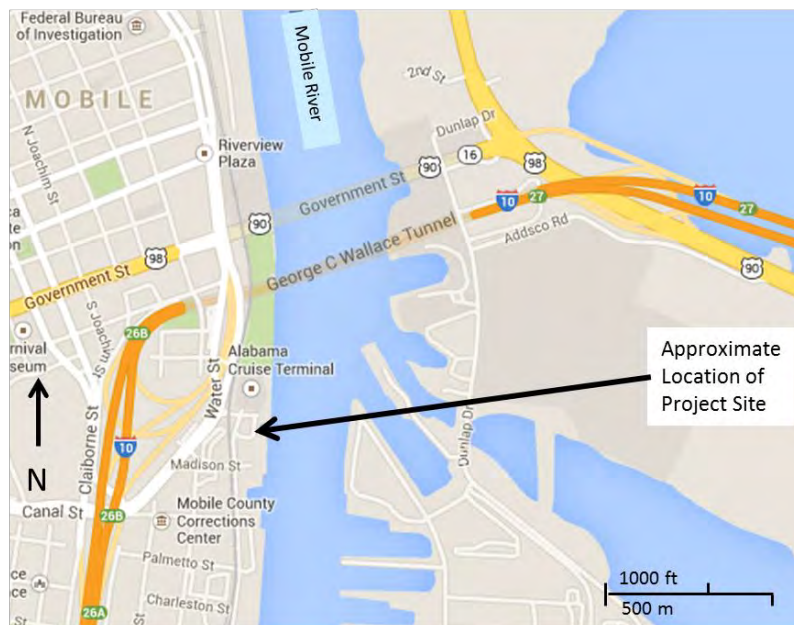
An investigation and vibration monitoring program was developed for four pile sizes that are often used by the Alabama Department of Transportation (ALDOT). The piles included thirty-six inch square and twenty-four inch square concrete piles, as well as, two steel H-Piles. The piles were driven using typical installation techniques and the vibration levels at various distances from the piles were monitored.

The investigation found that the largest vibrations were observed while driving the thirty-six inch concrete pile. The maximum vibrations observed had a magnitude of 0.82 inches per second at fifty feet from the pile. The vibrations at 150 feet from the pile had dissipated to 0.15 inches per second. The results of the monitoring program and a literature review determined that an allowable vibration level of 0.5 inches per second for modern structures and 0.1 inches per second for potentially sensitive structures should be established for construction activity at or near the location of the project site. Additionally, a survey distance of 150 feet for modern structures and 250 feet for potentially sensitive structures is recommended.

# INTRODUCTION

## Background

The following report contains the analysis of ground vibrations generated during a pile driving research study located at the Mobile River Bridge Project Site. The project site, owned by the Alabama Department of Transportation (ALDOT), is located on the Mobile River just south of the Alabama Cruise Terminal, Figure 1. The study consisted of monitoring ground vibrations during the installation of four driven piles; two precast concrete piles and two steel H-piles. The study was conducted in response to concerns raised by ALDOT related to possible damage of nearby structures from ground-borne vibrations. The primary objective of this project was to determine the distance that pile driving operations can be conducted with minimal risk to nearby structures. To accomplish this, the vibration levels at various distances from the driven piles were determined and a prediction equation for other distances was developed. This study was conducted by researchers from the Department of Civil Engineering at the University of South Alabama between August 15, 2013 and August 27, 2013.



**Figure 1:** Location of project site, Mobile, AL (Google 2013)

## Objective

This project consisted of several objectives. The first was to determine the vibration levels from typical piles used by ALDOT. The second objective was to develop a methodology to predict vibrations at any distance from the pile. The third and final objective of the project was to develop guidelines on allowable vibrations for the project site.

## **Scope**

The scope of this report is limited to the vibrations portion of the larger project: *Investigation of Pile Setup (Freeze) In Alabama: Development of a Setup Prediction Method and Implementation into LRFD Driven Pile Design; Addendum: Pile Driving Vibration Monitoring of the Future Mobile River Bridge Project* (Research Project 930-839R).

The vibrations portion of the project was limited to the aforementioned location near the Mobile River. The project included monitoring vibrations during pile installation and restrikes, analysis of vibration data, development of vibration prediction methodology, and vibration limit recommendations.

## **Report Organization**

The report is organized into five main sections: Introduction, Literature Review, Experimental Design, Results, and Conclusions. Each section contains sub sections as needed.



## LITERATURE REVIEW

### Construction Vibrations

Ground vibrations are commonly generated from several sources including roadway traffic, railroad traffic, and construction activity. Vibrations can be measured and quantified using several different parameters including: displacement, velocity, and acceleration. Ground vibrations are typically measured by the velocity of the ground surface and reported as Peak Particle Velocity or PPV. Typical units of PPV are inches per second (in/sec) in the US system or millimeters per second (mm/sec) in the SI system of units. Typical construction activity that generates vibrations includes: pile driving, heavy equipment operation, concrete breaking (jackhammers), and truck/equipment traffic. Although the level of vibrations generated from these sources can vary widely, some typical vibration levels have been included in Table 1.

**Table 1:** Typical ground vibrations from construction equipment (Hanson, Towes and Lance 2006)

<b>Equipment</b>		<b>PPV (in/sec)</b> (Distance = 25 ft.)
Pile Driver (impact)	upper range	1.518
	typical	0.644
Pile Driver (vibratory)	upper range	0.734
	typical	0.170
Bulldozer	large	0.089
	small	0.003
Caisson Drilling		0.089
Loaded Trucks		0.076
Jackhammer		0.035

Table 1 shows that under typical conditions, pile driving has the potential to create large vibration levels, relative to other construction activity. The pile installation method, however, can affect the level of vibrations. High displacement piles are typically driven using an impact hammer and low displacement piles are sometimes driven using a vibratory hammer. Research has shown that the vibration magnitudes from vibratory hammers are typically smaller than from impact hammers. Additionally, installation techniques such as pre-boring and jetting can reduce vibration levels from impact pile driving (Woods 1997).

The mechanism of vibration formation is the transfer of energy from the pile driving hammer to the pile and then to the surrounding soil. The transfer of energy comes from two main sources. The first is the skin friction that is developed along the surface of the pile and the second is the displacement of the soil at the pile tip. For high displacement piles, the main source of energy transfer is at the pile tip. Several factors can affect the magnitude of vibrations including pile size, pile type, soil type, and the hammer energy. The most important factor in determining vibration levels is the distance from the pile, since vibrations will mitigate or dampen with distance from the source (Dowding 1996).

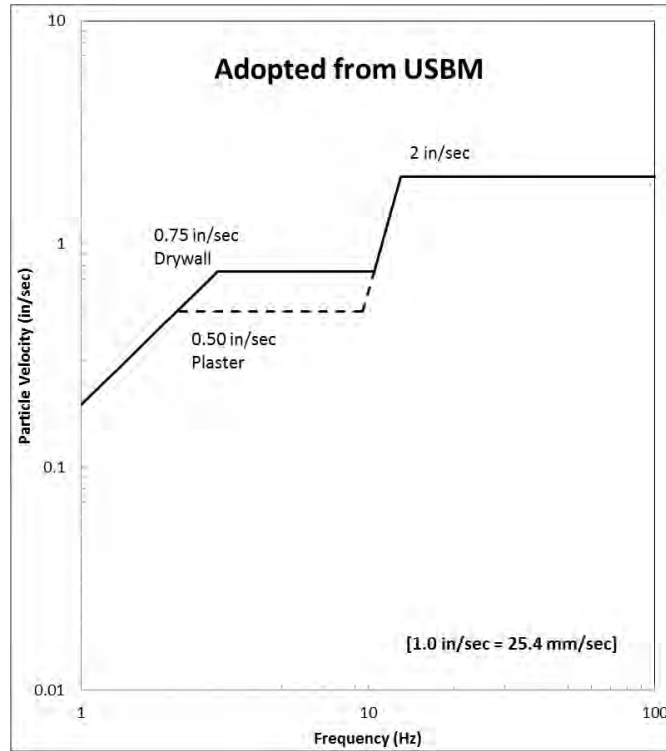
## Damage Thresholds

Vibrations generated from construction activity can cause several concerns at adjacent structures that range from annoyance to structural damage. Several studies have been conducted to determine the relationship between vibration levels, human perception, and structural damage. Table 2 contains a summary of a study reported by Hendriks (2002) for continuous vibrations. The study concluded that vibration levels that are large enough to “annoy people” are at threshold levels for architectural damage to structures that contain plaster walls or ceilings. Since these levels are below levels of even minor structural damage, the perception of building occupants can sometimes lead to discrepancies in the effects of vibrations. The values listed in Table 2 are generally conservative when compared to pile driving vibrations since they were developed for continuous vibrations. Pile driving operations develop discontinuous vibrations that can reduce the damage potential (Hendriks 2002).

**Table 2:** Continuous vibration levels and effects (Hendriks 2002)

<b>Vibration Level (Peak Particle Velocity)</b>	<b>Human Reaction</b>	<b>Building Effects</b>
0.006-0.019 in/sec	Threshold of perception;	Vibrations unlikely to cause damage
0.08 in/sec	Vibration readily perceptible	Recommended upper level for ruins and ancient monuments
0.1 in/sec	Continuous vibrations begin to annoy people	Virtually no risk of “architectural” damage to normal buildings
0.2 in/sec	Vibrations annoying to people in buildings	Threshold at which there is a risk of “architectural” damage to normal dwelling- houses with plaster wall and ceilings
0.4-0.6 in/sec	Vibrations considered unpleasant by people subjected to continuous vibrations	Vibrations at a greater level than normally expected from traffic, but would cause “architectural” damage and possible minor structural damage

In addition to the many studies to determine the effect of vibrations on structures, several State and Federal Agencies, as well as, International Organizations have developed guidelines on permissible vibration levels due to construction activity. Much of the early work related to vibrations was performed by the United States Bureau of Mines (USBM) in the 1970’s and 80’s (Siskind, et al. 1980). This research focused on vibrations from blasting operations. Figure 2 shows the recommended vibration limits for blasting as a function of frequency. The limits range from 0.2 to 2.0 inches per second (in/sec).



**Figure 2:** Vibration limits from the USBM (Siskind, et al. 1980)

The American Association of State Highway and Transportation Officials (AASHTO) and the Federal Transit Administration (FTA) have developed guidelines for vibration limits that range from 0.1 to 1.5 in/sec depending on the structure type as shown in Table 3.

**Table 3:** AASHTO and FTA criteria for construction vibrations

<b>Organization/Jurisdiction</b>	<b>Comments</b>	<b>PPV (in/sec)</b>
American Association of State Highway and Transportation Officials (AASHTO 1990)	Residential buildings, plastered walls	0.2-0.3
	Residential buildings in good repair with gypsum board walls	0.4-0.5
	Engineered structures, without plaster	1.0-1.5
	Historic sites or other critical locations	0.1
Federal Transit Administration (FTA 2006)	Reinforced-concrete, steel or timber	0.5
	Engineered concrete and masonry	0.3
	Non-engineered timber and masonry	0.2
	Buildings extremely susceptible to vibration damage	0.12

The vibration criteria developed by the various states also have a wide range of values as shown in Table 4. If the table is carefully analyzed, the vibration limits can be divided into several categories including: modern structures, sensitive structures, and miscellaneous structures. The range of vibration limits for modern structures is from 0.4 to 1.0 in/sec and sensitive structures have a range of 0.08 to 0.2 in/sec. These vibration limits correlate well to the AASHTO and FTA limits. A thorough review of construction vibration limits can be found in several reports including: (Tao and Zhang 2012), (Wilson Ihrig & Associates 2012), and (Cleary 2013).

**Table 4:** State criteria for construction vibrations

<b>Organization/Jurisdiction</b>	<b>Comments</b>	<b>PPV (in/sec)</b>
California Department of Transportation (Caltrans 2002)	Upper level for possible damage	0.4-0.6
	Threshold for damage to plaster	0.20
	Ruins and ancient monuments	0.08
Florida DOT (FDOT 2010)	All construction	0.5
	Fresh concrete	1.5
Iowa DOT (Iowa DOT n.d.)	Project specific specification	0.2
Louisiana Department of Transportation and Development (Tao and Zhang 2012)	General scenario	
	- New requirements	0.5
	- Old requirements	0.2
	Historic structures or loose sandy soil	0.1
New Hampshire DOT (NHDOT 2010)	Modern Homes	0.75
	Older Homes	0.50
New York City DOT (New York City DOT 2009)	Piles driven adjacent to subway structures (may be lowered)	0.5
Rhode Island DOT (RIDOT 2010)	Lower limits may be applied by engineer	1.0

## Dynamic Settlement

In addition to structural damage and human perception, dynamic settlement can occur due to construction vibrations. Research has shown that if loose cohesionless soils (loose sands) are present, relatively low vibration levels can cause densification (Dowding 1996). This densification can lead to settlement related damage in adjacent structures. Loose sands are typically defined as having a relative density less than 40% (Tao and Zhang 2012). Dynamic settlement has occurred in some soils at vibration levels as low as 0.1 in/sec. If loose sands are located on or near a project site, then special considerations for construction vibrations need to be considered.

## Vibration Prediction

Since it is typically unrealistic for most construction projects to conduct full scale testing to determine the expected levels of vibrations and since only a discrete number of locations are measured during testing, several methods have been developed to predict vibration levels. The first prediction equations were developed as early as 1912 by Golitsin who developed a simple equation to predict the peak particle displacement of ground vibrations from earthquakes. The equation, as reported by (Bayraktar, et al. 2013) is as follows,

$$A_2 = A_1 \sqrt{r_1/r_2} e^{-\gamma(r_2-r_1)}, \quad (1)$$

where  $A_1$  is the peak particle displacement of ground vibrations at a distance  $r_1$  from the source,  $A_2$  is the peak particle displacement of ground vibrations at a distance  $r_2$  from the source, and  $\gamma$  is a vibration attenuation coefficient.

More recently, several methods have been developed to predict the peak particle velocity (PPV) from construction activity, pile driving in particular. Hendriks (2002) reported several equations to predict the propagation of construction vibrations. The first equation presented by Hendriks was first reported by Richart, et.al. (1970), who cited Bornitz (1931),

$$V = V_o (D_o/D)^{0.5} e^{\alpha(D_o-D)} \quad (2)$$

where  $V$  is the peak particle velocity at distance  $D$ ,  $V_o$  is the peak particle velocity at reference distance  $D_o$ , and  $\alpha$  is a vibration attenuation parameter that must be determined experimentally.

Hendriks (2002) also reported a simplified equation for pile driving vibrations that is similar to an equation reported by Woods (1997) as follows,

$$V = V_o (D_o/D)^k \quad (3)$$

where  $V$  is the peak particle velocity at distance  $D$ ,  $V_o$  is the peak particle velocity at reference distance  $D_o$ , and  $k$  is a vibration attenuation parameter that must be determined experimentally.

Several researchers have found that a better correlation with predicted and measured vibrations could be determined by including the energy of the pile driving hammer in the equation. This approach is often referred to as the “scaled-distance” approach. One commonly used equation was developed by Wiss and reported by Bayraktar, et al. (2013),

$$v = k [D/\sqrt{W_t}]^{-n} \quad (4)$$

where  $W_t$  is the energy of the source,  $v$  is the peak particle velocity at distance  $D$ ,  $k$  is the intercept value of the peak particle velocity at a scaled distance of  $D/(W_t)^{1/2}$  equal to one, and  $n$  is a vibration attenuation parameter that must be determined experimentally.

The previous equations are relatively accurate at predicting ground vibrations when compared to experimental data, however, they all require testing to determine the soil parameters. Jones & Stokes (2004) performed an extensive literature review and determined that the following equation, with the assumed values shown, could be used to predict pile driving vibrations without experimental evaluations:

$$PPV_{Impact\ Pile\ Driver} = PPV_{Ref}(25/D)^n(E_{equip}/E_{ref})^{0.5} \quad (5)$$

where  $PPV_{Impact\ Pile\ Driver}$  is the peak particle velocity at distance D in feet,  $PPV_{Ref}$  is equal to 0.65 in/sec for a reference pile driver at 25 feet,  $E_{ref}$  is equal to 36,000 ft-lb (rated energy of reference pile driver),  $E_{equip}$  is the rated energy of impact pile driver in foot-pounds, and n is a vibration attenuation parameter with a recommended value of 1.1.

Jones and Stokes also provided a table, Table 5, with suggested “n” values based on the soil type.

**Table 5:** Suggested “n” values based on soil class: Adopted from (Jones & Stokes 2004)

<b>Soil Class</b>	<b>Description of Soil</b>	<b>Suggested Value of “n”</b>
I	Weak or soft soils: loose soils, dry or partially saturated peat and muck, mud, loose beach sand, and dune sand, recently plowed ground, soft spongy forest or jungle floor, organic soils, top soil. (shovel penetrates easily)	1.4
II	Competent soils: most sands, sandy clays, silty clays, gravel, silts, weathered rock. (can dig with shovel)	1.3
III	Hard soils: dense compacted sand, dry consolidated clay, consolidated glacial till, some exposed rock. (cannot dig with shovel, need pick to break up)	1.1
IV	Hard, competent rock: bedrock, freshly exposed hard rock. (difficult to break with hammer)	1.0

## EXPERIMENTAL DESIGN

### Overview

The main objective of this research was to determine the distance from nearby structures that pile driving operations can be conducted with minimal risk to those structures. It is important to note that these guidelines were developed for typical piles used by ALDOT at the project site. The project was divided into two phases, collecting data during pile driving and analyzing the data. The information related to the project site, the test piles, the pile driving equipment, and the data collection equipment is located below.

### Project Site

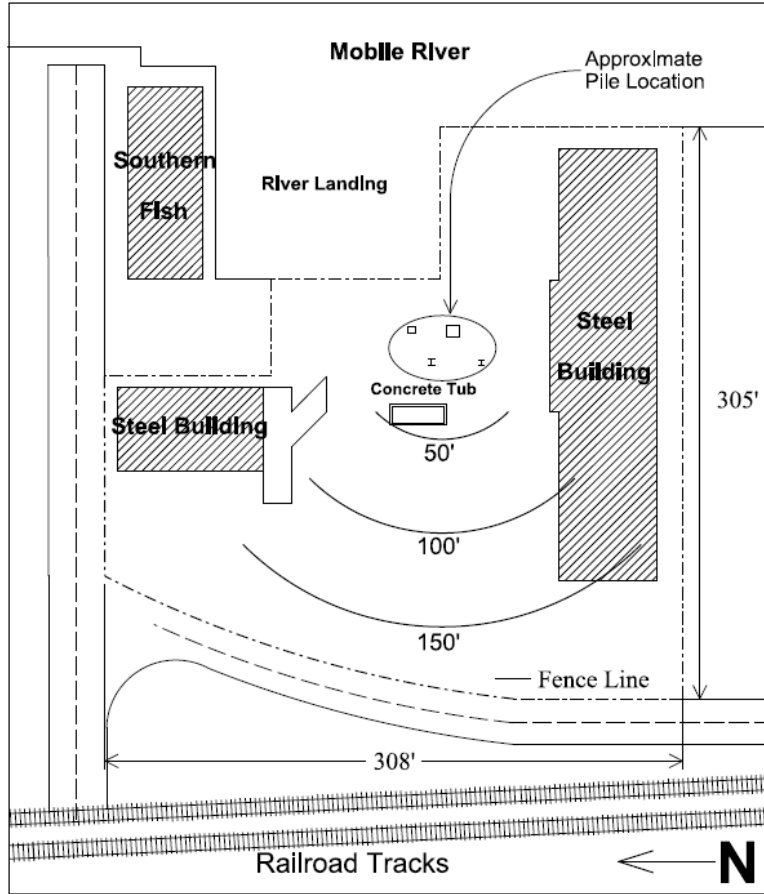
The project site is located on the west bank of the Mobile River, just south of the Alabama Cruise Terminal. The soil profile at the site consists primarily of sandy soils to a depth of 90 feet below the ground surface with a clay layer located at an approximate depth of 90 to 110 feet. Table 6 contains a summary of the soil layers that were defined by a standard penetration test (SPT) conducted at the project site. Appendix A contains the details of the soil investigations conducted by an ALDOT drill crew and Southern Earth Sciences.

**Table 6:** Soil profile at site location

<b>Depth (ft.)</b>	<b>Basic Material</b>	<b>Average Blow Count</b>	<b>Consistency</b>
0-23.5	Sand	12	Loose to Medium
23.5-89.5	Sand	31	Medium to Dense
89.5-108.5	Clay	28	Stiff to Very Stiff
108.5-115	Sand	27	Medium

Figure 3 contains a plan view of the project site. The dashed line in the figure represents the approximate property boundary. Note that the pile locations are approximate and the drawing is not to scale. The arc lines shown in the drawing represent the approximate distance from the piles to where the monitoring equipment was located.





**Figure 3:** Plan view of Mobile River Bridge Project Site

Four test piles were driven for this project, two prestressed precast concrete piles (PPC) and two steel H-Piles. Table 7 contains descriptions of the piles and Appendix B contains the details of the two pile driving hammers utilized on this project. The piles were installed using typical techniques including pile jetting or vibration followed by driving with a diesel hammer. The concrete piles were jetted to a depth of approximately 30 feet and driven to the final elevation using a Delmag Model D-62-22 diesel hammer. A vibratory driver was used to drive the steel HP 14 to 55 feet and the HP 12 to 15 feet. The steel piles were then driven to the final elevation using an APE Model D30-42 diesel hammer.

**Table 7:** Pile descriptions

<b>Pile</b>	<b>Cross Section</b>	<b>Material</b>	<b>Length</b>
#1	24" Square	Precast Concrete	81 ft
#2	36" Square	Precast Concrete	89 ft
#3	HP14x117	Steel	106 ft
#4	HP12x53	Steel	70 ft

## **Vibration Monitoring**

Data collectors were placed at various locations throughout the pile installation and testing process. The data collectors utilized for this project were Minimate Plus tri-axial geophones manufactured by Instatel. Each tri-axial geophone unit contains three geophones oriented on three mutually perpendicular axes. The units come with software allowing data collection and analysis in several configurations. For this research, the units were configured to collect histogram data during two-second intervals. When configured in this way the data collector measures all vibrations over the interval, but only records the maximum PPV and the frequency that it occurred at for each geophone over the two second interval.

The geophones were placed at predetermined distances from each pile during installation. Three of the data collectors were located at approximately 50, 100, and 150 feet. A fourth data collector, which had two geophone units attached to it, was located at various distances throughout testing to collect additional information. Table 8 contains a detailed account of the location of each data collector during testing.

During the initial driving of the 36-inch PPC pile, geophone number three was located at the edge of the project site near Southern Fish and Oyster, an adjacent property owner. The fourth data collector had one geophone unit placed at 100 feet from the pile and the other geophone unit was attached to the brick façade of a building that was located on the project site. Please note that the 30-day restrike was at 32-days for the 36-inch concrete pile and 31-days for the 24-inch concrete pile.

**Table 8:** Geophone location during testing

<b>Initial Drive</b>	<b>Pile Type</b>	<b>Geophone Unit</b>					<b>#4b</b>
		<b>#1</b>	<b>#2</b>	<b>#3</b>	<b>#4a</b>		
Aug. 19, 2013	36" PCP	50 ft	150 ft	69 ft	100 ft	Building	
Aug. 20, 2013	24" PCP	99.5 ft	142 ft	n/a	n/a	n/a	
Aug. 21, 2013	HP 12	53 ft	101 ft	144 ft	n/a	n/a	
Aug. 21, 2013	HP 14	58 ft	106 ft	146 ft	n/a	n/a	
<b>24 Hour Restrike</b>							
Aug. 22, 2013	HP 12	50 ft	150 ft	100 ft	n/a	n/a	
Aug. 22, 2013	HP 14	50 ft	150 ft	100 ft	n/a	n/a	
<b>3-Day Restrike</b>							
Aug. 22, 2013	36" PCP	50 ft	n/a	100 ft	n/a	n/a	
Aug. 23, 2013	24" PCP	50 ft	150 ft	100 ft	n/a	n/a	
<b>7-Day Restrike</b>							
Aug. 26, 2013	36" PCP	50 ft	150 ft	100 ft	75 ft	125 ft	
Aug. 27, 2013	24" PCP	50 ft	150 ft	100 ft	75 ft	125 ft	
<b>30-Day Restrike</b>							
Sept. 20, 2013	36" PCP	50 ft	150 ft	100 ft	n/a	n/a	
Sept. 20, 2013	24" PCP	55 ft	155 ft	105 ft	n/a	n/a	
Sept. 20, 2013	HP 12	50 ft	150 ft	100 ft	n/a	n/a	
Sept. 20, 2013	HP 14	50 ft	150 ft	100 ft	n/a	n/a	

## RESULTS

### Vibration Levels

Vibrations were monitored during installation and restrikes on the 36-inch concrete pile at three, seven, and thirty days. A communication error occurred between the ALDOT personnel, the pile driving contractor, and the research team during the installation of the 24-inch concrete pile which resulted in the start of driving prior to the installation of the vibration monitors. Due to this error, the 24-inch concrete pile only had vibrations monitored during the final stage of driving and at all restrikes. The steel piles were monitored during installation and during the one day and thirty day restrikes.

Baseline vibration data was collected at the project site by monitoring vibration levels due to railroad activity from a pair of railroad tracks located adjacent to the project site, Figure 3. The approximate distance from the tracks to the data collectors was determined and the vibration levels from train activity were evaluated. Due to the relatively low vibration levels recorded during train activity, baseline data was not collected for truck traffic.

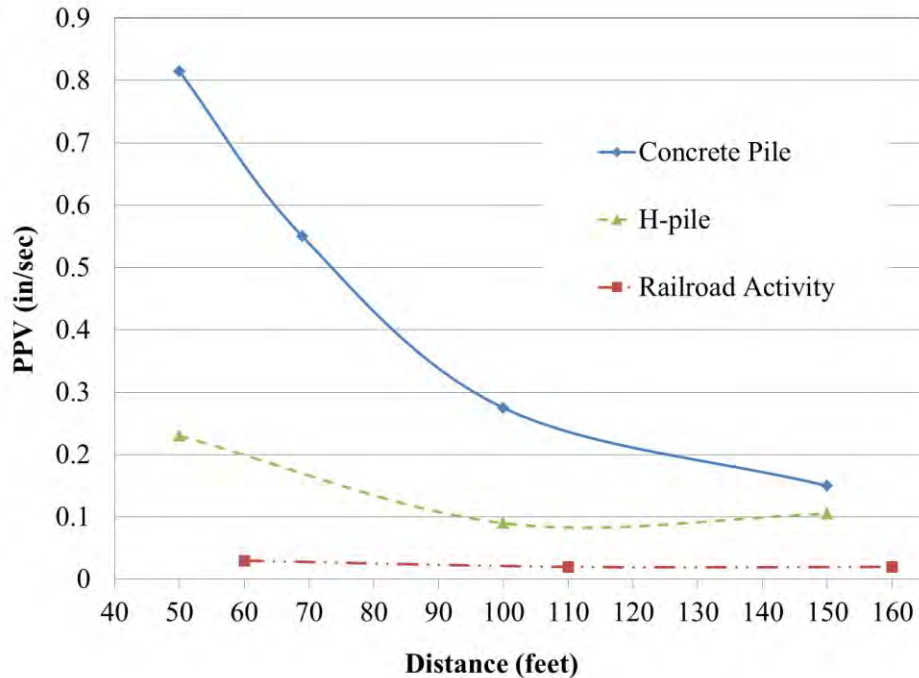
The vibration data collected from the project site was analyzed and the peak particle velocity (PPV) from each pile was recorded. Table 9 contains a summary of the results. The largest recorded vibration during this study occurred while driving the 36-inch concrete pile and resulted in a PPV of 0.82 inches per second at a distance of 50 feet.

**Table 9:** Maximum PPV (in/sec) during pile driving operations

Vibration Source	Horizontal Distance from Pile		
	50 feet	100 feet	150 feet
36" Concrete Pile	0.82	0.28	0.15
HP14x117	0.18	0.09	0.11
HP12x53	0.23	0.07	0.08
Railroad Activity	0.03 <sup>1</sup>	0.02 <sup>1</sup>	0.02 <sup>1</sup>

<sup>1</sup>The approximate distances were 60, 110, and 160 feet

Figure 4 shows the maximum PPV for the 36-inch concrete pile, the H-Piles, and railroad activity observed during testing. Since the maximum vibrations occurred during the beginning of the driving process, the 24-inch concrete pile was not included in this figure. The figure confirms that the largest vibrations recorded were associated with the installation of the 36-inch concrete pile.



**Figure 4:** Maximum recorded vibration levels during pile installation

During the driving of the 36-inch concrete pile, one of the geophones was attached to the brick façade of a building that was located on the project site. The building was located to the south of the piles, Figure 3, and was approximately 90 feet from the 36-inch concrete pile. The brick façade was located on the west end of the building and was approximately 140 feet from the pile. The data from this geophone was analyzed and it was determined that the vibration levels were below the threshold for detection, 0.005 in/sec. This indicates that the ground vibrations did not have enough energy to cause vibrations in the building. Additionally, crack width monitors were installed on the outside wall of the building. The crack widths and lengths were monitored throughout the project and it was determined that there were no changes in any of the cracks.

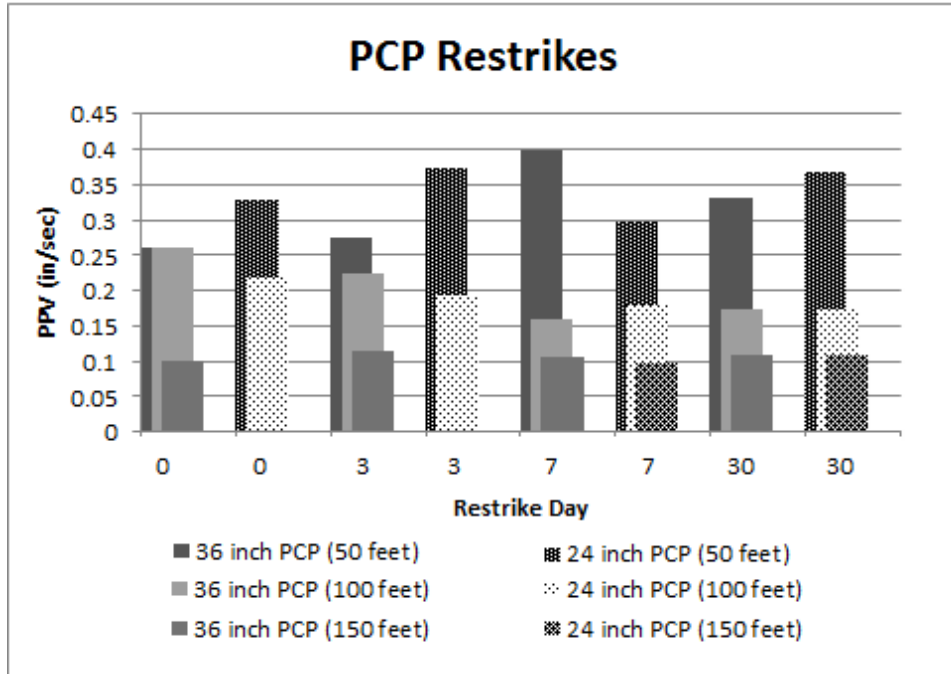


Figure 5: Bar chart of restrikes on precast concrete piles (PCP)

An analysis was performed to compare the vibrations between the 24- and 36-inch concrete piles since data was not collected throughout the driving of the 24-inch pile. Figure 5 shows a bar chart of the vibration levels for each of the concrete piles during the restrikes, note that day zero is at the end of drive. Figure 6 shows the same data in the form of a data plot. The data indicates that the vibration levels for the 24- and 36-inch concrete piles are similar and that the maximum vibrations, near the start of driving, would be expected to be approximately equal for each concrete pile.

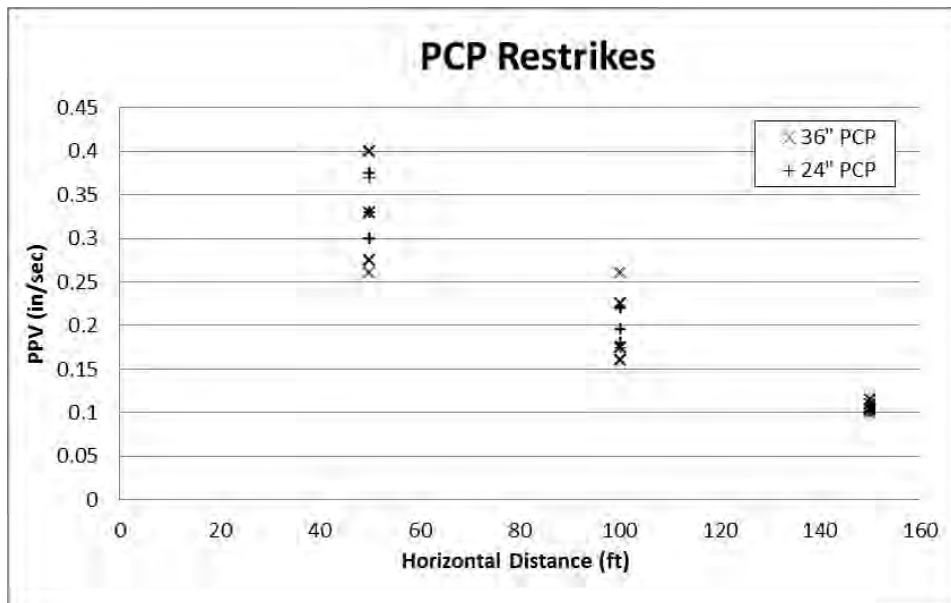


Figure 6: Data plot of restrikes on precast concrete piles (PCP)

## Prediction Equation

The second major objective of this project was to develop a methodology to predict the vibration level at various distances from the pile location. Since the primary use of this research is for determining the vibration levels for piles typically used by ALDOT located at or near the project site, two prediction equations were developed. The equations are based on the maximum peak particle velocities while driving the 36-inch concrete pile and the H-piles. Both equations are based on Equation 3, as presented by Hendriks (2002), where the vibration attenuation parameter ( $k$ ) was determined with the experimental data. Equation 6 was developed to predict vibrations for 36 inch concrete pile,

$$PPV = 0.15 \left( \frac{150}{d} \right)^{1.6}, \quad (6)$$

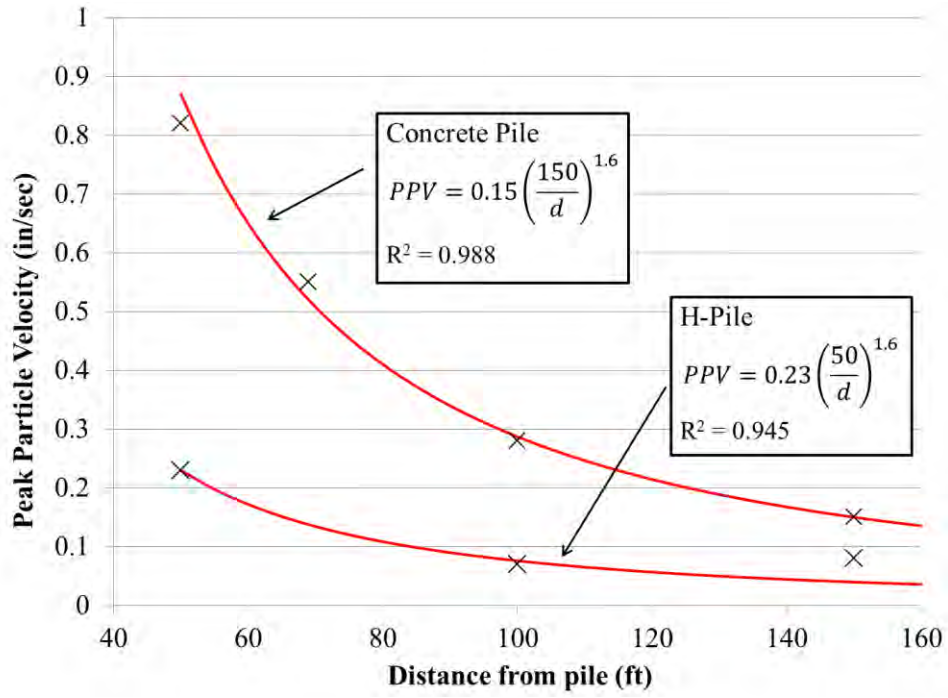
and Equation 7 was developed to predict vibrations for the H-piles,

$$PPV = 0.23 \left( \frac{50}{d} \right)^{1.6}, \quad (7)$$

where, in both equations,  $PPV$  is the peak particle velocity at distance ( $d$ ) in inches per second and  $d$  is the distance from the pile in feet.

Figure 7 shows a plot of the experimental data and the peak particle velocities based on the prediction equation. The results indicate that the prediction equation model fit the experimental data well. However, due to the unusual increase in vibration magnitude at 150 feet for the H-piles, the prediction equation under-predicts the vibration magnitude at 150 feet. It was also noted that the soil attenuation parameter ( $k$ ) for both equations was determined to be 1.6. This was expected since the parameter is primarily dependent on the soil properties and less dependent on the pile type or hammer energy.





**Figure 7:** Peak particle velocity versus distance

## CONCLUSIONS

The experimental data shows that the largest vibrations occurred during the installation of the 36-inch concrete pile, which was recorded as 0.82 inches per second. According to the research presented in Table 2 (Hendriks 2002), a vibration level of 0.82 inches per second has the potential to cause structural damage to an adjacent structure. However, this vibration was recorded at a distance of 50 feet from the pile; the vibration level at 100 feet from the pile was reduced to 0.275 inches per second. This vibration level could cause potential architectural damage to buildings constructed with plaster, but would not likely cause structural damage. At 150 feet the vibration levels were reduced to 0.15 inches per second, a level that would have little to no risk of damage to adjacent structures.

Based on the experimental data and a thorough review of the literature, it is recommended that a maximum vibration level of 0.5 inches per second for modern structures and 0.1 inches per second for potentially sensitive structures be allowed for construction activity at or near the location of the project site. These vibration levels are the allowable levels at the location of the structure. To determine if any structures should be surveyed and monitored for potential vibration damage, a survey distance of 150 feet for modern structures and 250 feet for potentially sensitive structures should be established. The monitoring distances should be measured from the source of the vibration. The ground vibration prediction equation that was developed would estimate a peak particle velocity of 0.15 inches per second at 150 feet and 0.07 inches per second at 250 feet. The survey distances are well beyond the distance where the prediction equation would estimate vibration levels of 0.5 and 0.1 inches per second and therefore would represent conservative survey distances to ensure adjacent structures are not damaged.

### **Recommendations for Future Research**

The research presented in this report contains detailed analysis for a particular location in the state of Alabama; however, data has not been collected and analyzed for other regions of the state with differing soil conditions. A state wide research project should be initiated to determine vibration propagation and attenuation criteria for soil conditions located throughout the state. This data could be used to develop prediction equations that could be used in project planning. Additionally, the results of this research could be used to develop model vibration specifications for the state of Alabama.

In addition to the research mentioned above, it is recommended that a vibration monitoring program be developed for any large scale construction projects in urban environments. These programs could be used not only to ensure the construction activity is not damaging nearby structures, but to ensure the public that the DOT is proactive in preventing damage.

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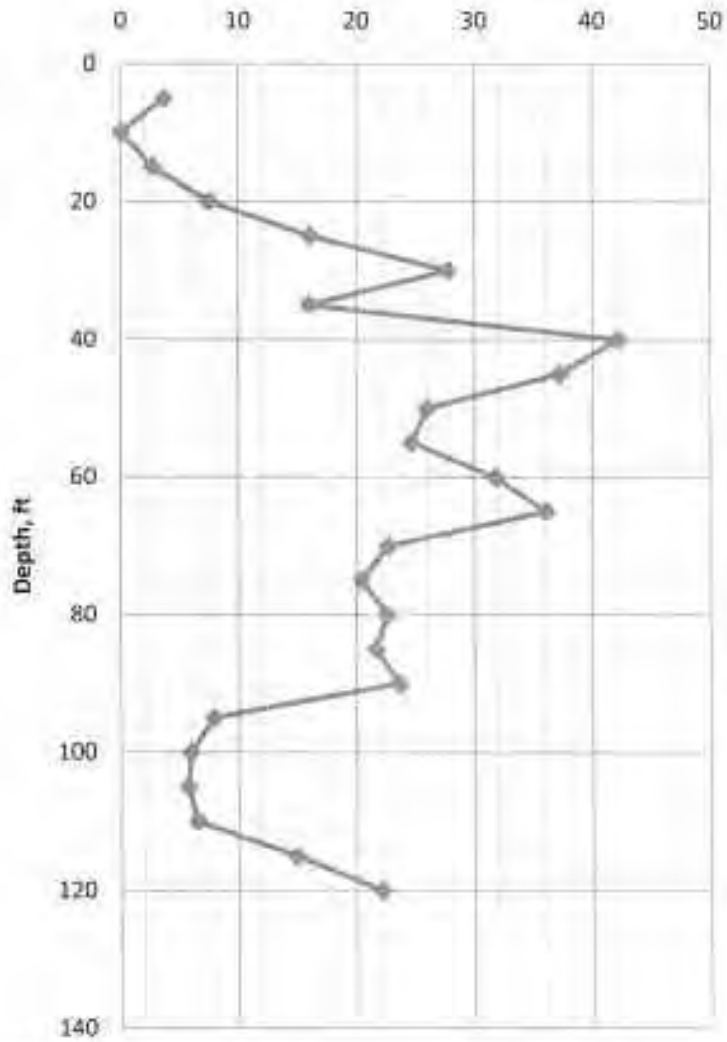
## **Appendix A: Soil Reports**

Two soil investigations were performed at the site. The first was a Standard Penetration Test (SPT), which was performed at two locations. The first location, labeled B-1 in the documents that follow, was located at a property owned by ALDOT that is several hundred feet to the west of the project site. This location was an alternate location for testing. The second location, labeled B-2, was at the project site in the vicinity of where the test piles were installed. The SPT test was performed by an ALDOT drill crew.

The second soil investigation performed was a Seismic Cone Penetration Test (SCPT). Two locations were also investigated, both on the project site. The first test was performed at the location of the test piles and the second was located at 100 to 120 feet from the test piles. The results of both investigations are included here. The SCPT was conducted by Southern Earth Sciences.

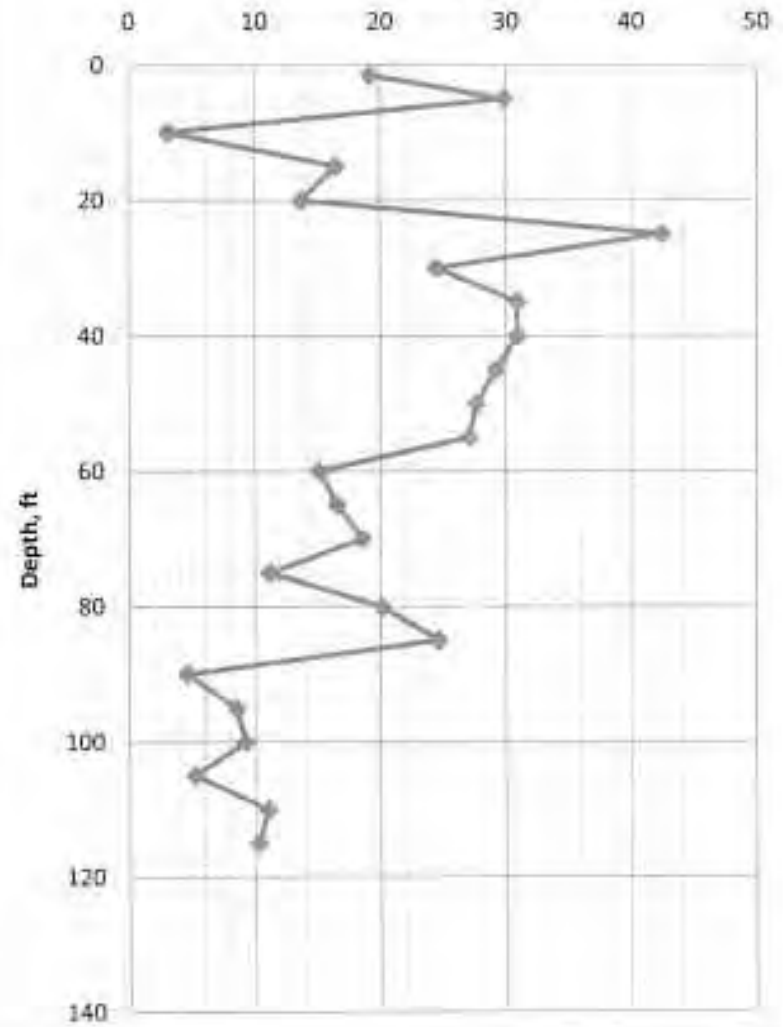
### B-1

#### $N_{160}$ Corrected Blow Count



### B-2

#### $N_{160}$ Corrected Blow Count



Project# DPI-0030 (005) Division 9<sup>th</sup>

Station \_\_\_\_\_ Offset \_\_\_\_\_ Ft \_\_\_\_\_

Ground Elev. \_\_\_\_\_ Water Elev. in Hole \_\_\_\_\_

Depth of Strata BOR# B-1 Visual BOR Loc. \_\_\_\_\_

From	To	Consistency or Density	Approx. Moisture	Color	Basic Matl.
0.0	0.3	Asphalt			
0.3	18.5	U. loose	Moist	Br	br sand
18.5	23.5	Loose	Moist	Br	SAND
23.5	38.5	Med	Moist	TAN	SAND
38.5	63.5	Dense	Moist	TAN	sand
63.5	68.5	Very Dense	Moist	TAN	Sand
68.5	93.5	Dense	Moist	Tan	Sand
93.5	108.5	stiff	Moist	Gray	CLAY
108.5	118.5	HARD	Moist	Gray	Clay
118.5	120.0	DENSE	Moist	Gray	SAND

Remarks by Driller Installed well monitor  
 GPS Cord. 42' 1"  
 LAT. Water -10' 3" 24hrs  
 LONG. \_\_\_\_\_

County Mobile Date 8-2-12

C/L Driller Young/Evans

Type Drill Used SE 9050 Total Hole Depth 120.0

Identification CME 5504 2.25 Hollow Stem

Other Pertinent Components	Sample No.	Penetration or Sample Elev.		N Blow			N Value
		From	To	5	1.0	1.5	
	* 1-A	3.5	5.0	1	1	1	2
W/ clay	* 1-B	8.5	10.0	W	0	H	UOH
	* 1-C	13.5	15.0	W	1	1	2
	1-D	18.5	20.0	1	2	4	6
	1-E	23.5	25.0	5	5	9	14
	1-F	28.5	30.0	10	12	14	26
	1-G	33.5	35.0	9	7	9	16
W/Sand	* 1-H	38.5	40.0	26	23	22	45
W/Sand	* 1-I	43.5	45.5	23	23	19	42
large moll	1-J	48.5	50.0	11	14	11	21
	1-K	53.5	55.0	9	16	15	31
	1-L	58.5	60.0	18	20	22	42
	* 1-M	63.5	65.0	20	23	27	50
	* 1-N	68.5	70.0	14	16	17	33
	1-O	73.5	75.0	7	15	16	31

2.25" Hollow Stem



Project# DPT-0030 (005) Division 9th

Station \_\_\_\_\_ Offset \_\_\_\_\_ Ft \_\_\_\_\_

Ground Elev. \_\_\_\_\_ Water Elev. in Hole \_\_\_\_\_

Depth of Strata BOR# B-1 Visual BOR Loc. \_\_\_\_\_

From To Consistency or Density Approx. Moisture Color Basic Matl.

From	To	Consistency or Density	Approx. Moisture	Color	Basic Matl.

Remarks by Driller \_\_\_\_\_  
GPS Cord. \_\_\_\_\_  
LAT. \_\_\_\_\_  
LONG. \_\_\_\_\_

County Mobile Date 8-8-12

C/L Driller Young/Evans

Type Drill Used SE 9050 Total Hole Depth 120.0

Identification CME 550X 2.25 14160 324

Other Pertinent Components Sample No. Penetration or Sample Elev. From To 'N' Blow 5 1.0 1.5 'N' Value

Other Pertinent Components	Sample No.	Penetration or Sample Elev. From	To	'N' Blow 5	1.0	1.5	'N' Value
	1-P	78.5	80.0	12	17	19	36
	1-Q	83.5	85.0	12	18	18	36
	1-R	88.5	90.0	17	22	19	41
	* 1-S	93.5	95.0	3	6	8	14
	1-T	98.5	100.0	5	5	6	11
	1-U	103.5	105.0	4	5	6	11
	* 1-V	108.5	110.0	3	6	7	13
	* 1-W	113.5	115.0	6	15	16	31
	* 1-X	118.5	120.0	13	21	27	48

- 1-S HAS 4 JAR samples at C/L

Project# DP1-0030 (005) Division 9+2

Station \_\_\_\_\_ Offset \_\_\_\_\_ Ft \_\_\_\_\_

Ground Elev. \_\_\_\_\_ Water Elev. in Hole \_\_\_\_\_

Depth of Strata BOR# B-2 Visual BOR Loc. \_\_\_\_\_

From	To	Consistency or Density	Approx. Moisture	Color	Basic Matl.
0.0	0.2	Topsoil	—		
0.2	3.5	Loose	Moist	Br	Sand
3.5	8.5	Med	Moist	Br	Sand
8.5	13.5	Loose	Moist	Br	Sand
13.5	23.5	Med	Moist	Gray	Sand
23.5	28.5	Dense	Moist	Tan	Sand
28.5	33.5	Med	Moist	Tan	Sand
33.5	58.5	Dense	Moist	Tan	Sand
58.5	78.5	Med	Moist	Tan	Sand
78.5	89.5	Dense	Moist	Tan	Sand
89.5	93.5	Stiff	Moist	Gray	Clay
93.5	103.5	Stiff	Moist	Gray	Clay
103.5	108.5	Stiff	Moist	Gray	Clay
108.5	115.0	Med	Moist	Gray	Sand

Remarks by Driller \_\_\_\_\_

GPS Cord. \_\_\_\_\_

LAT. \_\_\_\_\_

LONG. \_\_\_\_\_

County Mobile Date 8-9-12

C/L Driller Turner/Evan

Type Drill Used SE 9050 Total Hole Depth 115.0

Identification CNF 550X 2.25 Hollow Stem

Other Pertinent Components	Sample No.	Penetration or Sample Elev.		'N' Blow			'N' Value
		From	To	5	1.0	1.5	
	*2-A	0.0	1.5	9	4	4	8
w/Gravel	*2-B	3.5	5.0	5	7	9	16
w/ORG MATL	*2-C	8.5	10.0	1	1	1	2
	*2-D	13.5	15.0	2	5	7	12
	2-E	18.5	20.0	5	4	7	11
	*2-F	23.5	25.0	16	19	19	37
	2-G	28.5	30.0	10	11	12	23
	*2-H	33.5	35.0	7	15	16	31
	2-I	38.5	40.0	7	13	20	33
	2-J	43.5	45.0	9	14	19	33
w/SAND	2-K	48.5	50.0	7	15	18	33
ORG MATL	2-L	53.5	55.0	10	16	18	34
	*2-M	58.5	60.0	10	10	10	20
	2-N	63.5	65.0	6	12	11	23
	2-O	68.5	70.0	12	17	10	27

\* JAC Sample  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Project# \_\_\_\_\_ Division \_\_\_\_\_

Station \_\_\_\_\_ Offset \_\_\_\_\_ Ft \_\_\_\_\_

Ground Elev. \_\_\_\_\_ Water Elev. in Hole \_\_\_\_\_

Depth of Strata BOR# B-2 Visual BOR Loc. \_\_\_\_\_

From	To	Consistency or Density	Approx. Moisture	Color	Basic Matl.

Remarks by Driller \_\_\_\_\_

GPS Cord. \_\_\_\_\_

LAT. \_\_\_\_\_

LONG. \_\_\_\_\_

County \_\_\_\_\_ Date 8-9-12

C/L Driller \_\_\_\_\_

Type Drill Used \_\_\_\_\_ Total Hole Depth 115'

Identification

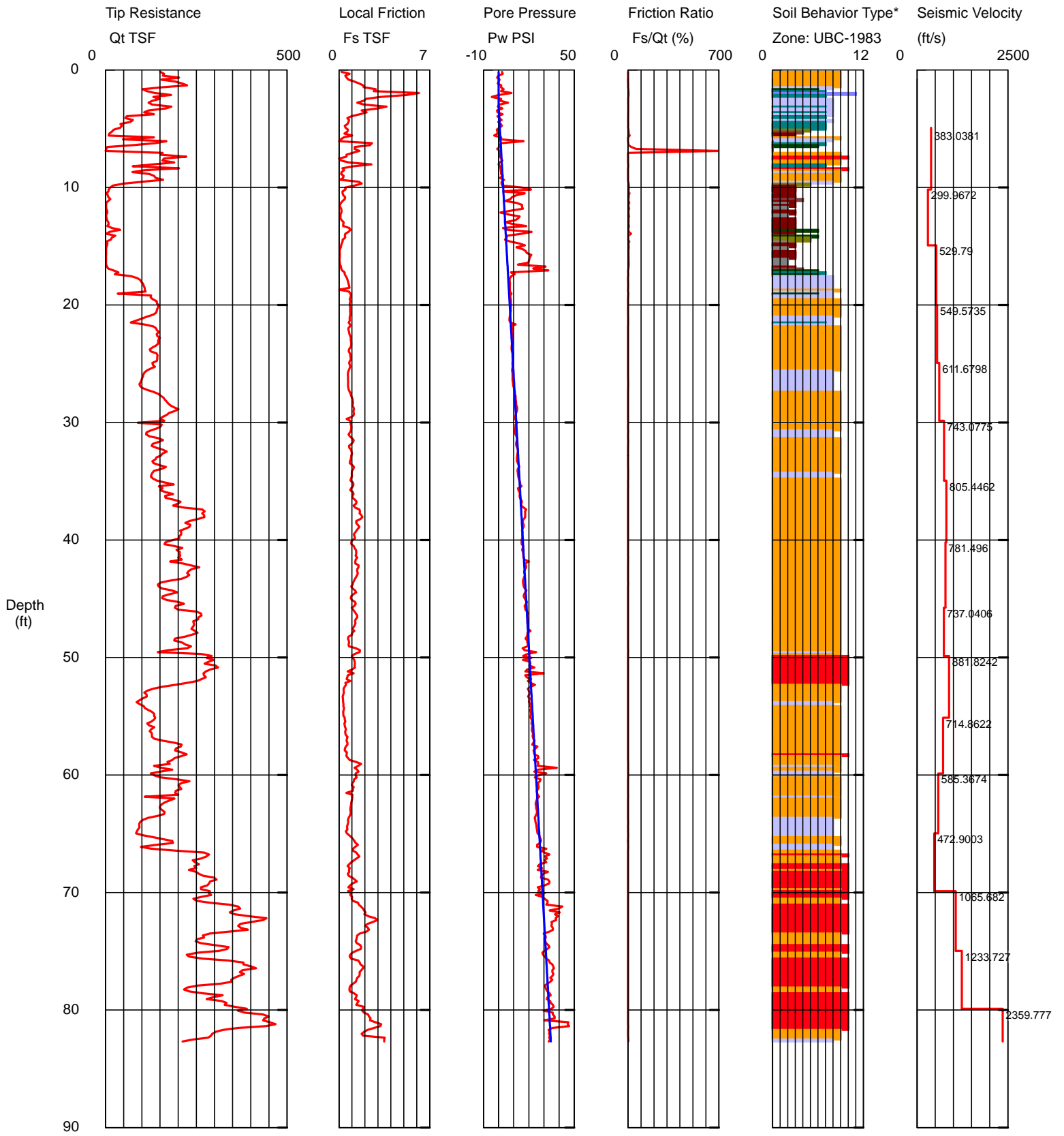
Other Pertinent Components	Sample No.	Penetration or Sample Elev.		'N' Blow			'N' Value
		From	To	5	10	15	
	2-1	73.5	75	6	7	10	17
	* 2-0	78.5	80	6	11	21	32
	2-R	83.5	85.0	12	18	23	41
	* 2-6	88.5	90.0	4	4	4	8
	* 2-7	93.5	95.0	6	7	8	15
	* 2-U	98.5	100.0	1	9	8	17
	* 2-V	103.5	105.0	7	4	6	10
	* 2-W	108.5	110.0	5	9	13	22
	2-X	113.5	115.0	6	9	13	22

2-7 HAS 3 Jars  
 2-U HAS 2 Jars  
 2-V HAS 3 Jars

# Southern Earth Sciences

Operator: Mike Wright  
 Sounding: SCPT-1  
 Cone Used: DDG0892

CPT Date/Time: 8/14/2013 9:08:56 AM  
 Location: Test Pile Evaluation  
 Job Number: 13-000



Maximum Depth = 82.68 feet

Depth Increment = 0.164 feet

- |                          |                             |                            |                                |
|--------------------------|-----------------------------|----------------------------|--------------------------------|
| 1 sensitive fine grained | 4 silty clay to clay        | 7 silty sand to sandy silt | 10 gravelly sand to sand       |
| 2 organic material       | 5 clayey silt to silty clay | 8 sand to silty sand       | 11 very stiff fine grained (*) |
| 3 clay                   | 6 sandy silt to clayey silt | 9 sand                     | 12 sand to clayey sand (*)     |

Groundwater measured at 3.1'

N30.68546 W88.03791

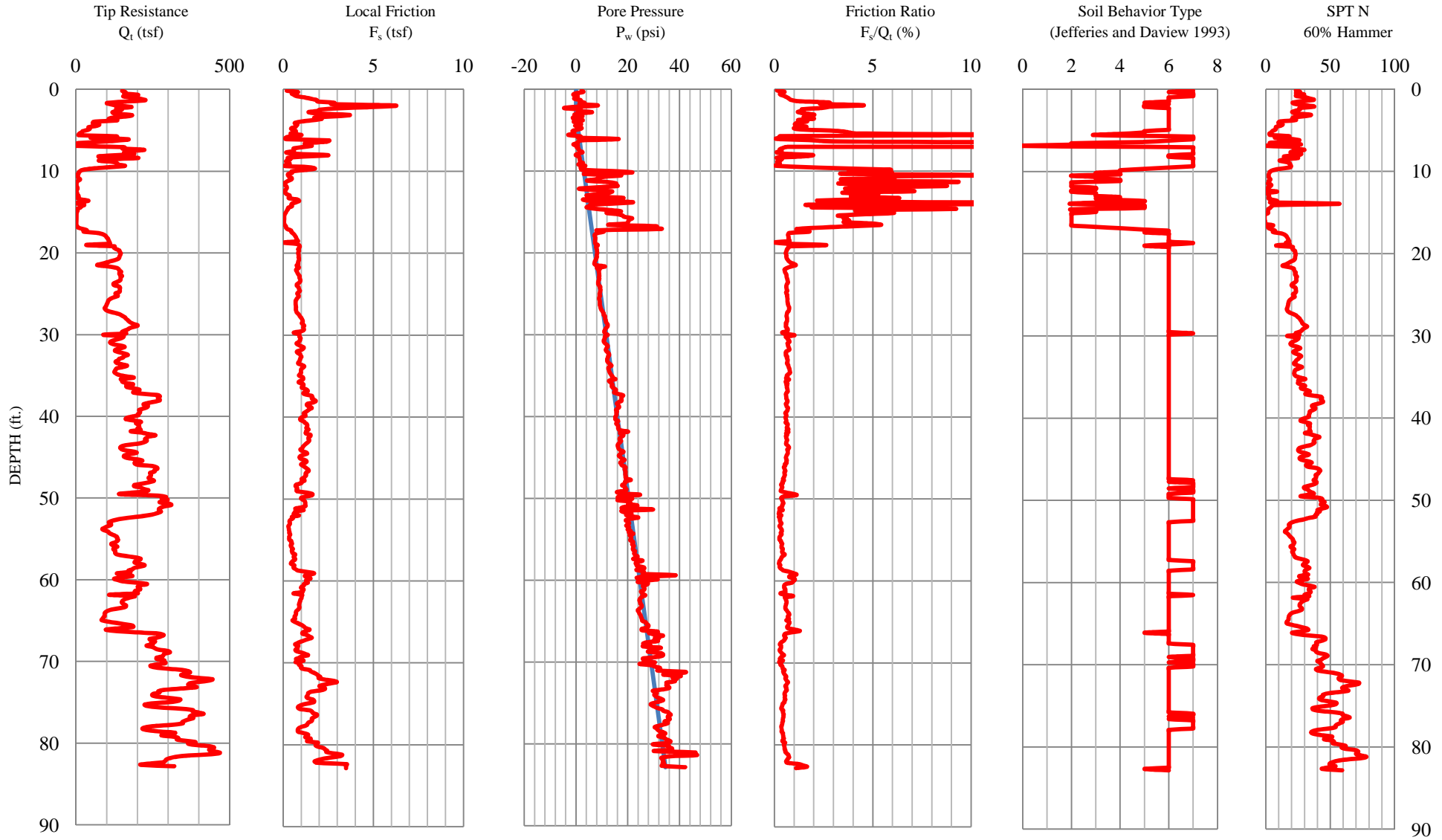
\*Soil behavior type and SPT based on data from UBC-1983

# CONE PENETRATION TEST LOG

**Project Name:** Test Pile Evaluation  
**Project No.:** 13-000  
**Sounding:** SCPT-1

**Cone Used:** DDG0892  
**Operator:** Mike Wright  
**CPT Date:** 8/14/2013

**Groundwater Level:** 3.1 feet  
**Elevation:** Unknown  
**Lat/Long:** N30.68546 W88.03791



**Baseline Data:**

	$Q_t$ (tsf)	$F_s$ (tsf)	$P_w$ (psi)
Initial Baseline:	0	0	0
Final Baseline:	-0.602	0.002	-0.172

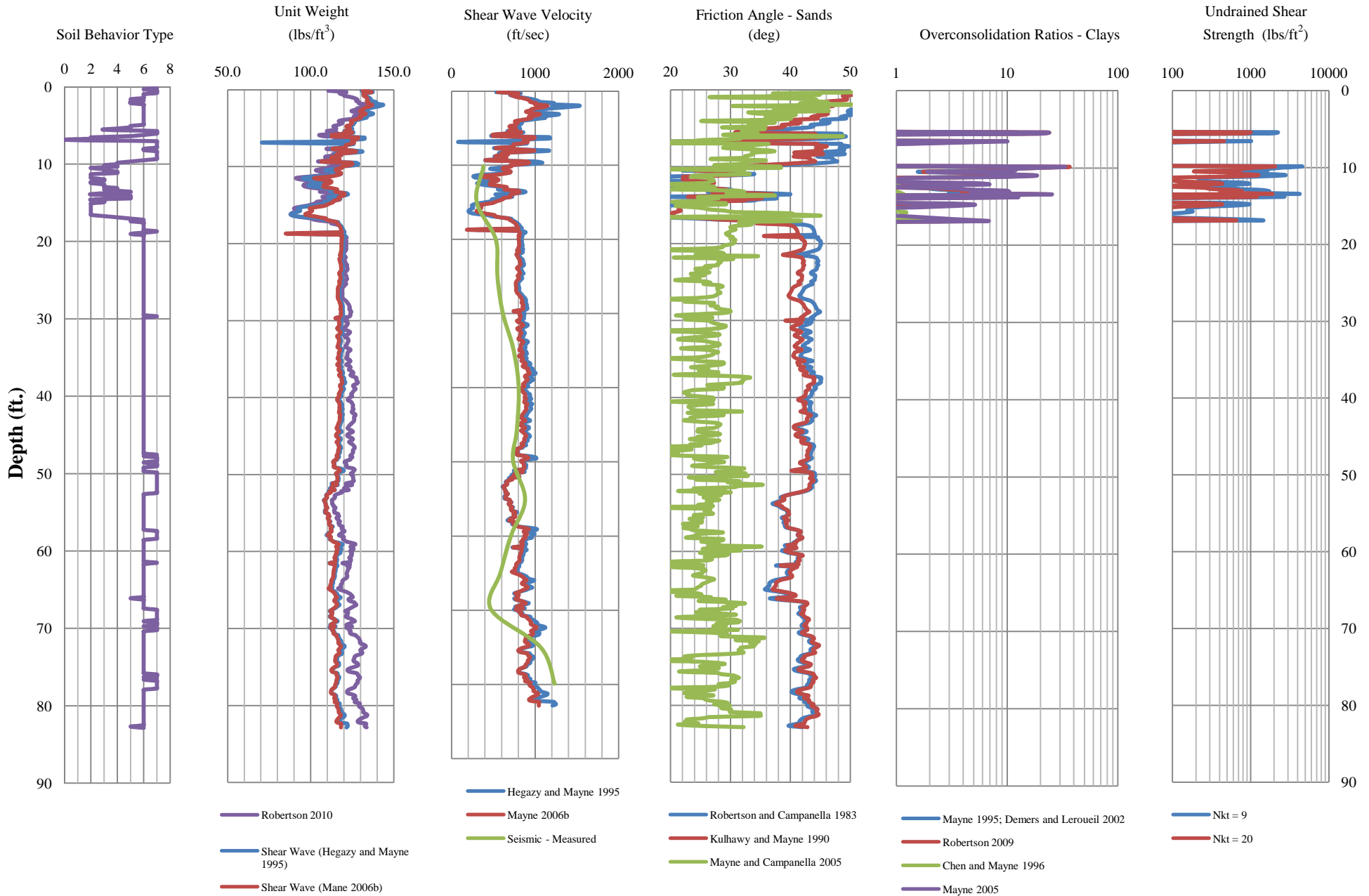
SPT N, SOIL BEHAVIOR TYPE, OR ZONE NUMBER FROM CPT CLASSIFICATION INDEX,  $I_c$   
 Organic Clay Soils = 2, Clays = 3, Silt Mixtures = 4, Sand Mixtures = 5, Sands = 6, Gravelly Sands = 7

# CONE PENETRATION TEST LOG

**Project Name:** Test Pile Evaluation  
**Project No.:** 13-000  
**Sounding:** SCPT-1

**Cone Used:** DDG0892  
**Operator:** Mike Wright  
**CPT Date:** 8/14/2013

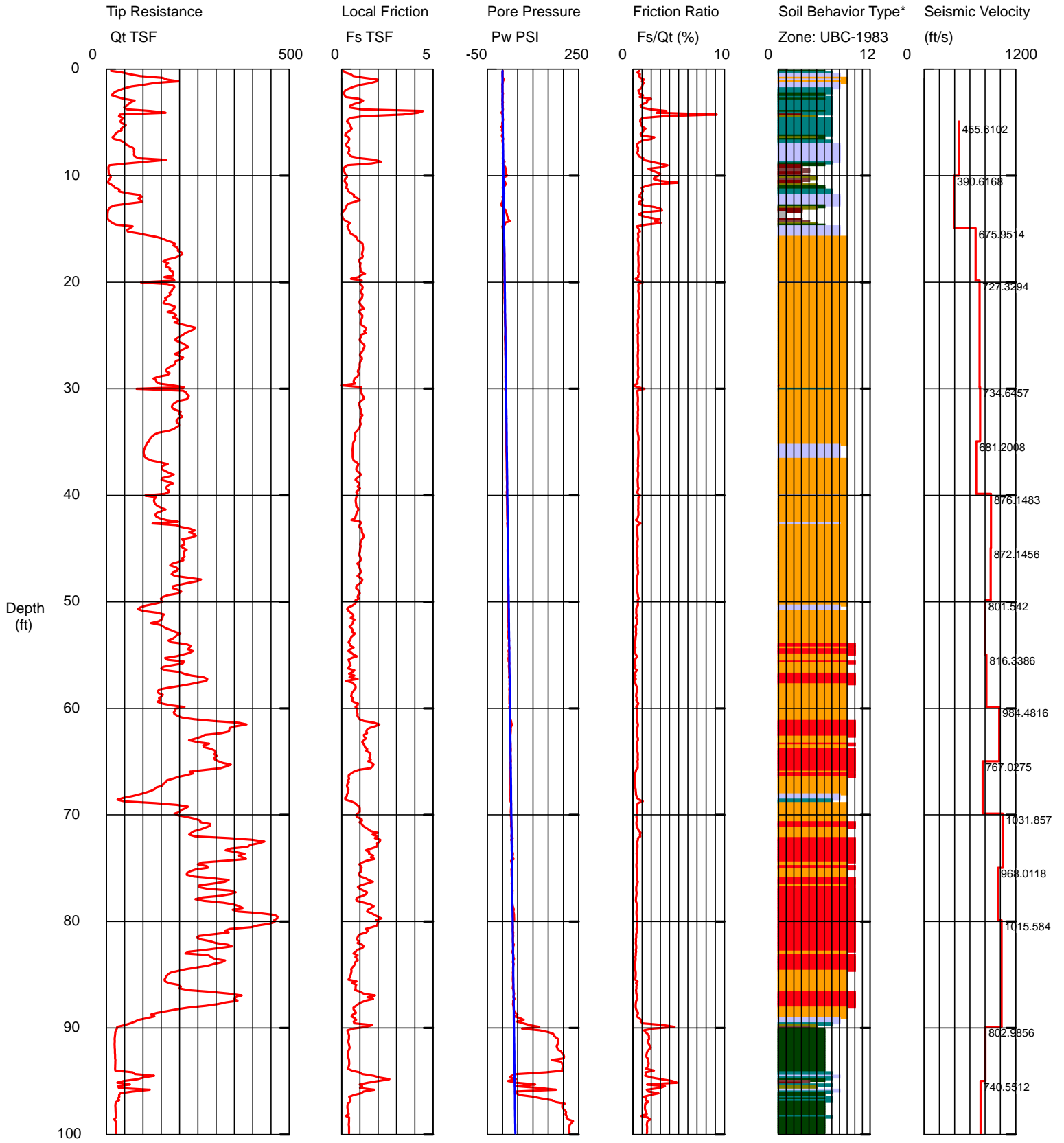
**Groundwater Level:** 3.1 feet  
**Elevation:** Unknown  
**Lat/Long:** N30.68546 W88.03791



# Southern Earth Sciences

Operator: Mike Wright  
 Sounding: SCPT-2  
 Cone Used: DDG0892

CPT Date/Time: 8/14/2013 10:35:15 AM  
 Location: Test Pile Evaluation  
 Job Number: 13-000



Maximum Depth = 99.90 feet

Depth Increment = 0.164 feet

- 1 sensitive fine grained
  - 2 organic material
  - 3 clay
- Groundwater measured at 3.2'

- 4 silty clay to clay
- 5 clayey silt to silty clay
- 6 sandy silt to clayey silt

- 7 silty sand to sandy silt
- 8 sand to silty sand
- 9 sand

- 10 gravelly sand to sand
- 11 very stiff fine grained (\*)
- 12 sand to clayey sand (\*)

N30.68541 W88.03821

\*Soil behavior type and SPT based on data from UBC-1983



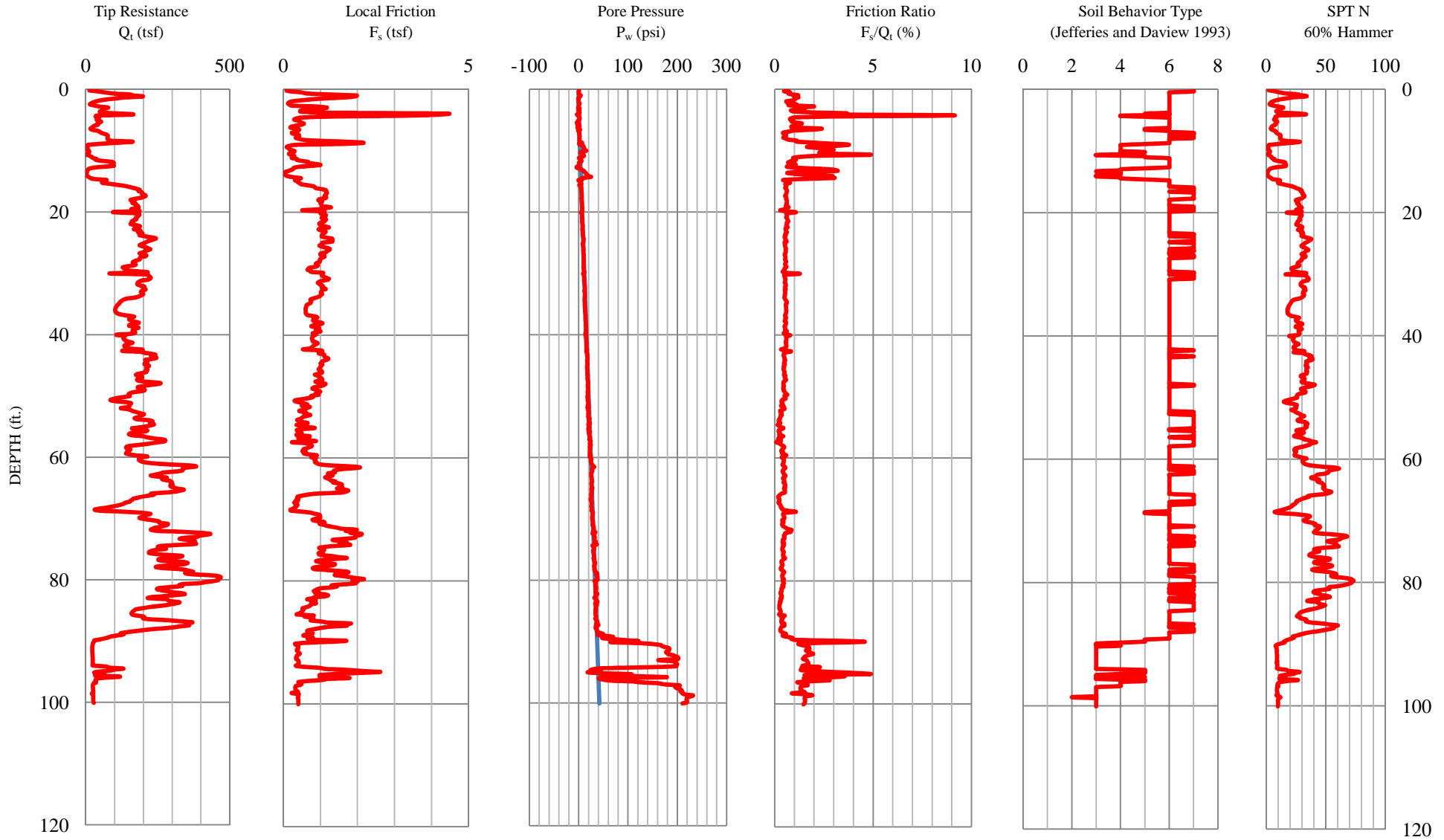
# CONE PENETRATION TEST LOG



**Project Name:** Test Pile Evaluation  
**Project No.:** 13-000  
**Sounding:** SCPT-2

**Cone Used:** DDG0892  
**Operator:** Mike Wright  
**CPT Date:** 8/14/2013

**Groundwater Level:** 3.2 feet  
**Elevation:** Unknown  
**Lat/Long:** N30.68541 W88.03821



**Baseline Data:**

	Q <sub>t</sub> (tsf)	F <sub>s</sub> (tsf)	P <sub>w</sub> (psi)
Initial Baseline:	0	0	0
Final Baseline:	0.357	0.012	0.210

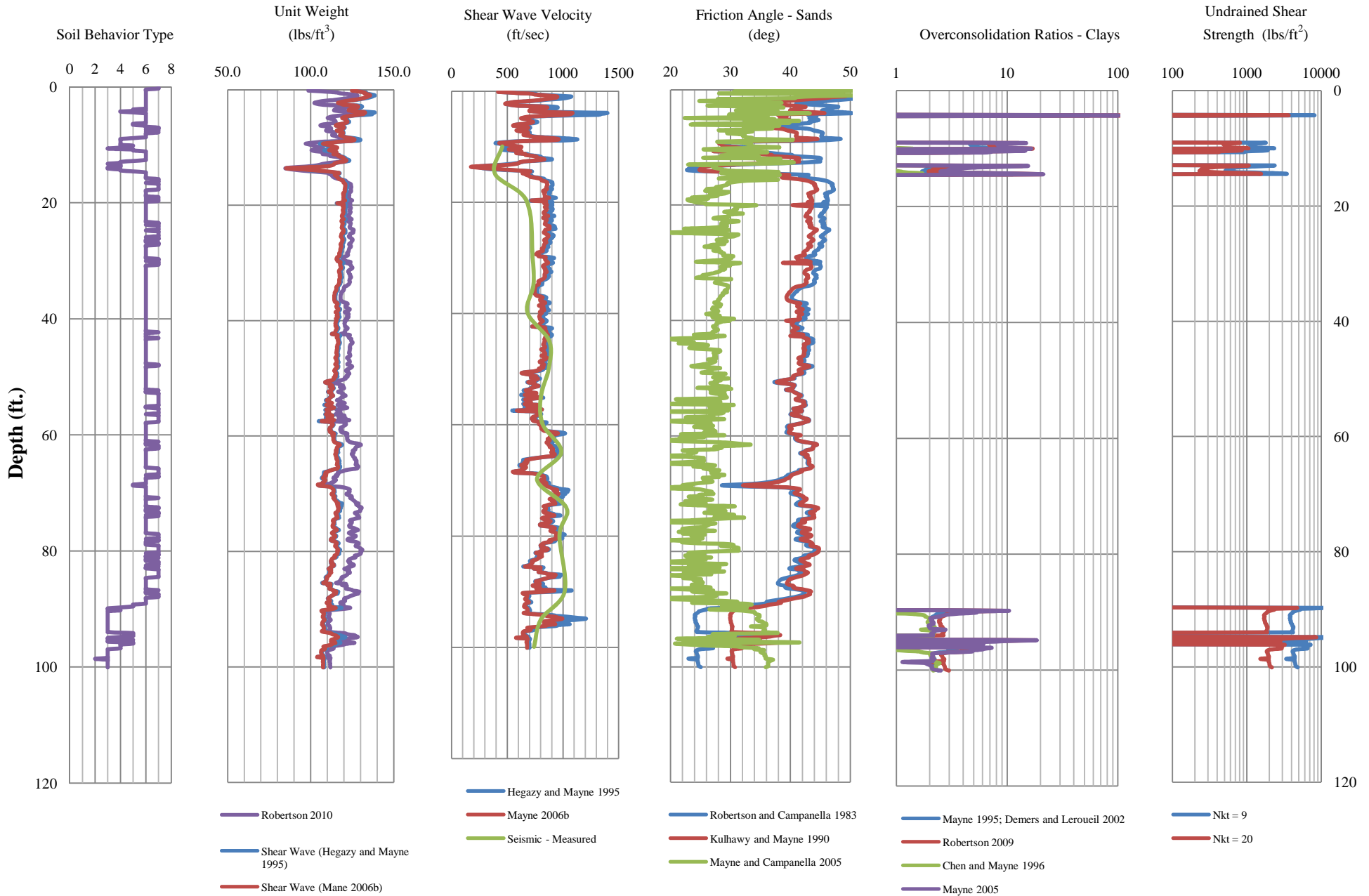
SPT N, SOIL BEHAVIOR TYPE, OR ZONE NUMBER FROM CPT CLASSIFICATION INDEX, I<sub>c</sub>  
 Organic Clay Soils = 2, Clays = 3, Silt Mixtures = 4, Sand Mixtures = 5, Sands = 6, Gravelly Sands = 7

# CONE PENETRATION TEST LOG

**Project Name:** Test Pile Evaluation  
**Project No.:** 13-000  
**Sounding:** SCPT-2

**Cone Used:** DDG0892  
**Operator:** Mike Wright  
**CPT Date:** 8/14/2013

**Groundwater Level:** 3.2 feet  
**Elevation:** Unknown  
**Lat/Long:** N30.68541 W88.03821



## Appendix B: Pile Driving Hammer Information

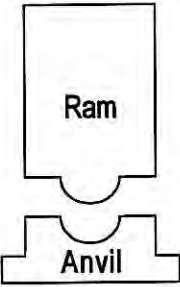
	Fuel Setting #1	Fuel Setting #2	Fuel Setting #3	Fuel Setting #4
<b>Concrete Piles used Delmag Model D-62-22 Single Acting Diesel Hammer</b>				
<b><u>36 in PCP</u></b>				
Setting Usage	Down to 43 feet	43 to 45 feet	45 to 48 feet	48 feet to end Restrikes
Rated Energy	78,960 ft. lbs.	109,725 ft. lbs.	138,960 ft. lbs.	165,000 ft. lbs
<b><u>24 in PCP</u></b>				
Setting Usage	Down to 61 feet	61 feet to end Restrikes	N/A	N/A
Rated Energy	78,960 ft. lbs.	109,725 ft. lbs.		
<b>Steel Piles used APE Model D30-42 Single Acting Diesel Hammer</b>				
<b><u>HP 14</u></b>				
Setting Usage	N/A	N/A	Entire depth Restrikes	N/A
Rated Energy			66,977 ft. lbs.	
<b><u>HP 12</u></b>				
Setting Usage	N/A	Entire depth Restrikes	N/A	N/A
Rated Energy		55,070 ft. lbs		


FORM C-14 **ALABAMA DEPARTMENT OF TRANSPORTATION**  
 Revised 08-07-95 **PILE AND DRIVING EQUIPMENT DATA FORM**

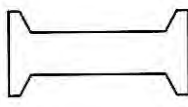
Project Number USA Test Pile & Vibration	County Mobile	Division 9th
---------------------------------------------	------------------	-----------------


Pile Driving Contractor or Subcontractor Jordan Pile Driving Inc.	Bridge Identification Number N/A
----------------------------------------------------------------------	-------------------------------------

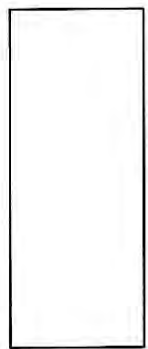
Details of access method to pile top for dynamic testing are:  Attached  Not Applicable

<b>Hammer Components</b>		<b>Hammer</b>	Manufacturer: <u>Delmag</u> Model: <u>D-62-22</u> Type: <u>S.A. Diesel</u> Serial No.: <u>238</u> Rated Energy: <u>165,000</u> (ft.-lbs.) at <u>11.3</u> (ft.) Length of Stroke Modifications: <u>Adjustable Fuel Pump</u> Pump Setting 1 <u>78,960 ft. lbs.</u> Pump Setting 2 <u>109,725 ft. lbs.</u> Pump Setting 3 <u>136,950 ft. lbs.</u> Pump Setting 4 <u>165,000 ft. lbs.</u>
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	<b>Capblock (Hammer Cushion)</b>	Material: <u>Aluminum &amp; Micarta Alternating</u> Thickness: <u>6</u> (in.) Area: <u>381</u> (in. <sup>2</sup> ) Modulus of Elasticity - E : <u>450 KSI</u> (P.S.I.) Coefficient of Restitution - e : <u>0.8</u>
-----------------------------------------------------------------------------------	----------------------------------	-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

	<b>Pile Cap</b>	<table border="1" style="display: inline-table;"> <tr> <td>Helmet</td> <td><input checked="" type="checkbox"/></td> </tr> <tr> <td>Bonnet</td> <td><input type="checkbox"/></td> </tr> <tr> <td>Anvil Block</td> <td><input type="checkbox"/></td> </tr> <tr> <td>Drivehead</td> <td><input type="checkbox"/></td> </tr> </table> Weight : <u>10,000</u> (lbs.) Note: Should include weight of striker plate.	Helmet	<input checked="" type="checkbox"/>	Bonnet	<input type="checkbox"/>	Anvil Block	<input type="checkbox"/>	Drivehead	<input type="checkbox"/>
Helmet	<input checked="" type="checkbox"/>									
Bonnet	<input type="checkbox"/>									
Anvil Block	<input type="checkbox"/>									
Drivehead	<input type="checkbox"/>									

	<b>Pile Cushion</b>	Cushion Material: <u>Plywood</u> Thickness: <u>10</u> (in.) Area: <u>576</u> (in. <sup>2</sup> ) Modulus of Elasticity - E : <u>45 KSI</u> (P.S.I.) Coefficient of Restitution - e : <u>0.5</u>
-------------------------------------------------------------------------------------	---------------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

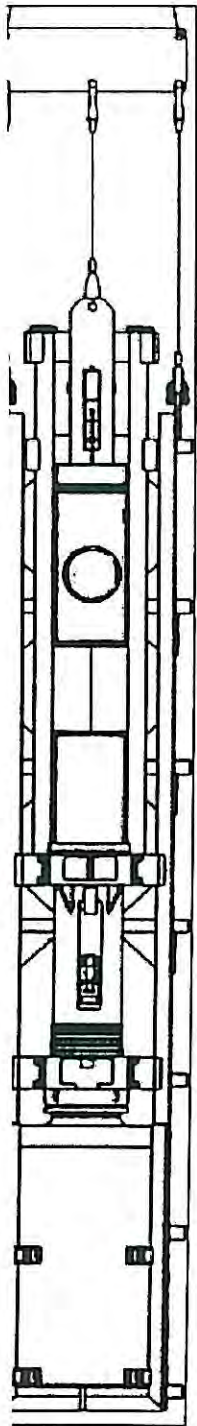
	<b>Pile</b>	Pile Type: <u>36" x 36" &amp; 24" x 24" Prestressed Concrete Test Pile</u> Length (in Leads): <u>89' &amp; 81'</u> (ft.) Weight / Ft: <u>936 &amp; 510</u> (lbs./ft.) Wall Thickness: <u>NA</u> (in.) Taper: <u>NA</u> Cross Sectional Area: <u>489 &amp; 898</u> (in <sup>2</sup> ) Design Pile Capacity: _____ (Tons) Description of Splice: <u>N/A</u> Tip Treatment Description: <u>N/A</u>
-------------------------------------------------------------------------------------	-------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

Note: If mandrel is used to drive this pile, attach separate manufacturer's detail sheet(s) including weight and dimensions.

Submitted By: Davis Daniel Date: \_\_\_\_\_

## Model D62-22 Diesel Hammer

Maximum obtainable energy	203,216 ft-lbs
Maximum obtainable stroke	178 inches
Pump setting 1: (minimum)	78,956 ft-lbs
Pump setting 2:	109,749 ft-lbs
Pump setting 3:	137,186 ft-lbs
Pump setting 4: (maximum)	164,250 ft-lbs
Stroke at rated energy	135 inches
Energy at rated stroke	165,000 ft-lbs
Speed (blows per minute)	36-50
Ram	13,700 lbs
Anvil	2,833 lbs
Hammer weight (includes trip device)	29,491 lbs
Typical operating (weight with drive cap)	32,963 lbs
Fuel tank (runs on diesel or bio-diesel)	25.86 gal
Oil tank	8.32 gal
Weight	1100 lbs
Diameter	25 inches
Thickness	8 inches
Type	Monocast MC 901
Diameter	25 inches
Thickness	2 inches
Elastic-modulus	285 kips per square inch
Coeff. of restitution	0.8
Weight (fits 8 by 26 inch leads)	1,350 lbs
Diesel or Bio-diesel fuel	5.28 gal/hr
Lubrication oil	0.84 gal/hr
**Grease twice per day	
Length overall	232.6 inches
Length over cylinder extension	272.0 inches
Impact block diameter	27.9 inches
Width over bolts	32.6 inches
Hammer width overall	31.5 inches
Width for guiding- face to face	22.0 inches
Hammer center to pump guard	19.3 inches
Hammer center to bolt center	15.0 inches
Hammer depth overall	38.2 inches
Minimum clearance for leads	19.7 inches

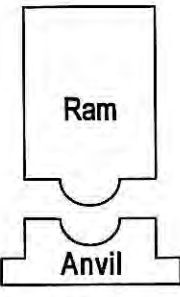



FORM C-14 **ALABAMA DEPARTMENT OF TRANSPORTATION**  
 Revised 08-07-95 **PILE AND DRIVING EQUIPMENT DATA FORM**

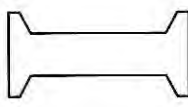
Project Number USA Test Pile & Vibration	County Mobile	Division 9th
---------------------------------------------	------------------	-----------------


Pile Driving Contractor or Subcontractor Jordan Pile Driving Inc.	Bridge Identification Number N/A
----------------------------------------------------------------------	-------------------------------------

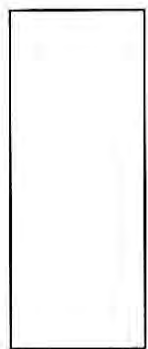
Details of access method to pile top for dynamic testing are:  Attached  Not Applicable

<b>Hammer Components</b>		<b>Hammer</b>	Manufacturer: <u>APE</u> Model: <u>D30-42</u> Type: <u>S.A. Diesel</u> Serial No.: _____ Rated Energy: <u>74,419</u> (ft.-lbs.) at <u>11.25</u> (ft.) Length of Stroke  Modifications: <u>Adjustable Fuel Pump</u> <table style="width:100%; border-collapse: collapse;"> <tr> <td style="width:70%;">Pump Setting 1</td> <td style="width:30%;">37,209 ft. lbs.</td> </tr> <tr> <td>Pump Setting 2</td> <td>55,070 ft. lbs.</td> </tr> <tr> <td>Pump Setting 3</td> <td>66,977 ft. lbs.</td> </tr> <tr> <td>Pump Setting 4</td> <td>74,419 ft. lbs.</td> </tr> </table>	Pump Setting 1	37,209 ft. lbs.	Pump Setting 2	55,070 ft. lbs.	Pump Setting 3	66,977 ft. lbs.	Pump Setting 4	74,419 ft. lbs.
	Pump Setting 1	37,209 ft. lbs.									
Pump Setting 2	55,070 ft. lbs.										
Pump Setting 3	66,977 ft. lbs.										
Pump Setting 4	74,419 ft. lbs.										

	<b>Capblock (Hammer Cushion)</b>	Material: <u>Aluminum &amp; Micarta Alternating</u> Thickness: <u>4</u> (in.) Area: <u>398</u> (in. <sup>2</sup> ) Modulus of Elasticity - E : <u>285</u> (P.S.I.) Coefficient of Restitution - e : <u>0.8</u>
-----------------------------------------------------------------------------------	----------------------------------	-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

	<b>Pile Cap</b>	Helmet <input checked="" type="checkbox"/> Bonnet <input type="checkbox"/> Anvil Block <input type="checkbox"/> Drivehead <input type="checkbox"/>	Weight : <u>1,704</u> (lbs.) Note: Should include weight of striker plate.
-------------------------------------------------------------------------------------	-----------------	-------------------------------------------------------------------------------------------------------------------------------------------------------------	-------------------------------------------------------------------------------

	<b>Pile Cushion</b>	Cushion Material: <u>N/A</u> Thickness: <u>N/A</u> (in.) Area: <u>N/A</u> (in. <sup>2</sup> ) Modulus of Elasticity - E : <u>N/A</u> (P.S.I.) Coefficient of Restitution - e : <u>N/A</u>
-------------------------------------------------------------------------------------	---------------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

	<b>Pile</b>	Pile Type: <u>HP 12 x 53 &amp; HP 14 x117</u> Length (in Leads): <u>70' &amp; 106'</u> (ft.) Weight / Ft: <u>53 &amp; 117</u> (lbs./ft.) Wall Thickness: <u>N/A</u> (in.) Taper: <u>NA</u> Cross Sectional Area: _____ (in. <sup>2</sup> ) Design Pile Capacity: _____ (Tons) Description of Splice: <u>Mechanical</u>  Tip Treatment Description: _____
-------------------------------------------------------------------------------------	-------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

Note: If mandrel is used to drive this pile, attach separate manufacturer's detail sheet(s) including weight and dimensions.

Submitted By: \_\_\_\_\_ Date: \_\_\_\_\_

Davis Daniel



# APE Model D30-42 Single Acting Diesel Impact Hammer

D30-42 Finishing Dolphin Piles.



## MODEL D30-42 (3.0 metric ton ram)

### SPECIFICATIONS

Stroke at maximum rated energy	135 in (343 cm)
Maximum rated energy (Setting 4)	74,419 ft-lbs (100.47 kNm)
Setting 3	66,977 ft-lbs (90.42 kNm)
Setting 2	55,070 ft-lbs (74.34 kNm)
Minimum rated energy (Setting 1)	37,209 ft-lbs (50.23 kNm)
<i>(Variable throttle allows for infinite fuel settings)</i>	

Maximum obtainable stroke	157 in (381 cm)
Maximum obtainable energy	86,546 ft-lbs (117 kNm)
Speed (blows per minute)	34-53

### WEIGHTS

Ram	6,615 lbs (3,000 kg)
Anvil	1,358 lbs (616 kg)
Anvil cross sectional area	367.94 in <sup>2</sup> (2373.80 cm <sup>2</sup> )
Hammer weight (includes trip device)	13,571 lbs (6,154 kg)
Typical operating (weight with DB26 and H-beam insert)	16,223 lbs (7,357 kg)

### CAPACITIES

Fuel tank (runs on diesel or bio-diesel)	17.4 gal (65 liters)
Oil tank	5 gal (19 liters)

### CONSUMPTION

Diesel or Bio-diesel fuel	2.6 gal/hr (9.84 liters/hr)
Lubrication	0.26 gal/hr (1 liters/hr)
Grease	8 to 10 pumps every 45 minutes of operation time.

Optional Variable Throttle Control.



### STRIKER PLATE FOR DB 26

Weight	628 lbs (284 kg)
Diameter	22.5 in (57.15 cm)
Area	398 in <sup>2</sup> (2567.74 cm <sup>2</sup> )
Thickness	6 in (15.24 cm)

### CUSHION MATERIAL

Type/Qty	Micarta / 2 each
Diameter-DB26	22.5 in (57.15 cm)
Thickness	1 in (25.4 mm)

Drive Base Assembly.



Type/Qty	Aluminum / 3 each
Thickness	1/2 in (12.7 mm)
Diameter	22.5 in (57.15 cm)
Total Combined Thickness	3.5 in (8.89 cm)
Area	398 in <sup>2</sup> (2567.74 cm <sup>2</sup> )
Elastic-modulus	285 ksi (1,965 mpa)
Coeff. of restitution	0.8

### DRIVE CAP

DB 26:	1,076 lbs (488 kg)
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### INSERT WEIGHT

H-Beam insert for 12" (305 mm) and 14" (355 mm):	948 lbs (430 kg)
Large pipe insert for sizes 12" to 24" diameter:	1,830 lbs (830 kg)

### MINIMUM BOX LEAD SIZE/OPERATING LENGTH

Minimum box leader size	8 in x 26 in (20.32 cm x 66 cm)
Operating length as described above	354 in (900 cm)



Corporate Offices  
7032 South 196th  
Kent, Washington 98032 USA  
(800) 248-8498 & (253) 872-0141  
(253) 872-8710 Fax

Visit our WEB site:  
[www.apevibro.com](http://www.apevibro.com)  
e-mail: [ape@apevibro.com](mailto:ape@apevibro.com)

*Note: All specifications are subject to change without notice 08/20/2012*



# **APPENDIX N**

## **Tribal Consultation**

**From:** timothy.heisler@dot.gov  
**To:** [missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)  
**Subject:** FW: DPI-0030(005), I-10 Mobile River Bridge and Bayway Widening, Mobile & Baldwin Counties, State of Alabama  
**Date:** Friday, June 29, 2018 8:43:32 AM

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FYI

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**From:** Heisler, Timothy (FHWA)  
**Sent:** Wednesday, August 20, 2014 4:22 PM  
**To:** Kenneth H Carleton ([kcarleton@choctaw.org](mailto:kcarleton@choctaw.org)) <[kcarleton@choctaw.org](mailto:kcarleton@choctaw.org)>; Lindsey D Bilyeu ([lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)) <[lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)>; Pare Bowlegs <[pbowlegs@outlook.com](mailto:pbowlegs@outlook.com)>; Robert Glenn Thrower ([rthrower@pci-nsn.gov](mailto:rthrower@pci-nsn.gov)) <[rthrower@pci-nsn.gov](mailto:rthrower@pci-nsn.gov)>; Alison Swing ([AlisonSwing@semtribe.com](mailto:AlisonSwing@semtribe.com)) <[AlisonSwing@semtribe.com](mailto:AlisonSwing@semtribe.com)>; celestine.bryant@actribe.org; chascolemans75@yahoo.com; Earl Barbry Jr. ([earlii@tunica.org](mailto:earlii@tunica.org)) <[earlii@tunica.org](mailto:earlii@tunica.org)>; Emman Spain ([espain@mcn-nsn.gov](mailto:espain@mcn-nsn.gov)) <[espain@mcn-nsn.gov](mailto:espain@mcn-nsn.gov)>; Geoffrey Wasson <[GeoffreyWasson@semtribe.com](mailto:GeoffreyWasson@semtribe.com)>; jjacobs@mcn-nsn.gov; Kara Gann ([kara.gann@kialegetribe.net](mailto:kara.gann@kialegetribe.net)) <[kara.gann@kialegetribe.net](mailto:kara.gann@kialegetribe.net)>; Lisa C Baker ([ukbthpo-larue@yahoo.com](mailto:ukbthpo-larue@yahoo.com)) <[ukbthpo-larue@yahoo.com](mailto:ukbthpo-larue@yahoo.com)>; Michael Tarpley ([kokua.aina57@gmail.com](mailto:kokua.aina57@gmail.com)) <[kokua.aina57@gmail.com](mailto:kokua.aina57@gmail.com)>; Natalie Harjo ([harjo.n@sno-nsn.gov](mailto:harjo.n@sno-nsn.gov)) <[harjo.n@sno-nsn.gov](mailto:harjo.n@sno-nsn.gov)>; rdushane@estoo.net; Russell Townsend ([russtown@nc-choke.com](mailto:russtown@nc-choke.com)) <[russtown@nc-choke.com](mailto:russtown@nc-choke.com)>; United Keetoowah Band of Cherokee Indians in Oklahoma <[lstapleton@unitedkeetoowahband.org](mailto:lstapleton@unitedkeetoowahband.org)>  
**Cc:** Urquhart, Lynne (FHWA) ([Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov)) <[Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov)>; Dunn, Heather M. ([dunnh@dot.state.al.us](mailto:dunnh@dot.state.al.us)) <[dunnh@dot.state.al.us](mailto:dunnh@dot.state.al.us)>  
**Subject:** DPI-0030(005), I-10 Mobile River Bridge and Bayway Widening, Mobile & Baldwin Counties, State of Alabama

The Draft Environmental Impact Statement (DEIS) for the Mobile River Bridge was signed on July 22, 2014. FHWA would like to make you aware that three important meetings are upcoming:

1. Consulting party meeting at 9:00 a.m. on Sep 23, 2014, ALDOT's 9<sup>th</sup> Division Training Room, 1701 West I-65 Service Rd North, Mobile, AL 36618
2. Public involvement meeting at 4:00 pm on Sep 23, Mobile Cruise Terminal, 201 South Water St, Mobile, AL 36002
3. Public involvement meeting at 4:00 pm on Sep 29, 5 Rivers Delta Resource Center, 30945 Five Rivers Blvd, Spanish Fort, AL 36527

A copy of DEIS may be found at <http://www.mobileriverbridge.com/> or if you would like a CD or hard copy, please let me know.

Thanks,

***Tim Heisler***  
**Transportation Engineer**  
**Federal Highway Administration – Alabama Division**  
**9500 Wynlakes Place**  
**Montgomery, Alabama 36117**  
**Phone: [334.274.6370](tel:334.274.6370)**  
**[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)**

**From:** Heisler, Timothy (FHWA)  
**To:** ["Lindsey Bilyeu"](#)  
**Cc:** [Urquhart, Lynne \(FHWA\) \(Lynne.Urquhart@dot.gov\)](#)  
**Subject:** RE: DPI-0030(005), I-10 Mobile River Bridge and Bayway Widening, Mobile & Baldwin Counties, State of Alabama  
**Date:** Friday, September 19, 2014 10:02:00 AM

---

Lindsey,

The CD of the DEIS was put in the mail on Tuesday. The call in number for the meeting is 251-470-8392 and please dial in at 8:45am CST.

Let me know if you need any additional information.

***Tim Heisler***  
**Transportation Engineer**  
**Federal Highway Administration – Alabama Division**  
**9500 Wynlakes Place**  
**Montgomery, Alabama 36117**  
**Phone: [334.274.6370](tel:334.274.6370)**  
**[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)**

---

**From:** Lindsey Bilyeu [<mailto:lbilyeu@choctawnation.com>]  
**Sent:** Wednesday, September 10, 2014 2:08 PM  
**To:** Heisler, Timothy (FHWA)  
**Subject:** RE: DPI-0030(005), I-10 Mobile River Bridge and Bayway Widening, Mobile & Baldwin Counties, State of Alabama

Tim,

Please send our office a CD of the DEIS. Also, will there be a call in number available for the Sep. 23<sup>rd</sup> meeting?

Thank You,

Lindsey D. Bilyeu  
Senior Section 106 Reviewer  
Historic Preservation Department  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74701  
580-924-8280 ext. 2631

---

**From:** [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov) [<mailto:timothy.heisler@dot.gov>]  
**Sent:** Wednesday, August 20, 2014 4:22 PM  
**To:** [kcarleton@choctaw.org](mailto:kcarleton@choctaw.org); Lindsey Bilyeu; [pbowlegs@outlook.com](mailto:pbowlegs@outlook.com); [rthrower@pci-nsn.gov](mailto:rthrower@pci-nsn.gov);

[AlisonSwing@semtribe.com](mailto:AlisonSwing@semtribe.com); [celestine.bryant@actribe.org](mailto:celestine.bryant@actribe.org); [chascoleman75@yahoo.com](mailto:chascoleman75@yahoo.com);  
[earlii@tunica.org](mailto:earlii@tunica.org); [espain@mcn-nsn.gov](mailto:espain@mcn-nsn.gov); [GeoffreyWasson@semtribe.com](mailto:GeoffreyWasson@semtribe.com); [jjacobs@mcn-nsn.gov](mailto:jjacobs@mcn-nsn.gov);  
[kara.gann@kialegeetribe.net](mailto:kara.gann@kialegeetribe.net); [ukbthpo-larue@yahoo.com](mailto:ukbthpo-larue@yahoo.com); [kokua.aina57@gmail.com](mailto:kokua.aina57@gmail.com); [harjo.n@sno-nsn.gov](mailto:harjo.n@sno-nsn.gov);  
[rdushane@estoo.net](mailto:rdushane@estoo.net); [russtown@nc-chokeee.com](mailto:russtown@nc-chokeee.com); [lstapleton@unitedkeetoowahband.org](mailto:lstapleton@unitedkeetoowahband.org)

**Cc:** [Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov); [dunnh@dot.state.al.us](mailto:dunnh@dot.state.al.us)

**Subject:** DPI-0030(005), I-10 Mobile River Bridge and Bayway Widening, Mobile & Baldwin Counties, State of Alabama

The Draft Environmental Impact Statement (DEIS) for the Mobile River Bridge was signed on July 22, 2014. FHWA would like to make you aware that three important meetings are upcoming:

1. Consulting party meeting at 9:00 a.m. on Sep 23, 2014, ALDOT's 9<sup>th</sup> Division Training Room, 1701 West I-65 Service Rd North, Mobile, AL 36618
2. Public involvement meeting at 4:00 pm on Sep 23, Mobile Cruise Terminal, 201 South Water St, Mobile, AL 36002
3. Public involvement meeting at 4:00 pm on Sep 29, 5 Rivers Delta Resource Center, 30945 Five Rivers Blvd, Spanish Fort, AL 36527

A copy of DEIS may be found at <http://www.mobileriverbridge.com/> or if you would like a CD or hard copy, please let me know.

Thanks,

***Tim Heisler***

**Transportation Engineer**

**Federal Highway Administration – Alabama Division**

**9500 Wynlakes Place**

**Montgomery, Alabama 36117**

**Phone: [334.274.6370](tel:334.274.6370)**

**[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)**

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**From:** Lindsey Bilyeu  
**To:** [Heisler, Timothy \(FHWA\)](#)  
**Subject:** RE: DPI-0030(005) Mobile River Bridge and Bayway Widening, Mobile and Baldwin Co.'s, AL  
**Date:** Thursday, October 16, 2014 11:13:38 AM

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Mr. Heisler,

The Choctaw Nation of Oklahoma thanks the FHWA, Alabama Division, for the correspondence regarding the above referenced project. Mobile and Baldwin Co.'s, AL lie in the Choctaw Nation of Oklahoma's area of historic interest. Our office has reviewed the DEIS and we will need some additional information before we are able to comment on these projects.

Since Mobile and Baldwin Co's lie in the Choctaw Nation of Oklahoma's homelands, would you please send our office the project's GIS shapefiles so that we can view the project in relation to known Choctaw sites? Also, it is my understanding that the archaeological sites that were found during the project survey are all 19<sup>th</sup> to 20<sup>th</sup> century sites, is this correct? If you have any questions, please contact me at 580-924-8280 ext. 2631.

Thank You,

Lindsey D. Bilyeu  
NHPA Senior Section 106 Reviewer  
Historic Preservation Department  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74701  
580-924-8280 ext. 2631

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**From:** timothy.heisler@dot.gov  
**To:** [missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)  
**Subject:** FW: I-10 MRB request from Choctaw Indians, Archaeological Sites 1MB410, 1MB411, 1MB412, 1MB498 and 1MB499  
**Date:** Friday, June 29, 2018 8:25:42 AM  
**Attachments:** [NRHP Eligible Archaeology Sites.zip](#)  
[How to UNZIP.html](#)  
[SecureZIP Attachments.zip](#)

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**From:** Heisler, Timothy (FHWA)  
**Sent:** Thursday, November 20, 2014 1:32 PM  
**To:** Lindsey D Bilyeu ([lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)) <[lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)>  
**Cc:** Urquhart, Lynne (FHWA) ([Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov)) <[Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov)>; [turnerw@dot.state.al.us](mailto:turnerw@dot.state.al.us); 'buddy.covington@volkert.com' <[buddy.covington@volkert.com](mailto:buddy.covington@volkert.com)>; Acoff, Alfredo <[acoffa@dot.state.al.us](mailto:acoffa@dot.state.al.us)>; Clay, Natasha <[clayn@dot.state.al.us](mailto:clayn@dot.state.al.us)>  
**Subject:** FW: I-10 MRB request from Choctaw Indians, Archaeological Sites 1MB410, 1MB411, 1MB412, 1MB498 and 1MB499

Lindsey,

Please see the attached requested information for I-10 Mobile River Bridge. If you need any additional information please don't hesitate to contact me.

Thanks,

***Tim Heisler***  
Transportation Engineer  
Federal Highway Administration – Alabama Division  
9500 Wynlakes Place  
Montgomery, Alabama 36117  
Phone: [334.274.6370](tel:334.274.6370)  
[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)

---

**From:** Dunn, Heather M. [<mailto:dunnh@dot.state.al.us>]  
**Sent:** Tuesday, October 21, 2014 3:50 PM  
**To:** Heisler, Timothy (FHWA)  
**Cc:** Urquhart, Lynne (FHWA); Turner, William; Acoff, Alfredo; Clay, Natasha; Covington, Buddy  
**Subject:** FW: I-10 MRB request from Choctaw Indians, Archaeological Sites 1MB410, 1MB411, 1MB412, 1MB498 and 1MB499

Tim,

Please see attached for the information the Choctaw Indians requested on I-10 MRB. Please provide ETS and Volkert back a copy of correspondence from your office to the THPO so that we can add to the FEIS.'

Thanks,  
Heather

---

**From:** Covington, Buddy [<mailto:buddy.covington@volkert.com>]

**Sent:** Tuesday, October 21, 2014 2:59 PM

**To:** Turner, William

**Cc:** Acoff, Alfredo; Dunn, Heather M.; Clay, Natasha; Overstreet, Christy; Goffinet, Jason; Overstreet, Jerald; Gaar, Brett; Nichols, Kenneth; Webber, David

**Subject:** RE: I-10 MRB request from Choctaw Indians, Archaeological Sites 1MB410, 1MB411, 1MB412, 1MB498 and 1MB499

Bill,

Attached are the shape files for the five (5) archaeological sites identified thus far along the I-10 Mobile River Bridge project.

Five archaeological sites (1MB410, 1MB411, 1MB412, 1MB498, and 1MB499) considered eligible for listing on the NRHP based on Criterion D were identified during archaeological surveys conducted on land. Under Criterion D, a property has yielded, or has the potential to yield, information important to prehistory or history. There is a probability of intact historic-period features, such as structure foundations, refuse pits, and privies, associated with the late nineteenth and early twentieth-century occupations at each of the sites. Earlier archaeological surveys were conducted for Alternative A under the previous EA in 2003. Alternative A has no archaeological impacts. Alternatives B and B' (Preferred) impact site 1MB412. Alternative C impacts sites 1MB410, 1MB411, 1MB498, and 1MB499.

SHPO concurred in their correspondence dated November 15, 2012 and June 30, 2014, that sites 1MB410, 1MB411, 1MB412, 1MB498, and 1MB499 are considered eligible for listing on the NRHP.

These 5 sites are all expected to contain historic-period features.

SHPO concurred that there are no significant submerged resources along Alternatives A, B, B' (Preferred), and C in their correspondence dated May 22, 2002, and August 9, 2006.

Let Jerald Overstreet or me know if you need any additional information.

Please provide us a copy of what is sent to the Choctaw for documentation in the FEIS.

Thanks

Buddy Covington  
Vice President  
Environmental Services  
Volkert, Inc



(251) 342-1070 Office  
(251) 316-3854 Fax  
[buddy.covington@volkert.com](mailto:buddy.covington@volkert.com)  
[www.volkert.com](http://www.volkert.com)

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**From:** Dunn, Heather M. [<mailto:dunnh@dot.state.al.us>]  
**Sent:** Friday, October 17, 2014 4:16 PM  
**To:** Covington, Buddy  
**Cc:** Turner, William; Acoff, Alfredo  
**Subject:** request from Choctaw Indians, I-10

Buddy,

Can you guys provide us with shape files for I-10 MRB and bayway that show any known archeological sites please? The Choctaw Indians have requested them to overlay with their known sites. Send this directly to Bill Turner and copy myself and Alfredo on the email. Can you please get them to us by the middle of next week at the latest?

Thanks,

Heather Dunn, P.E.  
Assistant Environmental Coordinator  
ALDOT Design Bureau/ETS  
(334) 242-6147

**From:** Lindsey Bilyeu  
**To:** [Heisler, Timothy \(FHWA\)](#)  
**Subject:** RE: DPI-0030(005), Mobile River Bridge & Bayway Widening, Mobile & Baldwin Counties, AL  
**Date:** Wednesday, February 25, 2015 9:25:17 AM

---

Mr. Heisler,

The Choctaw Nation of Oklahoma thanks the FHWA Alabama Division for the correspondence regarding the above referenced project. Mobile and Baldwin Co.'s, AL lie in the Choctaw Nation's area of historic interest. The Choctaw Nation is unaware of any cultural or sacred sites located in the immediate project area. The Choctaw Nation Historic Preservation Department concurs that the project should proceed as planned. However, as the project lies in an area of historic interest to the Tribe, we ask that work be stopped and our office contacted immediately in the event that Native American artifacts or human remains are encountered.

If you have any questions, please contact me at 580-924-8280 ext. 2631.

Thank you,

Lindsey D. Bilyeu  
NHPA Senior Section 106 Reviewer  
Historic Preservation Department  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74701  
580-924-8280 ext. 2631  
[lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)

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**From:** timothy.heisler@dot.gov  
**To:** [missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)  
**Subject:** FW: DPI-0030(005), Mobile River Bridge Section 106 Determination of Effects, Mobile & Baldwin Counties, State of Alabama  
**Date:** Friday, June 29, 2018 8:27:37 AM  
**Attachments:** [0030005 FHWA Sect 106 ltr to consulting parties signed 6.25.2015.pdf](#)

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**From:** Heisler, Timothy (FHWA)

**Sent:** Tuesday, June 30, 2015 10:28 AM

**To:** Catherine Foreman-Gray (catherine-gray@cherokee.org) <catherine-gray@cherokee.org>; celestine.bryant@actribe.org; Charles Coleman - Thlopthlocco Tribal Town (thpo@tttown.org) <thpo@tttown.org>; Earl Barbry Jr. (earlii@tunica.org) <earlii@tunica.org>; Emman Spain (espain@mcn-nsn.gov) <espain@mcn-nsn.gov>; jjacobs@mcn-nsn.gov; Kara Gann (kialegeettcpo@gmail.com) <kialegeettcpo@gmail.com>; Lisa C Baker (ukbthpo-larue@yahoo.com) <ukbthpo-larue@yahoo.com>; Michael Tarpley (kokua.aina57@gmail.com) <kokua.aina57@gmail.com>; Molly Franks (aqhpo@mail.com) <aqhpo@mail.com>; Natalie Harjo (harjo.n@sno-nsn.gov) <harjo.n@sno-nsn.gov>; rdushane@estoo.net; Tyler B. Howe (tylehowe@nc-choctaw.com) <tylehowe@nc-choctaw.com>; United Keetoowah Band of Cherokee Indians in Oklahoma (lstapleton@unitedkeetoowahband.org); Kenneth H Carleton (kcarleton@choctaw.org) <kcarleton@choctaw.org>; Lindsey D Bilyeu (lbilyeu@choctawnation.com) <lbilyeu@choctawnation.com>; Molly Franks (aqhpo@mail.com) <aqhpo@mail.com>; Robert Glenn Thrower (rthrower@pci-nsn.gov) <rthrower@pci-nsn.gov>

**Cc:** Urquhart, Lynne (FHWA) (Lynne.Urquhart@dot.gov) <Lynne.Urquhart@dot.gov>; Bartlett, Mark (FHWA) <Mark.Bartlett@dot.gov>

**Subject:** DPI-0030(005), Mobile River Bridge Section 106 Determination of Effects, Mobile & Baldwin Counties, State of Alabama

At this time FHWA is working on the Mobile River Bridge project in Mobile and Baldwin Counties. We want to make sure you are up-to-date in regards to Section 106 and this project.

Last week, FHWA signed a letter regarding Section 106 Determination of Effects Revisions. This letter states that a cumulative visual impact may further diminish the settings of the two historic districts, which would be considered an adverse effect. Now discussion will ensue regarding mitigation measures to resolve and/or minimize any adverse impacts, including potential archaeological impacts. Ultimately, mitigation measures will be incorporated in the Memorandum of Agreement.

Should you have any questions about this project, you may contact Mr. Timothy Heisler at (334) 274-6370 or email address at [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov).

***Tim Heisler***  
**Transportation Engineer**  
**Federal Highway Administration – Alabama Division**  
**9500 Wynlakes Place**



U.S. Department  
of Transportation  
**Federal Highway  
Administration**

Alabama Division

June 25, 2015

9500 Wynlakes Place  
Montgomery, AL 36117  
334-274-6350  
334-274-6352  
[Alabama.FHWA@dot.gov](mailto:Alabama.FHWA@dot.gov)

In Reply Refer To:  
HDA-AL

To: All Mobile River Bridge  
Section 106 Consulting Parties

Re: Project DPI-0030(005), Mobile & Baldwin Counties  
I-10 Mobile River Bridge & Bayway Widening  
Section 106 Determination of Effects Revisions  
AHC Reference Numbers: AHC 00-1934; AHC 00-0352

Dear Sir/Madam:

Since the September 23, 2014 Section 106 Consulting Party Meeting, we have been in consultation with the Alabama Historical Commission (ACH) and the Advisory Council on Historic Preservation (ACHP). Based on these discussions and follow-up meetings, we have made some revisions to our Determination of Effects for the referenced project. Attached is our recent correspondence with the AHC.

The primary areas of concern were visual impacts to historic properties within the Church Street East Historic District and the Lower Dauphin Street Historic District. We have determined that there may be an adverse visual effect to these historic districts by the Build Alternatives. The proposed project will be clearly visible from the edges of these historic districts. Due to the extensive modern structures (infill) to the southeast of the historic districts, the visual effects of the project will be limited. However, when the project is combined with the visual effects of the numerous other modern structures within view, a cumulative impact will occur. This cumulative impact may further diminish the settings of these historic districts, which would be an adverse effect. We are hereby notifying you of our finding of additional impacts due to visual changes to the settings of these two historic districts.

We now propose to begin discussing appropriate mitigation measures to resolve and/or minimize any adverse impacts. Ultimately, agreed upon mitigation measures would be included in a Memorandum of Agreement that would not only address adverse visual effects but potential effects to archaeological sites, documentation of the Union Hall, bridge aesthetics, access to the Battleship Park, and measures to avoid impacts from lighting and vibration, as well.

If you have any questions or comments regarding this matter, please contact Ms. Lynne Urquhart of this office at (334) 274-6371 or [lynne.urquhart@dot.gov](mailto:lynne.urquhart@dot.gov).

Sincerely,

Mark D. Bartlett, P. E.  
Division Administrator

MB/lu/af

Attachments:

1. Section 106 Consulting Parties List
2. FHWA Determination Letter May 2015
3. AHC Response June 2015

Section 106 Consulting Party  
Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening  
Baldwin and Mobile Counties  
1 | Page

**OFFICIAL LIST OF CONSULTING PARTIES AS OF 4/24/14 FOR  
MOBILE I-10 AND BAYWAY WIDENING (Project DPI-0030(005) IN  
MOBILE AND BALDWIN COUNTIES**

**Agencies/Organizations that are already Consulting Parties**

Ms. Jean Dodge, President  
Mobile Historic Development Commission  
Post Office Box 1827  
Mobile, AL 36633-1827

Ms. Elizabeth Merritt, Deputy General Counsel  
National Trust for Historic Preservation  
The Watergate Office Building  
2600 Virginia Avenue NW, Suite 1100  
Washington, DC 20037

Mr. John Hildreth, Vice President  
Eastern Field Services  
National Trust for Historic Preservation  
517 Savannah Highway  
Charleston, SC 29407

**Agencies/Organizations included in previous Section 106 Coordination/Consultation**

Ms. Lee Anne Wofford, Deputy State Historic Preservation Officer  
Alabama Historic Commission  
468 South Perry Street  
Montgomery, AL 36130-0900

Mr. Chris Wilson, FHWA Liaison  
Advisory Council on Historic Preservation  
401 F Street NW, Suite 308  
Washington, DC 20001-2637

**Municipalities**

The Honorable Sandy Stimpson, Mayor  
City of Mobile  
Post Office Box 1827  
Mobile, AL 36633-1827

Commissioner Connie Hudson, President  
Mobile County Commission  
205 Government Street  
Mobile, AL 36644-1001

Section 106 Consulting Party  
Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening  
Baldwin and Mobile Counties  
2 | Page

The Honorable Dane Haygood, Mayor  
City of Daphne  
Post Office Box 400  
Daphne, AL 36526

The Honorable Michael M. McMillan, Mayor  
City of Spanish Fort  
Post Office Box 7226  
Spanish Fort, AL 36527

The Honorable Charles F. "Skip" Gruber  
Office of Commission Chairman  
Baldwin County Commission  
201 East Section Avenue  
Foley, AL 36535

**Maritime Property Owners with Potential for Direct Impacts to Eligible Historic Structure(s) and/or District**

Mr. Walter Meigs, Legal Counsel  
BAE Systems/Southeast Shipyards Alabama, LLC  
Post Office Box 3202  
Mobile, AL 36652

Mr. Ray Harris, Facilities Manager  
Signal Shipyard/Bender Shipbuilding & Repair Co.  
601 South Royal Street  
Mobile, AL 36602

Section 106 Consulting Party  
Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening  
Baldwin and Mobile Counties  
3 | Page

**Agreed to be CP by letter 3-23-11 (ALDOT invitation 3-14-11)**

USS Alabama Commission  
Battleship Memorial Park  
Post Office Box 65  
Mobile, AL 36601-0065

**Approved by FHWA via request to become Consulting Party**

Ms. Mary L. Cousar  
6 St. Joseph Street  
Mobile, AL 36602

Ms. Elizabeth S. Sanders  
Downtown Mobile Alliance  
Post Office Box 112  
Mobile, AL 36601

Ms. Caroline Etherton  
Colonial Dames and Conde-Charlotte Museum House  
Post Office Box 1968  
Mobile, AL 36633

Mr. Douglas Burtu Kearley  
Ten Wisteria Avenue  
Mobile, AL 36607

Mr. Herdon Inge  
Post Office Box 40188  
Mobile, AL 36640

Ms. Ann Bedsole  
6 St. Joseph Stree  
Mobile, AL 36602

Reverend Canon Beverly F. Gibson  
Christ Church Cathedral  
115 South Conception Street  
Mobile, AL 36602

Ms. Lauren Vanderbijl  
Historic Mobile Preservation Society  
300 Oakleigh Place  
Mobile, AL 36604



Section 106 Consulting Party  
Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening  
Baldwin and Mobile Counties  
4 | Page

Ms. Katherine Frangos  
Friends of the Museum  
Post Office Box 2068  
Mobile, AL 36602

Restore Mobile, Inc. c/o Mr. Jarrod J. White  
Cabaniss Johnston, LLP  
Mobile Office, Riverview Plaza  
63 South Royal Street, Suite 700  
Mobile, AL 36652



U.S. Department  
of Transportation  
Federal Highway  
Administration

Alabama Division

May 18, 2015

9500 Wynlakes Place  
Montgomery, AL 36117  
334-274-6350  
334-274-6352  
[Alabama.FHWA@dot.gov](mailto:Alabama.FHWA@dot.gov)

In Reply Refer To:  
HDA-AL

Ms. Lee Anne Wofford  
Deputy State Historic Preservation Officer  
Alabama Historical Commission  
468 South Perry Street  
Montgomery, Alabama 36130

Subject: Project DPI-0030(005), Mobile & Baldwin Counties  
I-10 Mobile River Bridge & Bayway Widening  
Section 106 Determination of Effects Revisions

Dear Ms. Wofford:

This letter is in response to your June 30, 2014 and November 4, 2014 letters to the Alabama Department of Transportation (ALDOT) highlighting concerns pertaining to the referenced project. These concerns were discussed in our 2014 Section 106 Consulting Parties Meeting and further conversations with your office and the Advisory Council on Historic Preservation (ACHP). Based on these discussions and follow-up meetings, we have made some revisions to our Determination of Effects for the referenced project.

Specifically, your June letter expressed concerns over visual impacts to historic properties within the Church Street East Historic District and the Lower Dauphin Street Historic District. We have determined that there may be adverse visual effects to these historic districts by the Build Alternatives. The proposed project will be clearly visible from the edges of these historic districts. Due to the extensive modern structures (infill) to the southeast of the historic districts, the visual effects of the project will be limited. However, when the project is combined with the visual effects of the numerous other modern structures within view, a cumulative impact will occur. The cumulative impact on the historic districts' viewshed may further diminish the settings of these historic districts, which would be an adverse effect. We are hereby seeking your concurrence with our finding of additional impacts due to visual changes to the settings of these two historic districts.

Once we receive your concurrence with this determination, we intend to notify all consulting parties and begin discussing appropriate mitigation to minimize any adverse impacts. Ultimately, agreed upon mitigation measures would be included in a Memorandum of Agreement that would address the following areas of concern: adverse visual effects; potential effects to archaeological sites; documentation of the Union Hall; bridge aesthetics; access to the Battleship Park; and measures to avoid impacts from lighting and vibration.

If you have any questions or comments regarding this matter, please contact Ms. Lynne Urquhart of this office at (334) 274-6371 or [lynne.urquhart@dot.gov](mailto:lynne.urquhart@dot.gov).

Sincerely,

Mark D. Bartlett, P. E.  
Division Administrator

cc:

Ms. Alfredo Acoff, ALDOT  
Ms. Pat Patterson, ALDOT



STATE OF ALABAMA  
ALABAMA HISTORICAL COMMISSION  
468 SOUTH PERRY STREET  
MONTGOMERY, ALABAMA 36130-0900

FRANK W. WHITE  
EXECUTIVE DIRECTOR

June 11, 2015

TEL: 334-242-3184  
FAX: 334-240-3477

Mark D. Bartlett  
U.S. Department of Transportation  
9500 Wynlakes Place  
Montgomery, AL 36117

Re: AHC 00-0352  
Widen I-10 from Broad Street in Mobile to East of US 98 in Baldwin County  
Baldwin and Mobile Counties

Dear Mr. Bartlett:

Upon review of the above referenced project, we concur with the adverse affect determination. We look forward to developing the MOA.

We appreciate your commitment to helping us preserve Alabama's historic archaeological and architectural resources. Should you have any questions, please contact Amanda McBride at 334.230.2692 or [Amanda.McBride@preserveala.org](mailto:Amanda.McBride@preserveala.org). Have the AHC tracking number referenced above available and include it with any future correspondence.

Sincerely,

A handwritten signature in cursive script that reads "Lee Anne Wofford".

Lee Anne Wofford  
Deputy State Historic Preservation Officer

LAW/AMH/amh

**From:** Lisa LaRue-Baker - UKB THPO  
**To:** [Heisler, Timothy \(FHWA\)](mailto:Heisler_Timothy@FHWA)  
**Cc:** [ebird@unitedkeetoowahband.org](mailto:ebird@unitedkeetoowahband.org)  
**Subject:** Re: DPI-0030(005), Mobile River Bridge Section 106 Determination of Effects, Mobile & Baldwin Counties, State of Alabama  
**Date:** Wednesday, July 1, 2015 11:01:47 AM

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The United Keetoowah Band of Cherokee Indians in Oklahoma has reviewed your project under Section 106 of the NHPA and at this time, have no comments or objections. However, should any human remains be inadvertently discovered, please cease all work and contact us immediately. In addition, the United Keetoowah Band of Cherokee Indians in Oklahoma reserves the right to re-enter consultation on this project at any time.

Thank you,

Lisa C. Baker  
Acting THPO  
United Keetoowah Band of Cherokee Indians in Oklahoma  
PO Box 746  
Tahlequah, OK 74465

c 918.822.1952  
[ukbthpo-larue@yahoo.com](mailto:ukbthpo-larue@yahoo.com)

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Please FOLLOW our historic preservation page and LIKE us on FACEBOOK

-----  
On Tue, 6/30/15, [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov) <[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)> wrote:

Subject: DPI-0030(005), Mobile River Bridge Section 106 Determination of Effects, Mobile & Baldwin Counties, State of Alabama

To: [catherine-gray@cherokee.org](mailto:catherine-gray@cherokee.org), [celestine.bryant@actribe.org](mailto:celestine.bryant@actribe.org), [thpo@ttown.org](mailto:thpo@ttown.org), [earlii@tunica.org](mailto:earlii@tunica.org), [espain@mcn-nsn.gov](mailto:espain@mcn-nsn.gov), [jjacobs@mcn-nsn.gov](mailto:jjacobs@mcn-nsn.gov), [kialegeettcpo@gmail.com](mailto:kialegeettcpo@gmail.com), [ukbthpo-larue@yahoo.com](mailto:ukbthpo-larue@yahoo.com), [kokua.aina57@gmail.com](mailto:kokua.aina57@gmail.com), [aqhpo@mail.com](mailto:aqhpo@mail.com), [harjo.n@sno-nsn.gov](mailto:harjo.n@sno-nsn.gov), [rdushane@estoo.net](mailto:rdushane@estoo.net), [tylehowe@nc-cherokee.com](mailto:tylehowe@nc-cherokee.com), [lstapleton@unitedkeetoowahband.org](mailto:lstapleton@unitedkeetoowahband.org), [kcarleton@choctaw.org](mailto:kcarleton@choctaw.org), [lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com), [aqhpo@mail.com](mailto:aqhpo@mail.com), [rthrower@pci-nsn.gov](mailto:rthrower@pci-nsn.gov)

Cc: [Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov), [Mark.Bartlett@dot.gov](mailto:Mark.Bartlett@dot.gov)

Date: Tuesday, June 30, 2015, 10:28 AM

At this time FHWA is working on the Mobile River Bridge project in Mobile and Baldwin Counties. We want to make sure you are up-to-date in regards to Section 106 and this project.

Last week, FHWA signed a letter regarding Section 106 Determination of Effects Revisions. This letter states that a cumulative visual impact may further diminish the settings of the two historic districts, which would be considered an adverse effect. Now discussion will ensue regarding mitigation measures to resolve and/or minimize any adverse impacts, including potential archaeological impacts. Ultimately, mitigation measures will be incorporated in the Memorandum of Agreement.

Should you have any questions about this project, you may contact Mr. Timothy Heisler at (334) 274-6370 or email address at [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov).

Tim Heisler  
Transportation  
Engineer  
Federal Highway Administration  
– Alabama Division  
9500 Wynlakes Place

Montgomery, Alabama  
36117

Phone:  
334.274.6370

[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)

**From:** Lindsey Bilyeu  
**To:** [Heisler, Timothy \(FHWA\)](#)  
**Subject:** RE: DPI-0030(005), Mobile River Bridge Section 106 Determination of Effects, Mobile & Baldwin Counties, State of Alabama  
**Date:** Friday, July 31, 2015 9:14:22 AM

---

Mr. Heisler,

Thank you for providing the information regarding the adverse visual effect determination. The Choctaw Nation Historic Preservation Department will defer to the Alabama Historical Commission regarding the adverse visual effects to the historic districts.

Thank you,

Lindsey D. Bilyeu  
NHPA Senior Section 106 Reviewer  
Historic Preservation Department  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74701  
580-924-8280 ext. 2631  
[lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)

---

**From:** timothy.heisler@dot.gov [mailto:timothy.heisler@dot.gov]  
**Sent:** Tuesday, June 30, 2015 10:28 AM  
**To:** catherine-gray@cherokee.org; celestine.bryant@actribe.org; thpo@tttown.org; earlii@tunica.org; espain@mcn-nsn.gov; jjacobs@mcn-nsn.gov; kialegeettcpo@gmail.com; ukbthpolarue@yahoo.com; kokua.aina57@gmail.com; aqhpo@mail.com; harjo.n@sno-nsn.gov; rdushane@estoo.net; tylehowe@nc-cherokee.com; lstapleton@unitedkeetoowahband.org; kcarleton@choctaw.org; Lindsey Bilyeu; aqhpo@mail.com; rthrower@pci-nsn.gov  
**Cc:** Lynne.Urquhart@dot.gov; Mark.Bartlett@dot.gov  
**Subject:** DPI-0030(005), Mobile River Bridge Section 106 Determination of Effects, Mobile & Baldwin Counties, State of Alabama

At this time FHWA is working on the Mobile River Bridge project in Mobile and Baldwin Counties. We want to make sure you are up-to-date in regards to Section 106 and this project.

Last week, FHWA signed a letter regarding Section 106 Determination of Effects Revisions. This letter states that a cumulative visual impact may further diminish the settings of the two historic districts, which would be considered an adverse effect. Now discussion will ensue regarding mitigation measures to resolve and/or minimize any adverse impacts, including potential archaeological impacts. Ultimately, mitigation measures will be incorporated in the Memorandum of Agreement.

Should you have any questions about this project, you may contact Mr. Timothy Heisler at (334) 274-6370 or email address at [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov).

***Tim Heisler***  
**Transportation Engineer**  
**Federal Highway Administration – Alabama Division**  
**9500 Wynlakes Place**  
**Montgomery, Alabama 36117**  
**Phone: [334.274.6370](tel:334.274.6370)**  
**[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)**

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**From:** Lindsey Bilyeu  
**To:** [Heisler, Timothy \(FHWA\)](#)  
**Subject:** RE: DPI-0030(005), Mobile River Bridge & Bayway Widening, Mobile & Baldwin Counties, AL  
**Date:** Thursday, April 12, 2018 9:56:30 AM  
**Attachments:** [image001.png](#)

---

Tim,

As per our telephone conversation, the Choctaw Nation wishes to continue in consultation on the above referenced project. While we had sent a prior concurrence for the project on 2/25/2015, new information has come to light and we would like to re-enter consultation to make sure Choctaw cultural and sacred sites are protected.

If you have any questions, please contact me.

Thank you,  
Lindsey D. Bilyeu, MS  
Senior Compliance Review Officer  
Historic Preservation Department  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74702  
580-924-8280 ext. 2631



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**From:** Stephen Yerka  
**To:** [Heisler, Timothy \(FHWA\)](#)  
**Subject:** RE: DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Consulting Parties Meeting, Mobile & Baldwin Counties, State of Alabama, Federal Highway Administration & Alabama Department of Transportation  
**Date:** Tuesday, May 8, 2018 7:10:28 AM  
**Attachments:** [image002.png](#)

---

Good morning Timothy,

Thank you for offering us the opportunity to comment and consult on the I-10 Mobile River Bridge & Bayway Widening Project, DPI-0030(005). The project area is, however, outside of the area that the Eastern Band of Cherokee Indians (EBCI) THPO considers Traditional Territory. We therefore will differ to the other Tribal Nations listed as consulting parties and the SHPO/Division of Archaeology. We do wish that you have a successful outcome, and that all parties can reach an agreement for appropriate action.

Thank you for keeping us in the loop, and you can keep us on the email list if you like, and we will participate as interested observers.

Sincerely,

Stephen

Stephen J. Yerka  
Historic Preservation Specialist, THPO  
Eastern Band of Cherokee Indians (<https://ebci.com/>)  
[syerka@nc-choctaw.com](mailto:syerka@nc-choctaw.com)  
(828) 359-6852



---

**From:** Heisler, Timothy (FHWA) <timothy.heisler@dot.gov>  
**Sent:** Monday, May 7, 2018 6:20 PM  
**To:** Alina Shively (ashively@jenachoctaw.org) <ashively@jenachoctaw.org>; Andrew Weidman (andrewweidman@semtribe.com) <andrewweidman@semtribe.com>; Bradley Mueller (bradleymueller@semtribe.com) <bradleymueller@semtribe.com>; Carolyn White (cwhite@pci-nsn.gov) <cwhite@pci-nsn.gov>; celestine.bryant@actribe.org; Chickasaw Nation (HPO@chickasaw.net) <HPO@chickasaw.net>; David Cook (david.cook@kialegetribe.net) <david.cook@kialegetribe.net>; Earl Barbry Jr. (earlii@tunica.org) <earlii@tunica.org>; Kenneth H Carleton (kcarleton@choctaw.org) <kcarleton@choctaw.org>; Leonard Longhorn (llonghorn@astribe.com) <llonghorn@astribe.com>; Linda Langley (LLangley@CoushattaTribelA.org) <LLangley@CoushattaTribelA.org>; Lindsey D Bilyeu (lbilyeu@choctawnation.com) <lbilyeu@choctawnation.com>; Muscogee (Creek) Nation Section 106 (section106@mcn-nsn.gov) <section106@mcn-nsn.gov>; rdushane@estoo.net; Russell Townsend <RussellT@nc-choctaw.com>; Samantha Robinson (aqhpo@mail.com) <aqhpo@mail.com>; Stephen Yerka <syerka@nc-choctaw.com>; Ted Isham - Seminole Nation of Oklahoma (isham.t@sno-nsn.gov) <isham.t@sno-nsn.gov>; Terry Clouthier <thpo@ttown.org>; United Keetoowah Band of Cherokee Indians in Oklahoma (sbird@ukb-nsn.gov) <sbird@ukb-nsn.gov>

**Cc:** Urquhart, Lynne (FHWA) <Lynne.Urquhart@dot.gov>; Bartlett, Mark (FHWA) <Mark.Bartlett@dot.gov>; Missi M. Shumer (missi@shumerconsulting.com) <missi@shumerconsulting.com>; Wood, Andrew <wooda@dot.state.al.us>; Ericksen, Matthew <ericksenm@dot.state.al.us>; walkers@dot.state.al.us; Clay, Natasha <clayn@dot.state.al.us>  
**Subject:** RE: DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Consulting Parties Meeting, Mobile & Baldwin Counties, State of Alabama, Federal Highway Administration & Alabama Department of Transportation

Attached is the agenda and presentation for tomorrow's Section 106 meeting at 9:00 a.m.

Thanks,

**Tim Heisler**  
Transportation Engineer  
Federal Highway Administration – Alabama Division  
9500 Wynlakes Place  
Montgomery, Alabama 36117  
Phone: [334.274.6370](tel:334.274.6370)  
[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)

---

**From:** Heisler, Timothy (FHWA)  
**Sent:** Thursday, April 26, 2018 10:09 AM  
**To:** Alina Shively ([ashively@jenachoctaw.org](mailto:ashively@jenachoctaw.org)) <[ashively@jenachoctaw.org](mailto:ashively@jenachoctaw.org)>; Andrew Weidman ([andrewweidman@semtribe.com](mailto:andrewweidman@semtribe.com)) <[andrewweidman@semtribe.com](mailto:andrewweidman@semtribe.com)>; Bradley Mueller ([bradleymueller@semtribe.com](mailto:bradleymueller@semtribe.com)) <[bradleymueller@semtribe.com](mailto:bradleymueller@semtribe.com)>; Carolyn White ([cwhite@pci-nsn.gov](mailto:cwhite@pci-nsn.gov)) <[cwhite@pci-nsn.gov](mailto:cwhite@pci-nsn.gov)>; [celestine.bryant@actribe.org](mailto:celestine.bryant@actribe.org); Chickasaw Nation ([HPO@chickasaw.net](mailto:HPO@chickasaw.net)) <[HPO@chickasaw.net](mailto:HPO@chickasaw.net)>; David Cook ([david.cook@kialegetribe.net](mailto:david.cook@kialegetribe.net)) <[david.cook@kialegetribe.net](mailto:david.cook@kialegetribe.net)>; Earl Barbry Jr. ([earlii@tunica.org](mailto:earlii@tunica.org)) <[earlii@tunica.org](mailto:earlii@tunica.org)>; Kenneth H Carleton ([kcarleton@choctaw.org](mailto:kcarleton@choctaw.org)) <[kcarleton@choctaw.org](mailto:kcarleton@choctaw.org)>; Leonard Longhorn ([llonghorn@astribe.com](mailto:llonghorn@astribe.com)) <[llonghorn@astribe.com](mailto:llonghorn@astribe.com)>; Linda Langley <[LLangley@CoushattaTribelA.org](mailto:LLangley@CoushattaTribelA.org)>; Lindsey D Bilyeu ([lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)) <[lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)>; Muscogee (Creek) Nation Section 106 ([section106@mcn-nsn.gov](mailto:section106@mcn-nsn.gov)) <[section106@mcn-nsn.gov](mailto:section106@mcn-nsn.gov)>; [rdushane@estoo.net](mailto:rdushane@estoo.net); Russell Townsend ([russtown@nc-chokeee.com](mailto:russtown@nc-chokeee.com)) <[russtown@nc-chokeee.com](mailto:russtown@nc-chokeee.com)>; Samantha Robinson ([aqhpo@mail.com](mailto:aqhpo@mail.com)) <[aqhpo@mail.com](mailto:aqhpo@mail.com)>; Stephen Yerka ([syerka@nc-chokeee.com](mailto:syerka@nc-chokeee.com)) <[syerka@nc-chokeee.com](mailto:syerka@nc-chokeee.com)>; Ted Isham - Seminole Nation of Oklahoma ([isham.t@sno-nsn.gov](mailto:isham.t@sno-nsn.gov)) <[isham.t@sno-nsn.gov](mailto:isham.t@sno-nsn.gov)>; Terry Clouthier <[thpo@ttown.org](mailto:thpo@ttown.org)>; United Keetoowah Band of Cherokee Indians in Oklahoma ([sbird@ukb-nsn.gov](mailto:sbird@ukb-nsn.gov)) <[sbird@ukb-nsn.gov](mailto:sbird@ukb-nsn.gov)>  
**Cc:** Urquhart, Lynne (FHWA) ([Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov)) <[Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov)>; Bartlett, Mark (FHWA) <[Mark.Bartlett@dot.gov](mailto:Mark.Bartlett@dot.gov)>; Missi M. Shumer ([missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)) <[missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)>; Wood, Andrew <[wooda@dot.state.al.us](mailto:wooda@dot.state.al.us)>; Ericksen, Matthew <[ericksenm@dot.state.al.us](mailto:ericksenm@dot.state.al.us)>; 'walkers@dot.state.al.us' <[walkers@dot.state.al.us](mailto:walkers@dot.state.al.us)>; 'Clay, Natasha' <[clayn@dot.state.al.us](mailto:clayn@dot.state.al.us)>  
**Subject:** RE: DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Consulting Parties

Meeting, Mobile & Baldwin Counties, State of Alabama, Federal Highway Administration & Alabama Department of Transportation

Here is the skype Meeting link for Section 106 meeting on May 8<sup>th</sup>, 2018 from 9:00 a.m. to 12:00 p.m.

.....  
[→ Join Skype Meeting](#)

Join by Phone

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Participant code: 5878686

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***Tim Heisler***

**Transportation Engineer**

**Federal Highway Administration – Alabama Division**

**9500 Wynlakes Place**

**Montgomery, Alabama 36117**

**Phone: [334.274.6370](#)**

**[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)**

---

**From:** Heisler, Timothy (FHWA)

**Sent:** Thursday, April 12, 2018 1:32 PM

**To:** Alina Shively ([ashively@jenachoctaw.org](mailto:ashively@jenachoctaw.org)) <[ashively@jenachoctaw.org](mailto:ashively@jenachoctaw.org)>; Andrew Weidman ([andrewweidman@semtribe.com](mailto:andrewweidman@semtribe.com)) <[andrewweidman@semtribe.com](mailto:andrewweidman@semtribe.com)>; Bradley Mueller ([bradleymueller@semtribe.com](mailto:bradleymueller@semtribe.com)) <[bradleymueller@semtribe.com](mailto:bradleymueller@semtribe.com)>; Carolyn White ([cwhite@pci-nsn.gov](mailto:cwhite@pci-nsn.gov)) <[cwhite@pci-nsn.gov](mailto:cwhite@pci-nsn.gov)>; [celestine.bryant@actribe.org](mailto:celestine.bryant@actribe.org); Chickasaw Nation ([HPO@chickasaw.net](mailto:HPO@chickasaw.net)) <[HPO@chickasaw.net](mailto:HPO@chickasaw.net)>; David Cook ([david.cook@kialegetribe.net](mailto:david.cook@kialegetribe.net)) <[david.cook@kialegetribe.net](mailto:david.cook@kialegetribe.net)>; Earl Barbry Jr. ([earlii@tunica.org](mailto:earlii@tunica.org)) <[earlii@tunica.org](mailto:earlii@tunica.org)>; Kenneth H Carleton ([kcarleton@choctaw.org](mailto:kcarleton@choctaw.org)) <[kcarleton@choctaw.org](mailto:kcarleton@choctaw.org)>; Leonard Longhorn ([llonghorn@astribe.com](mailto:llonghorn@astribe.com)) <[llonghorn@astribe.com](mailto:llonghorn@astribe.com)>; Linda Langley ([LLangley@CoushattaTribelA.org](mailto:LLangley@CoushattaTribelA.org)); Lindsey D Bilyeu ([lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)) <[lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)>; Muscogee (Creek) Nation Section 106 ([section106@mcn-nsn.gov](mailto:section106@mcn-nsn.gov)) <[section106@mcn-nsn.gov](mailto:section106@mcn-nsn.gov)>; [rdushane@estoo.net](mailto:rdushane@estoo.net); Russell Townsend ([rustown@nc-chokeee.com](mailto:rustown@nc-chokeee.com)) <[rustown@nc-chokeee.com](mailto:rustown@nc-chokeee.com)>; Samantha Robinson ([aqhpo@mail.com](mailto:aqhpo@mail.com)) <[aqhpo@mail.com](mailto:aqhpo@mail.com)>; Stephen Yerka ([syerka@nc-chokeee.com](mailto:syerka@nc-chokeee.com)) <[syerka@nc-chokeee.com](mailto:syerka@nc-chokeee.com)>; Ted Isham - Seminole Nation of Oklahoma ([isham.t@sno-nsn.gov](mailto:isham.t@sno-nsn.gov)) <[isham.t@sno-nsn.gov](mailto:isham.t@sno-nsn.gov)>; Terry Clouthier ([thpo@ttown.org](mailto:thpo@ttown.org)); United Keetoowah Band of Cherokee Indians in Oklahoma ([sbird@ukb-nsn.gov](mailto:sbird@ukb-nsn.gov)) <[sbird@ukb-nsn.gov](mailto:sbird@ukb-nsn.gov)>

**Cc:** Urquhart, Lynne (FHWA) ([Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov)) <[Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov)>; Bartlett, Mark

(FHWA) <[Mark.Bartlett@dot.gov](mailto:Mark.Bartlett@dot.gov)>; Missi M. Shumer ([missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)) <[missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)>; Wood, Andrew <[wooda@dot.state.al.us](mailto:wooda@dot.state.al.us)>; Ericksen, Matthew <[ericksenm@dot.state.al.us](mailto:ericksenm@dot.state.al.us)>; 'walkers@dot.state.al.us' <[walkers@dot.state.al.us](mailto:walkers@dot.state.al.us)>; Clay, Natasha <[clayn@dot.state.al.us](mailto:clayn@dot.state.al.us)>

**Subject:** DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Consulting Parties Meeting, Mobile & Baldwin Counties, State of Alabama, Federal Highway Administration & Alabama Department of Transportation

Attached please find a letter that ALDOT sent out regarding an upcoming Consulting Parties meeting for the I-10 Mobile River Bridge & Bayway Widening project being held on May 8<sup>th</sup>, 2018 from 9:00 a.m. to 12:00 p.m. For those unavailable to attend in person, FHWA will provide a call-in number to the meeting.

Let me know if you need any additional information.

Thanks,

***Tim Heisler***  
**Transportation Engineer**  
**Federal Highway Administration – Alabama Division**  
**9500 Wynlakes Place**  
**Montgomery, Alabama 36117**  
**Phone: [334.274.6370](tel:334.274.6370)**  
**[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)**

**From:** timothy.heisler@dot.gov  
**To:** [missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)  
**Subject:** FW: DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Mobile & Baldwin Counties, State of Alabama, Federal Highway Administration & Alabama Department of Transportation  
**Date:** Friday, June 29, 2018 8:50:52 AM

---

**From:** Heisler, Timothy (FHWA)

**Sent:** Thursday, May 10, 2018 11:18 AM

**To:** llonghorn@astribe.com; ashively@jenachoctaw.org; andrewweidman@semtribe.com; bradleymueller@semtribe.com; celestine.bryant@actribe.org; HPO@chickasaw.net; david.cook@kialegetribe.net; earlii@tunica.org; thpo@tttown.org; KCarleton@choctaw.org; lbilyeu@choctawnation.com; sbird@ukb-nsn.gov; LLangley@CoushattaTribelA.org; aqhpo@mail.com; section106@mcn-nsn.gov; isham.t@sno-nsn.gov; rdushane@estoo.net; cwhite@pci-nsn.gov; russtown@nc-chokeee.com; syerka@nc-chokeee.com

**Cc:** wooda@dot.state.al.us; clayn@dot.state.al.us; Urquhart, Lynne (FHWA) <Lynne.Urquhart@dot.gov>; turnerw@dot.state.al.us

**Subject:** DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Mobile & Baldwin Counties, State of Alabama, Federal Highway Administration & Alabama Department of Transportation

**You have received 17 secure files from [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov).**

Use the secure links below to download.

Included in this email I have attached all of the cultural resource assessments performed to date, as well as the State Historic Preservation Officer's concurrence letters, ROW maps showing properties needed for this project, and the draft Section 106 MOA.

For future cultural resource assessments, FHWA will submit these to the Tribal Governments at the same time we submit them to the SHPO.

Thanks,

Tim Heisler  
334-274-6370

#### **Secure File Downloads:**

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[2018-03-29 MRB Section 106 Disposition of Comments.pdf](#)

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[2018-03-29 Section 106 MOA DRAFT .pdf](#)

4.40 MB, Fingerprint: c7c4f9b715544aa56797554d006e61eb ([What is this?](#))

[April 2017 Draft Report Phase I Buffalo Marine Property.pdf](#)

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[Archaeological Status Update Since Last Coordination.pdf](#)

4.64 MB, Fingerprint: e56cb5f65867e2a6b6e819c3246ce853 ([What is this?](#))

[Canal Street full report.pdf](#)

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**From:** timothy.heisler@dot.gov  
**To:** [missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)  
**Subject:** FW: Revised Alternative B Prime  
**Date:** Friday, June 29, 2018 8:50:17 AM  
**Attachments:** [image001.png](#)

---

**From:** Lindsey Bilyeu [<mailto:lbilyeu@choctawnation.com>]  
**Sent:** Tuesday, May 15, 2018 9:20 AM  
**To:** Heisler, Timothy (FHWA) <[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)>  
**Subject:** RE: Revised Alternative B Prime

Tim,

Thank you for providing the updated shapefiles. Everything worked out fine with this batch.

Thank you,

Lindsey D. Bilyeu, MS  
Senior Compliance Review Officer  
Historic Preservation Department  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74702  
580-924-8280 ext. 2631



---

**From:** Heisler, Timothy (FHWA) [<mailto:timothy.heisler@dot.gov>]  
**Sent:** Monday, May 14, 2018 1:00 PM  
**To:** Lindsey Bilyeu <[lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)>  
**Subject:** RE: Revised Alternative B Prime

Attached are the files you have requested.

Let me know if this works for you.

Tim Heisler

---

**From:** Lindsey Bilyeu [<mailto:lbilyeu@choctawnation.com>]  
**Sent:** Monday, May 14, 2018 9:31 AM  
**To:** Heisler, Timothy (FHWA) <[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)>  
**Subject:** RE: Revised Alternative B Prime

Yes, that's a very good description of To Bridge a Gap. I appreciate you working on the shapefile for the project.

Thank you,

Lindsey D. Bilyeu, MS  
Senior Compliance Review Officer  
Historic Preservation Department  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74702  
580-924-8280 ext. 2631



---

**From:** Heisler, Timothy (FHWA) [<mailto:timothy.heisler@dot.gov>]  
**Sent:** Monday, May 14, 2018 9:21 AM  
**To:** Lindsey Bilyeu <[bilyeu@choctawnation.com](mailto:bilyeu@choctawnation.com)>  
**Subject:** RE: Revised Alternative B Prime

I'm working on getting you the shapefile. The KMZ file will open up in Google Earth.

I think Bill Turner, ALDOT, will be attending To Bridge a Gap. I would like to attend one year, since I have never been and hear so much about it. Someone has told me that, To Bridge a Gap is Tribal Government speed dating with Federal Agencies.

Tim Heisler

---

**From:** Lindsey Bilyeu [<mailto:lilyeu@choctawnation.com>]  
**Sent:** Friday, May 11, 2018 11:09 AM  
**To:** Heisler, Timothy (FHWA) <[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)>  
**Subject:** RE: Revised Alternative B Prime

Tim,

I was able to download the file, but unfortunately, I actually need a GIS shapefile, not KMZ. Sorry to be so much trouble.

Also, will you or anyone from ALDOT be attending To Bridge a Gap?

Thank you,

Lindsey D. Bilyeu, MS  
Senior Compliance Review Officer

Historic Preservation Department  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74702  
580-924-8280 ext. 2631



---

**From:** Heisler, Timothy (FHWA) [<mailto:timothy.heisler@dot.gov>]  
**Sent:** Thursday, May 10, 2018 11:18 AM  
**To:** Lindsey Bilyeu <[lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)>  
**Subject:** RE: Revised Alternative B Prime

Lindsey,

Did the large file transfer site work for this?

Tim Heisler

---

**From:** Lindsey Bilyeu [<mailto:lbilyeu@choctawnation.com>]  
**Sent:** Tuesday, May 8, 2018 9:15 AM  
**To:** Heisler, Timothy (FHWA) <[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)>  
**Subject:** RE: Revised Alternative B Prime

Tim,

Can you send the shapefiles in a zip file? Our email server quarantines these types of files that are sent just as attachments.

Thank you,

Lindsey D. Bilyeu, MS  
Senior Compliance Review Officer  
Historic Preservation Department  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74702  
580-924-8280 ext. 2631



---

**From:** Heisler, Timothy (FHWA) [<mailto:timothy.heisler@dot.gov>]  
**Sent:** Tuesday, May 08, 2018 8:48 AM

**To:** Lindsey Bilyeu <[lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)>

**Subject:** FW: Revised Alternative B Prime

---

Halito!

\*\*\*WARNING: External email. Please verify sender before opening attachments or clicking on links.\*\*\*

---

Lindsey,

The shapefile you have requested for Mobile River Bridge is attached. Let me know if you need anything else.

Tim Heisler

---

**From:** Greg Lowe [<mailto:glowe@thompsonengineering.com>]

**Sent:** Monday, May 7, 2018 3:37 PM

**To:** Heisler, Timothy (FHWA) <[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)>; 'Andrew Wood ([wooda@dot.state.al.us](mailto:wooda@dot.state.al.us))' <[wooda@dot.state.al.us](mailto:wooda@dot.state.al.us)>

**Cc:** Missi Shumer <[missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)>

**Subject:** FW: Revised Alternative B Prime

The email message contained a ZIP attachment. The file was removed, as all ZIP files are temporarily blocked at this time. Other file types (e.g. Word, PowerPoint, PDF, etc.) can be received. If you recognize the sender and would like to view the attachment, please ask the sender to resend the message with a different file type, if possible.

FYI – attached is a revised shape file and KMZ of the B' alignment.

---

**From:** Tom Harjung

**Sent:** Monday, May 7, 2018 3:07 PM

**To:** Missi Shumer <[missi@shumerconsulting.com](mailto:missi@shumerconsulting.com)>; Greg Lowe <[glowe@thompsonengineering.com](mailto:glowe@thompsonengineering.com)>

**Cc:** Katie Parker <[kathryn.parker@mottmac.com](mailto:kathryn.parker@mottmac.com)>; Mary Mekkers <[mmekkers@thompsonengineering.com](mailto:mmekkers@thompsonengineering.com)>; Steve O'Hearn <[sohearn@thompsonengineering.com](mailto:sohearn@thompsonengineering.com)>; Matt Chelette <[mchelette@thompsonengineering.com](mailto:mchelette@thompsonengineering.com)>

**Subject:** FW: Revised Alternative B Prime

Missi/Greg,

The revised shape file for B' is attached for your use.

Please let us know if you need any additional information.

Thanks,

Tom

---

**From:** Mary Mekkers

**Sent:** Monday, May 7, 2018 2:55 PM  
**To:** Tom Harjung <[tharjung@thompsonengineering.com](mailto:tharjung@thompsonengineering.com)>  
**Subject:** Revised Alternative B Prime

Tom,  
Attached is the revised shapefile (and KMZ file which will open directly in Google Earth).

As discussed, Alternatives A, B and C, have not changed. Alternative B' has been modified to show the new alignment along the I-10 corridor from the west end of the bridge south to the Texas Street, thereby removing the "split". I am also including a KMZ file of the original B' for comparison, if needed.

Please let me know if there is anything else that may be needed.  
Thanks,  
Mary



**Mary Meekers**  
**GIS Analyst**  
thompsonENGINEERING  
*Celebrating 60 Years of Excellence*

251.706.6519(o) | 251.666.6422(f)  
[mmekkers@thompsonengineering.com](mailto:mmekkers@thompsonengineering.com)  
2970 Cottage Hill Road, Suite 190, Mobile, AL 36606  
[www.thompsonengineering.com](http://www.thompsonengineering.com)

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**From:** Heisler, Timothy (FHWA)  
**To:** [lhorn@astribe.com](mailto:lhorn@astribe.com); [ashively@jenachoclaw.org](mailto:ashively@jenachoclaw.org); [andrewweidman@semtribe.com](mailto:andrewweidman@semtribe.com); [bradleymueller@semtribe.com](mailto:bradleymueller@semtribe.com); [celestine.bryant@actribe.org](mailto:celestine.bryant@actribe.org); [HPO@chickasaw.net](mailto:HPO@chickasaw.net); [david.cook@kialegetribe.net](mailto:david.cook@kialegetribe.net); [earlii@tunica.org](mailto:earlii@tunica.org); [thpo@ttown.org](mailto:thpo@ttown.org); [KCarleton@choctaw.org](mailto:KCarleton@choctaw.org); [lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com); [sbird@ukb-nsn.gov](mailto:sbird@ukb-nsn.gov); [LLangley@CoushattaTribelA.org](mailto:LLangley@CoushattaTribelA.org); [aghpo@mail.com](mailto:aghpo@mail.com); [section106@mcn-nsn.gov](mailto:section106@mcn-nsn.gov); [isham.t@sno-nsn.gov](mailto:isham.t@sno-nsn.gov); [rdushane@estoo.net](mailto:rdushane@estoo.net); [cwhite@pci-nsn.gov](mailto:cwhite@pci-nsn.gov); [rustown@nc-chokeee.com](mailto:rustown@nc-chokeee.com); [syerka@nc-chokeee.com](mailto:syerka@nc-chokeee.com)  
**Cc:** [wooda@dot.state.al.us](mailto:wooda@dot.state.al.us); [clayn@dot.state.al.us](mailto:clayn@dot.state.al.us); Urquhart, Lynne (FHWA); [turnerw@dot.state.al.us](mailto:turnerw@dot.state.al.us); [missi@shumerconsulting.com](mailto:missi@shumerconsulting.com); Bartlett, Mark (FHWA); [dragottas@dot.state.al.us](mailto:dragottas@dot.state.al.us)  
**Subject:** DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Phase II Testing Proposal for Site 1Mb498, Mobile & Baldwin Counties, State of Alabama, Federal Highway Administration & Alabama Department of Transportation  
**Date:** Wednesday, August 22, 2018 10:44:34 AM  
**Attachments:** [Phase II Testing Proposal for Site 1Mb498.pdf](#)

---

Attached is the Phase II Testing proposal for site 1Mb498. Please let me know if you have any comment on the proposal.

Thanks,

***Tim Heisler***  
Transportation Engineer  
Federal Highway Administration – Alabama Division  
9500 Wynlakes Place  
Montgomery, Alabama 36117  
Phone: [334.274.6370](tel:334.274.6370)  
[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)



AUG-15 '18 PM03:31



**ALABAMA DEPARTMENT OF TRANSPORTATION**

1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-3050



*Kay Ivey*  
Governor

August 15, 2018

*John R. Cooper*  
Transportation Director

Ms. Lee Anne Wofford  
Deputy State Historic Preservation Officer  
Alabama Historical Commission  
468 South Perry Street  
Montgomery, Alabama 36130-3050

RE: Phase II Testing Proposals for Sites associated with the  
I-10 Mobile River Bridge and Bayway Widening Project ALDoT DPI-0030(005)  
Mobile and Baldwin Counties, Alabama  
AHC 00-1934 and 00-0352

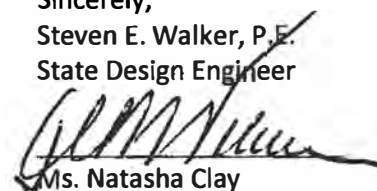
Dear Ms. Wofford:

Please find enclosed for your review and concurrence Phase II Research Designs prepared by the University of South Alabama Center for Archaeological Studies covering sites involved with the proposed I-10 Bridge and Bayway project. A Phase II proposal for site 1Mb498 was previously reviewed and concurred with. The current submittal relating to 1Mb498 covers Phase II testing of an additional area within the site that has recently become accessible. The second proposal covers two contiguous sites both of which are contained within the same City block. {REDACTED} A Phase I/II report covering initial work at 1Mb555 was previously submitted and a concurrence issued.

Phase II fieldwork results will be coordinated with you and your staff and if warranted, FHWA and ALDoT will present mitigation strategies. The Alabama Department of Transportation respectfully requests concurrence with the Phase II strategies presented for these sites. If questions arise, please contact Bill Turner of our staff. Thank you for your assistance in moving this project forward.

Sincerely,  
Steven E. Walker, P.E.  
State Design Engineer

By:

  
Ms. Natasha Clay  
State Environmental Administrator

SEW/NC/WBT  
enclosure

c: Lynne Urquhart, FHWA  
Dolha Kayisavera  
File

**From:** [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)  
**To:** [aqhpo@mail.com](mailto:aqhpo@mail.com); [ashively@jenachocotaw.org](mailto:ashively@jenachocotaw.org); [andrewweidman@semtribe.com](mailto:andrewweidman@semtribe.com); [bradleymueller@semtribe.com](mailto:bradleymueller@semtribe.com); [cwhite@pci-nsn.gov](mailto:cwhite@pci-nsn.gov); [celestine.bryant@actribe.org](mailto:celestine.bryant@actribe.org); [HPO@chickasaw.net](mailto:HPO@chickasaw.net); [david.cook@kialegetribe.net](mailto:david.cook@kialegetribe.net); [earlii@tunica.org](mailto:earlii@tunica.org); [kcarleton@choctaw.org](mailto:kcarleton@choctaw.org); [llonghorn@astribe.com](mailto:llonghorn@astribe.com); [LLangley@CoushattaTribelA.org](mailto:LLangley@CoushattaTribelA.org); [lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com); [section106@mcn-nsn.gov](mailto:section106@mcn-nsn.gov); [rdushane@estoo.net](mailto:rdushane@estoo.net); [russtown@nc-chokeee.com](mailto:russtown@nc-chokeee.com); [syerka@nc-chokeee.com](mailto:syerka@nc-chokeee.com); [isham.t@sno-nsn.gov](mailto:isham.t@sno-nsn.gov); [thpo@ttown.org](mailto:thpo@ttown.org); [sbird@ukb-nsn.gov](mailto:sbird@ukb-nsn.gov)  
**Cc:** [Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov); [missi@shumerconsulting.com](mailto:missi@shumerconsulting.com); [wooda@dot.state.al.us](mailto:wooda@dot.state.al.us); [dragottas@dot.state.al.us](mailto:dragottas@dot.state.al.us); [clayn@dot.state.al.us](mailto:clayn@dot.state.al.us); [turnerw@dot.state.al.us](mailto:turnerw@dot.state.al.us); [Mark.Bartlett@dot.gov](mailto:Mark.Bartlett@dot.gov)  
**Subject:** DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Archaeology Management Summaries, Mobile & Baldwin Counties, State of Alabama, Federal Highway Administration & Alabama Department of Transportation  
**Date:** Tuesday, January 8, 2019 4:35:45 PM

**You have received 15 secure files from [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov).**

Use the secure links below to download.

Attached are the latest archaeological management summaries for various sites throughout the Mobile River Bridge and Bayway widening project limits.

There is an upcoming Tribal coordination webinar scheduled for January 30th, that we can discuss any issues you might have with the management summaries. My contact information is below if you would prefer to contact me directly.

Thanks,

Tim Heisler  
Transportation Engineer  
Federal Highway Administration – Alabama Division  
9500 Wynlakes Place  
Montgomery, Alabama 36117  
Phone: 334.274.6370  
[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)

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[FINAL FINAL 1BA251 PH II Management Summary.pdf](#)

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[FINAL FINAL 1MB513 SB15 PH II Management Summary.pdf](#)

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[FINAL FINAL 1MB531 SB18 Management Summary.pdf](#)

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[FINAL FINAL 1MB551 SB22 PH II Management Summary.pdf](#)

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---

**From:** Heisler, Timothy (FHWA) <timothy.heisler@dot.gov>

**Sent:** Thursday, January 10, 2019 11:21 AM

**To:** Alabama-Quassarte Tribal Town (aqhpo@mail.com) <aqhpo@mail.com>; Alina Shively (ashively@jenachoctaw.org) <ashively@jenachoctaw.org>; Andrew Weidman (andrewweidman@semtribe.com) <andrewweidman@semtribe.com>; Bradley Mueller (bradleymueller@semtribe.com) <bradleymueller@semtribe.com>; Carolyn White (cwhite@pci-nsn.gov) <cwhite@pci-nsn.gov>; celestine.bryant@actribe.org; Chickasaw Nation (HPO@chickasaw.net) <HPO@chickasaw.net>; David Cook (david.cook@kialegeetribe.net) <david.cook@kialegeetribe.net>; Earl Barbry Jr. (earlii@tunica.org) <earlii@tunica.org>; Elizabeth Toombs <elizabeth-toombs@cherokee.org>; Kenneth H Carleton (kcarleton@choctaw.org) <kcarleton@choctaw.org>; Leonard Longhorn (llonghorn@astribe.com) <llonghorn@astribe.com>; Linda Langley <LLangley@CoushattaTribeLA.org>; Lindsey D Bilyeu (lbilyeu@choctawnation.com) <lbilyeu@choctawnation.com>; Muscogee (Creek) Nation Section 106 (section106@mcn-nsn.gov) <section106@mcn-nsn.gov>; rdushane@estoo.net; Russell Townsend (russtown@nc-cherokee.com) <russtown@nc-cherokee.com>; Stephen Yerka (syerka@nc-cherokee.com) <syerka@nc-cherokee.com>; Ted Isham - Seminole Nation of Oklahoma (isham.t@sno-nsn.gov) <isham.t@sno-nsn.gov>; Terry Clouthier <thpo@tttown.org>; United Keetoowah Band of Cherokee Indians in Oklahoma (sbird@ukb-nsn.gov) <sbird@ukb-nsn.gov>

**Cc:** Urquhart, Lynne (FHWA) <Lynne.Urquhart@dot.gov>; Wofford, Lee Anne (LeeAnne.Wofford@preserveala.org) <LeeAnne.Wofford@preserveala.org>; Turner, William <turnerw@dot.state.al.us>; Bartlett, Mark (FHWA) <Mark.Bartlett@dot.gov>; Wood, Andrew <wooda@dot.state.al.us>; Mandy Ranslow (mranslow@achp.gov) <mranslow@achp.gov>

**Subject:** Likely Human Remains Discovered, Appear to be from African American Settlement - DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Mobile and Baldwin Counties, State of Alabama

Yesterday, we received notification that bone fragments from archaeological site 1MB498 appear likely to be human. This site had undergone Phase II Archaeological Testing and materials from the site were being analyzed. Attached is an e-mail from Dr. Phillip Carr detailing the discovery and the fragments' origins. The bone fragments are likely from the African American Settlement described in the attached Management Summary.

The field work on the site has already been completed and the site was previously closed. Therefore, no further disturbance of the site or its resources is underway, so resolving this issue does not require expedited response times.

We anticipate consulting on this via phone calls/meetings with the SHPO (AHC), as needed, and

during our regular conference calls on this project with the tribes. But please let us know if you have any immediate concerns or suggestions. Thanks for your assistance.

***Tim Heisler***

**Transportation Engineer**

**Federal Highway Administration – Alabama Division**

**9500 Wynlakes Place**

**Montgomery, Alabama 36117**

**Phone: [334.274.6370](tel:334.274.6370)**

**[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)**

---

**From:** Heisler, Timothy (FHWA) <timothy.heisler@dot.gov>

**Sent:** Monday, January 14, 2019 8:42 AM

**To:** Bradley Mueller <bradleymueller@semtribe.com>

**Cc:** Urquhart, Lynne (FHWA) <Lynne.Urquhart@dot.gov>; Bartlett, Mark (FHWA) <Mark.Bartlett@dot.gov>; Turner, William <turnerw@dot.state.al.us>; Wood, Andrew <wooda@dot.state.al.us>

**Subject:** RE: Likely Human Remains Discovered, Appear to be from African American Settlement - DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Mobile and Baldwin Counties, State of Alabama

Bradley,

Attached are the photos you have requested.

***Tim Heisler***

**Transportation Engineer**

**Federal Highway Administration – Alabama Division**

**9500 Wynlakes Place**

**Montgomery, Alabama 36117**

**Phone: [334.274.6370](tel:334.274.6370)**

**[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)**

---

**From:** Bradley Mueller [<mailto:bradleymueller@semtribe.com>]

**Sent:** Thursday, January 10, 2019 1:43 PM

**To:** Heisler, Timothy (FHWA) <[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)>

**Subject:** RE: Likely Human Remains Discovered, Appear to be from African American Settlement - DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Mobile and Baldwin Counties, State of Alabama

SEMINOLE TRIBE OF FLORIDA  
TRIBAL HISTORIC PRESERVATION OFFICE  
AH-TAH-THI-KI MUSEUM

TRIBAL HISTORIC  
PRESERVATION OFFICE  
SEMINOLE TRIBE OF FLORIDA  
AH-TAH-THI-KI MUSEUM  
30290 JOSIE BILLIE HIGHWAY  
PMB 1004  
CLEWISTON, FL 33440  
THPO PHONE (863) 983-6549  
MUSEUM PHONE (863) 902-1113  
FAX (863) 902-1117  
THPO WEBSITE: WWW.STOFTHPO.COM  
MUSEUM WEBSITE: WWW.AHTAHTHIKI.COM



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CHAIRMAN

MITCHELL CYPRESS  
VICE CHAIRMAN

LAVONNE ROSE  
SECRETARY

PETER A. HAHN  
TREASURER

January 10, 2019

Good Afternoon Mr. Heisler,

Thank you for contacting the Seminole Tribe of Florida – Tribal Historic Preservation Office (STOF-THPO), Compliance Section regarding the possible human remains discovery at site 1MB498. Our NAGPRA Committee would like to request copies of any photos you have of the fragmentary bone for our initial review. We look forward to a future conference call on this matter. Please feel free to contact us with any questions or concerns.

Respectfully,

Bradley M. Mueller, MA, Compliance Supervisor  
STOF-THPO, Compliance Review Section  
30290 Josie Billie Hwy, PMB 1004  
Clewiston, FL 33440

Office: 863-983-6549 ext 12245  
Fax: 863-902-1117  
Email: [bradleymueller@semtribe.com](mailto:bradleymueller@semtribe.com)  
Web: [www.stofthpo.com](http://www.stofthpo.com)

---

**From:** Heisler, Timothy (FHWA) [<mailto:timothy.heisler@dot.gov>]

**Sent:** Thursday, January 10, 2019 12:21 PM

**To:** Alabama-Quassarte Tribal Town ([aqhpo@mail.com](mailto:aqhpo@mail.com)); Alina Shively ([ashively@jenachoctaw.org](mailto:ashively@jenachoctaw.org)); Andrew Weidman ([andrewweidman@semtribe.com](mailto:andrewweidman@semtribe.com)); Bradley Mueller; Carolyn White ([cwhite@pci-nsn.gov](mailto:cwhite@pci-nsn.gov)); [celestine.bryant@actribe.org](mailto:celestine.bryant@actribe.org); Chickasaw Nation ([HPO@chickasaw.net](mailto:HPO@chickasaw.net)); David Cook ([david.cook@kialegetribe.net](mailto:david.cook@kialegetribe.net)); Earl Barbry Jr. ([earlii@tunica.org](mailto:earlii@tunica.org)); Elizabeth Toombs; Kenneth H Carleton ([kcarleton@choctaw.org](mailto:kcarleton@choctaw.org)); Leonard Longhorn ([llonghorn@astribe.com](mailto:llonghorn@astribe.com)); Linda Langley; Lindsey D Bilyeu ([lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com)); Muscogee (Creek) Nation Section 106 ([section106@mcn-nsn.gov](mailto:section106@mcn-nsn.gov));



[rdushane@estoo.net](mailto:rdushane@estoo.net); Russell Townsend ([russtown@nc-chokeee.com](mailto:russtown@nc-chokeee.com)); Stephen Yerka ([syerka@nc-chokeee.com](mailto:syerka@nc-chokeee.com)); Ted Isham - Seminole Nation of Oklahoma ([isham.t@sno-nsn.gov](mailto:isham.t@sno-nsn.gov)); Terry Clouthier; United Keetoowah Band of Cherokee Indians in Oklahoma ([sbird@ukb-nsn.gov](mailto:sbird@ukb-nsn.gov))  
**Cc:** Urquhart, Lynne (FHWA); Wofford, Lee Anne ([LeeAnne.Wofford@preserveala.org](mailto:LeeAnne.Wofford@preserveala.org)); [turnerw@dot.state.al.us](mailto:turnerw@dot.state.al.us); Bartlett, Mark (FHWA); Wood, Andrew; Mandy Ranslow ([mranslow@achp.gov](mailto:mranslow@achp.gov))  
**Subject:** Likely Human Remains Discovered, Appear to be from African American Settlement - DPI-0030(005), I-10 Mobile River Bridge & Bayway Widening Project, Mobile and Baldwin Counties, State of Alabama

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Yesterday, we received notification that bone fragments from archaeological site 1MB498 appear likely to be human. This site had undergone Phase II Archaeological Testing and materials from the site were being analyzed. Attached is an e-mail from Dr. Phillip Carr detailing the discovery and the fragments' origins. The bone fragments are likely from the African American Settlement described in the attached Management Summary.

The field work on the site has already been completed and the site was previously closed. Therefore, no further disturbance of the site or its resources is underway, so resolving this issue does not require expedited response times.

We anticipate consulting on this via phone calls/meetings with the SHPO (AHC), as needed, and during our regular conference calls on this project with the tribes. But please let us know if you have any immediate concerns or suggestions. Thanks for your assistance.

***Tim Heisler***  
**Transportation Engineer**  
**Federal Highway Administration – Alabama Division**  
**9500 Wynlakes Place**  
**Montgomery, Alabama 36117**  
**Phone: [334.274.6370](tel:334.274.6370)**  
**[timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)**

**From:** [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov)  
**To:** [aqhpo@mail.com](mailto:aqhpo@mail.com); [ashively@jenachocotaw.org](mailto:ashively@jenachocotaw.org); [andrewweidman@semtribe.com](mailto:andrewweidman@semtribe.com); [bradleymueller@semtribe.com](mailto:bradleymueller@semtribe.com); [celestine.bryant@actribe.org](mailto:celestine.bryant@actribe.org); [HPO@chickasaw.net](mailto:HPO@chickasaw.net); [david.cook@kialegetribe.net](mailto:david.cook@kialegetribe.net); [earlii@tunica.org](mailto:earlii@tunica.org); [elizabeth-toombs@cherokee.org](mailto:elizabeth-toombs@cherokee.org); [lhaikey@pci-nsn.gov](mailto:lhaikey@pci-nsn.gov); [kcarleton@choctaw.org](mailto:kcarleton@choctaw.org); [llonghorn@astribe.com](mailto:llonghorn@astribe.com); [LLangley@CoushattaTribeLA.org](mailto:LLangley@CoushattaTribeLA.org); [lbilyeu@choctawnation.com](mailto:lbilyeu@choctawnation.com); [section106@mcn-nsn.gov](mailto:section106@mcn-nsn.gov); [rdushane@estoo.net](mailto:rdushane@estoo.net); [russtown@nc-chokeee.com](mailto:russtown@nc-chokeee.com); [syerka@nc-chokeee.com](mailto:syerka@nc-chokeee.com); [isham.t@sno-nsn.gov](mailto:isham.t@sno-nsn.gov); [thpo@tttown.org](mailto:thpo@tttown.org); [sbird@ukb-nsn.gov](mailto:sbird@ukb-nsn.gov)  
**Cc:** [Mark.Bartlett@dot.gov](mailto:Mark.Bartlett@dot.gov); [Lynne.Urquhart@dot.gov](mailto:Lynne.Urquhart@dot.gov); [wooda@dot.state.al.us](mailto:wooda@dot.state.al.us); [missi@shumerconsulting.com](mailto:missi@shumerconsulting.com); [clayn@dot.state.al.us](mailto:clayn@dot.state.al.us)  
**Subject:** DPI-0030(005), Mobile River Bridge and Bayway Project, Consulting Parties Invitation, Mobile and Baldwin Counties, State of Alabama  
**Date:** Wednesday, February 20, 2019 10:33:34 AM

**You have received 8 secure files from [timothy.heisler@dot.gov](mailto:timothy.heisler@dot.gov).**

Use the secure links below to download.

Attached please find a letter that ALDOT sent out regarding an upcoming Consulting Parties meeting for the I-10 Mobile River Bridge & Bayway Widening project being held on March 12th, 2019 from 10:00 a.m. to 12:00 p.m. For those unavailable to attend in person, FHWA will provide a call-in number to the meeting.

Let me know if you need any additional information.

Thanks,

Tim Heisler  
334-274-6370

**Secure File Downloads:**

Available until: **22 March 2019**

Click links to download:

[Africatown CR Report and Letter.pdf](#)

5.23 MB, Fingerprint: 8dda24a9fb607205d31a06e844057e55 ([What is this?](#))

[CP Invite 02-11-19.pdf](#)

293.76 KB, Fingerprint: 4cd89a70539f14859e13036d4090dfce ([What is this?](#))

[May 2018 CP Disposition of Comments.pdf](#)

171.90 KB, Fingerprint: d400bd4efb9ba9524a9255b868e41e78 ([What is this?](#))

[MRB Draft MOA Feb 2019.pdf](#)

13.10 MB, Fingerprint: 144fd886472ab6e38809bee72658e302 ([What is this?](#))

[US-90-98 Tensaw.pdf](#)

15.56 MB, Fingerprint: 4d85519c46619495bd708346c76de0d3 ([What is this?](#))

[Volume 1. I-10 Historical Background.pdf](#)

24.38 MB, Fingerprint: 4f78f16cdd1620ca2772d4e7dd41e886 ([What is this?](#))

[Volume 2. I-10 Historical Background and Historic Building Survey 7-2011.pdf](#)

55.71 MB, Fingerprint: 39656bbd271731ae5b74aeed0a147f18 ([What is this?](#))

[Volume 3. I-10 Viewshed Impact Assessment 7-2011.pdf](#)

110.97 MB, Fingerprint: d2c0115dca68a96cdc8e3b433007f0aa ([What is this?](#))

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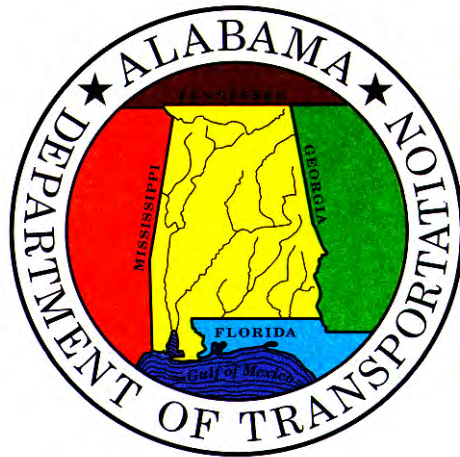
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# **APPENDIX O**

**Corridor Public Hearing**



**ALABAMA DEPARTMENT  
OF  
TRANSPORTATION**



**Corridor Hearing Report**

**Project DPI-0030 (005)  
I-10 Mobile River Bridge and Bayway Widening  
Mobile and Baldwin Counties  
September 23 and September 29, 2014**

**Volume 1 of 3  
December 1, 2014**



Robert Bentley  
GOVERNOR

# ALABAMA DEPARTMENT OF TRANSPORTATION

SOUTHWEST REGION  
OFFICE OF REGION ENGINEER  
1701 I-65 WEST SERVICE ROAD NORTH  
MOBILE, ALABAMA 36618-1109  
TELEPHONE: (251) 470-8200  
FAX: (251) 473-3624



John R. Cooper  
TRANSPORTATION DIRECTOR

December 16, 2014

Mr. William F. Adams, P.E.  
Chief Design Engineer  
Alabama Department of Transportation  
1409 Coliseum Boulevard  
Montgomery, Alabama 36110

ATTENTION: Ms. Alfredo Acoff

Dear Mr. Adams:

Re: **Project No. DPI-0030(005)**  
**I-10 Mobile River Bridge and Bayway Widening**  
**Mobile and Baldwin Counties**

A Corridor Hearing was held on the above-referenced project on September 23, 2014, from 4 p.m. to 8 p.m., at the Alabama Cruise Terminal located at 201 South Water Street, Mobile, AL 36602 and on September 29, 2014, from 4 p.m. to 8 p.m., at the Five Rivers Delta Resource Center located at 30945 Five Rivers Boulevard in Spanish Fort, AL 36527. The meeting was conducted using an informal open house format and formal public forum with short presentation. All guest attendees were asked to register and complete the comment forms furnished to them.

Attached is one hard copy and a CD containing PDFs of the Corridor Hearing Report for your review and further handling. A summary and our recommendations are contained in Section 3.0 and 4.0, respectively, of this report.

If you have any questions, please contact this office.

Sincerely,

Vincent E. Calametti, P.E.  
Region Engineer

VEC/ELP/jcr  
Attachments

C: FHWA w/Attachment (CD only)  
Volkert w/Attachment (CD only)  
File w/Attachments

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Note: Appendix C has been omitted from the Supplemental DEIS. Comment sheets and the transcript are available for review upon request to ALDOT.



## **1.0 PRE-HEARING ACTIVITIES**

The planning for the Corridor Hearing began in July 2014. Since Alabama Department of Transportation (ALDOT) anticipated a large number of attendees at the Corridor Hearing and because the proposed project crosses county lines, two hearings were scheduled for public convenience. The first hearing was scheduled in Mobile County at the Alabama Cruise Terminal located at 201 South Water Street in Mobile on September 23, 2014. The second hearing was scheduled for September 29, 2014 at the Five Rivers Delta Resource Center located at 30945 Five Rivers Boulevard in Spanish Fort in Baldwin County. The time, date, and locations were confirmed with the Cruise Terminal and Five Rivers by letters, dated September 4, 2014. A copy of these letters are included in Appendix A. These locations were considered ideal due to their proximity to the project.

Mr. William F. Adams, State Design Engineer was notified by a letter dated August 18, 2014, confirming the two Corridor Hearing dates, times and locations. This letter listed a schedule of events that included mailing of invitations, placement of posters, distribution of news releases, the pre-meeting conference, and the corridor hearing. A brief summary of these events is provided in the following paragraphs. A copy of the confirmation letter is included in Appendix A.

On August 15, 2014, posters were placed throughout the project impact area. A reduced copy of the poster and a list of posted locations are included in Appendix A.

On August 22, 2014, invitations were mailed to county and city officials, local legislators, and local business owners and residents. A copy of the invitation is attached in Appendix A, along with a list of names and addresses of people and organizations that were sent invitations.

On August 25, 2014, a news release was emailed to local radio and television stations and newspapers. A copy of the news release and the distribution list are all included in Appendix A.

On August 17, August 31, and September 21, 2014, a public notice was published in the Press Register advertising the date, time, location and purpose of the Corridor Hearing. The same advertisement was also run on September 4, September 11, and September 18 in the Lagniappe. A copy of the public notice is included in Appendix A.

## 2.0 CORRIDOR HEARING ACTIVITIES

### Alabama Cruise Terminal

Corridor Hearing activities began at 8:00 a.m. on September 22, 2014 with setting up the tables, displays, stage, and chairs at the Cruise Terminal. A walk-through meeting was held at 3:30 p.m. to familiarize ALDOT and consultant personnel with the exhibits and scope of the proposed project. An additional pre-meeting was held at 3:00 p.m. the day of the meeting.

Fact sheets were prepared and provided to personnel who participated in the hearing. The fact sheet contained pertinent information about the project such as project description, cost estimates, relocation information, proposed letting date, etc. A copy of the fact sheet is included in Appendix B.

The hearing location was well lit and provided ample space. The weather was warm and calm. Registration tables were set up just inside the entrance to the room. Within the meeting room, project exhibits were displayed on easels at multiple locations, monitors were setup to show the project website and presentation, and an area was setup and staffed by personnel from ALDOT's Visualization Department to show renderings of the project. The presentation and hearing portion was in the same room separated by sound dampening floor to ceiling drapes. Photographs of the meeting location are included in Appendix B.

The exhibits consisted of aerial photographs and layouts showing the alternates, renderings of the bridge from different locations around the city, and possible bicycle and pedestrian routes across Mobile River. Some representatives from ALDOT and Volkert were stationed at the exhibits and others circulated through the audience answering questions and directing citizens to specific areas of concern.

Registration began at 4:00 p.m. for the open house and at 5:00 p.m. for those wishing to speak at the public forum. The formal presentation began at 5:30 p.m. with the hearing following afterwards. There were a total of **308** registrants, **62** of whom were ALDOT and consultant personnel. A copy of the registration sheets is included in Appendix B.

Public handouts consisting of a welcome/introduction letter, a project alternatives map, and a comment sheet were provided to all registered guests. A copy of the public handouts is included in Appendix B.

### Five Rivers

Corridor Hearing activities began at 11:00 a.m. on September 29, 2014 with setting up the tables, displays, stage, and chairs at Five Rivers. A pre-meeting was held at 3:30 p.m. to familiarize ALDOT and consultant personnel with the exhibits and scope of the proposed project.

The same fact sheets from the previous meeting were used. A copy of the fact sheet is included in Appendix B.

The hearing location was well lit, provided ample space. The weather was warm and rained for a brief period of time. Registration tables were set up outside in the breezeway between the buildings used for the open house and hearing. Within the meeting room, project exhibits were displayed on easels at multiple locations, monitors were setup to show the project website and presentation, and an area was setup and staffed by personnel from ALDOT's Visualization Department to show renderings of the project. The presentation and hearing portion was in the auditorium in an adjacent building. A monitor was setup in the breezeway to allow additional people to view the public forum. Photographs of the meeting location are included in Appendix B.

The same exhibits shown at the Cruise Terminal were used. Some representatives from ALDOT and Volkert were stationed at the exhibits and others circulated through the audience answering questions and directing citizens to specific areas of concern.

Registration began at 4:00 p.m. for the open house and at 5:00 p.m. for those wishing to speak at the public forum. The presentation began at 5:30 p.m. with the hearing following afterwards. There were a total of **248** registrants, **39** of whom were ALDOT and consultant personnel. A copy of the registration sheets is included in Appendix B.

Public handouts consisting of a welcome/introduction letter, a project alternatives map, and a comment sheet were provided to all registered guests. A copy of the public handouts is included in Appendix B.

### 3.0 COMMENT SUMMARY

In total at both hearings, there were **556** registrants, of whom **101** were ALDOT and consultant personnel.

There were **35** people that spoke during the public forum portion at the Cruise Terminal hearing. There were **25** people that spoke during the public forum at the Five Rivers hearing.

There were **2** people at the Cruise Terminal hearing and **4** people at the Five Rivers hearing that provided their comments to the court reporter.

There were **51** comment sheets received at the Cruise Terminal hearing and **38** comment sheets received at the Five Rivers hearing. An additional **524** comments post marked by 5 p.m. on November 7, 2014 were received by mail, fax, or e-mail. The total number of written comments received was **613**.

Comments submitted multiple ways by the same individual were combined and counted as one comment. The total number of comments spoken at the public forum, submitted to the court reporter, or in writing was **641** and are summarized below:

- How often do you use the existing I-10 Wallace Tunnel?  
None [ 4 ]      Daily [ 108 ]      Weekly [ 232 ]      Occasionally [ 133 ]
- How often would you use a bike/pedestrian crossing for Mobile River?  
None [ 253 ]      Daily [ 15 ]      Weekly [ 71 ]      Occasionally [ 173 ]

<u>Comment Group</u>	<u>Number of Comments</u>
Support the project	558
Do not support the project	40
In favor of Bicycle/Pedestrian crossing of Mobile River	111
Do not support Bicycle/Pedestrian crossing of Mobile River	29

- The consensus of opinions are in favor of the proposed project with 6% against.
- Of the 558 Support the project comments, 4 comments supported Alternatives A, B, or C. The remaining 554 comments supported “B Prime” or did not specify an Alternative.
- The majority of comments supporting the project were to relieve congestion through the Wallace Tunnel.
- Of the 40 comments not supporting the project, half were in favor of a route or similar route to one that was previously studied and deemed not feasible. The other respondents did not see the need for the project, thought the congestion could be fixed by modifications to the Wallace Tunnel entrance, or the project would damage historic areas.
- Of the 111 comments in favor of Bicycle/Pedestrian crossing of Mobile River, 72 were in support of the crossing being included on the bridge.

There were three (3) petitions received attached to the comment forms. The Mobile Bicycle Pedestrian Advocacy Committee submitted a petition signed by numerous local, state and federal organizations and 3,213 individuals supporting the inclusion of a bike/pedestrian lane on the bridge. The Build the I-10 Bridge Coalition submitted a petition signed by 374 individuals

supporting the construction of the I-10 Bridge. Garland Mason submitted a petition with over 4,200 people that support naming the future I-10 Mobile River Bridge “The Corporal Christopher Edward Mason Bridge”.

Copies of the comment sheets, petitions, and transcripts of those that spoke at the public forum or submitter their comments to the court reporter are made a part of this report and are included in Appendix C.

#### **4.0 CONCLUSION**

Based on the public comment evaluation in Section 3.0, it is recommended to proceed with further study and development of the preferred alternate "B Prime" in the Final Environmental Impact Statement (FEIS). Also, further study of the inclusion of bicycle and pedestrian facilities across Mobile River is recommended.

# APPENDIX A





**ALABAMA  
DEPARTMENT OF TRANSPORTATION**



**SOUTHWEST REGION  
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MOBILE, ALABAMA 36618-1109  
TELEPHONE: (251) 470-8200  
FAX (251) 473-3624**

*Robert Bentley*  
GOVERNOR

*John R. Cooper*  
TRANSPORTATION DIRECTOR

September 4, 2014

Sheila H. Gurganus  
General Manager  
Alabama Cruise Terminal  
201 South Water Street  
Mobile, Al 36602

Dear Ms. Gurganus:


**Re: Project No. DPI-0030 (005)  
I-10 Mobile River Bridge and Bayway Widening  
Mobile and Baldwin Counties**

The purpose of this letter is to confirm the use of your facility for the purpose of conducting a Corridor Hearing for the above-referenced project. This meeting will be held on Tuesday, September 23, 2014 from 4 p.m. to 8 p.m. **We will arrive Monday morning in order to set up our exhibits.**

We appreciate you for allowing the Alabama Department of Transportation to use your facility. If you have any questions or need additional information, please contact Mr. Edwin Perry at 470-8243 or [perrye@dot.state.al.us](mailto:perrye@dot.state.al.us).

Sincerely,

Vincent E. Calametti, P.E., REGION ENGINEER

By:   
Edwin L. Perry III, P.E.  
Design Engineer - Mobile

abh/  
C: File



**ALABAMA  
DEPARTMENT OF TRANSPORTATION**

**SOUTHWEST REGION  
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TELEPHONE: (251) 470-8200  
FAX (251) 473-3624**



*Robert Bentley*  
GOVERNOR

*John R. Cooper*  
TRANSPORTATION DIRECTOR

September 4, 2014

Mr. Hank Burch  
Manager  
Five Rivers – Alabama's Delta Resource Center  
30945 Five Rivers Boulevard  
Spanish Fort, Al 36527

Dear Mr. Burch:

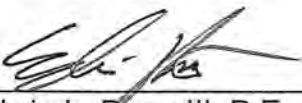
**Re: Project No. DPI-0030 (005)  
I-10 Mobile River Bridge and Bayway Widening  
Mobile and Baldwin Counties**

The purpose of this letter is to confirm the use of your facility for the purpose of conducting a Corridor Hearing for the above-referenced project. This meeting will be held on Monday, September 29, 2014 from 4 p.m. to 8 p.m. **We will arrive at approximately 11:00 a.m. in order to set up our exhibits.**

We appreciate you for allowing the Alabama Department of Transportation to use your facility. If you have any questions or need additional information, please contact Mr. Edwin Perry at 470-8243 or [perrye@dot.state.al.us](mailto:perrye@dot.state.al.us).

Sincerely,

Vincent E. Calametti, P.E., REGION ENGINEER

By:   
\_\_\_\_\_  
Edwin L. Perry III, P.E.  
Design Engineer - Mobile

abh/  
C: File



**ALABAMA  
DEPARTMENT OF TRANSPORTATION**



SOUTHWEST REGION  
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1701 I-65 WEST SERVICE ROAD NORTH  
MOBILE, ALABAMA 36618-1109  
TELEPHONE: (251) 470-8200  
FAX (251) 473-3624

*Robert Bentley*  
GOVERNOR

*John R. Cooper*  
TRANSPORTATION DIRECTOR

October 15, 2014

Sheila H. Gurganus  
General Manager  
Alabama Cruise Terminal  
201 South Water Street  
Mobile, Al 36602

Dear Ms. Gurganus:

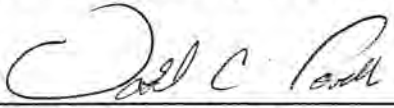
Re: **Project No. DPI-0030 (005)**  
**I-10 Mobile River Bridge and Bayway Widening**  
**Mobile and Baldwin Counties**

We would like to take this opportunity to thank you for providing the meeting room facility for the Corridor Hearing that we held for the above-referenced project. The meeting location was excellent and you did a great job taking care of all our needs.

Again, thank you very much.

Sincerely,

Vincent E. Calametti, P.E., REGION ENGINEER

By: 

Donald C. Powell, P.E.  
Regional Pre-Construction Engineer

abh/  
C:File



**ALABAMA  
DEPARTMENT OF TRANSPORTATION**



SOUTHWEST REGION  
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*Robert Bentley*  
GOVERNOR

*John R. Cooper*  
TRANSPORTATION DIRECTOR

October 15, 2014

Mr. Hank Burch  
Manager  
Five Rivers – Alabama’s Delta Resource Center  
30945 Five Rivers Boulevard  
Spanish Fort, Al 36527

Dear Mr. Burch:


**Re: Project No. DPI-0030 (005)  
I-10 Mobile River Bridge and Bayway Widening  
Mobile and Baldwin Counties**

We would like to take this opportunity to thank you for providing the meeting room facility for the Corridor Hearing that we held for the above-referenced project. The meeting location was excellent and you did a great job taking care of all our needs.

Again, thank you very much.

Sincerely,

Vincent E. Calametti, P.E., REGION ENGINEER

By:   
\_\_\_\_\_  
Donald C. Powell, P.E.  
Regional Pre-Construction Engineer

abh/  
C: File



Robert Bentley  
GOVERNOR

# ALABAMA DEPARTMENT OF TRANSPORTATION

SOUTHWEST REGION  
OFFICE OF REGION ENGINEER  
1701 I-65 WEST SERVICE ROAD NORTH  
MOBILE, ALABAMA 36618-1109  
TELEPHONE: (251) 470-8200  
FAX: (251) 473-3624



John R. Cooper  
TRANSPORTATION DIRECTOR

August 18, 2014

Mr. William F. Adams  
State Design Engineer  
Alabama Department of Transportation  
1409 Coliseum Boulevard  
Montgomery, Alabama 36110

Dear Mr. Adams:

Re: **Project No. DPI-0030 (005)**  
**I-10 Mobile River Bridge and Bayway Widening**  
**Mobile and Baldwin Counties**

We plan to conduct the following two Corridor Hearings on the above-referenced project:

Tuesday, September 23, 2014  
Alabama Cruise Terminal  
201 South Water Street  
Mobile, Alabama 36602  
4:00 P.M. – 8:00 P.M.

Monday, September 29, 2014  
5 Rivers – Alabama's Delta Resource Center  
30945 Five Rivers Blvd.  
Spanish Fort, Alabama 36527  
4:00 P.M. - 8:00 P.M.

The following is our proposed itinerary for the meetings:

- 8-15-14 Place posters at public places throughout project area.
- 8-17-14 Place a Public Notice in a major area newspaper.
- 8-22-14 Mail out invitations to municipal and county officials, legislators, and various local, state and federal agencies.
- 8-25-14 Issue news release to local newspapers, radio stations and television stations.
- 8-31-14 Place a Public Notice in a major area newspaper.
- 9-21-14 Place a Public Notice in a major area newspaper.
- 9-23-14 Pre-meeting "get-together" at 2:30 p.m.
- 9-23-14 Public Involvement Meeting from 4 p.m. to 8 p.m.

Mr. William F. Adams  
August 18, 2014  
Page 2

9-29-14 Pre-meeting "get-together" at 2:30 p.m.

9-29-14 Public Involvement Meeting from 4 p.m. to 8 p.m.

The same information will be presented in both meetings. The meeting format will be an open house from 4:00 p.m. to 8:00 p.m., where the public may review project information and exhibits of all the alternatives including the preferred on display. A public forum is scheduled to start at 5:30 p.m. and persons wishing to provide comments during the public forum should register at the sign-in table. There is a two (2) minute time limit for each person to speak.

A registration desk will be set up at the main entrance to hand out brochures and comment sheets, register attendees and direct them into the meeting.

The pre-meeting "get together" scheduled at 2:30 p.m. on the meeting dates will be important for all meeting workers to attend. This will be to review and familiarize them with the exhibits and facts concerning the planned project. It will also include discussion of any known potential problems that may surface at the meeting.

Sincerely,



Vincent E. Calametti, P.E. <sup>MFE</sup>  
Region Engineer

DCP/ELP/abh

C: Mr. Mark D. Bartlett, F.H.W.A.  
Mr. John R. Cooper  
Ms. Alfredo Acoff  
Mr. Carry Kelly  
Mr. Brian Ingram  
Mr. Leon Malone  
Environmental File





# PUBLIC NOTICE



Notice is hereby given that the Alabama Department of Transportation (ALDOT) will hold a Corridor Hearing regarding the Draft Environmental Impact Statement, Project No. DPI-0030 (005), I-10 Mobile River Bridge and Bayway Widening, Mobile and Baldwin Counties, Alabama.

**The purpose and need of the proposed project is to increase the capacity of I-10 to meet existing and predicted future traffic volumes and provide a more direct route for vehicles transporting hazardous materials across the Mobile River.**

You are cordially invited to attend and participate in either of the following public meetings:

**Tuesday, September 23, 2014**  
**Alabama Cruise Terminal**  
**201 South Water Street**  
**Mobile, Alabama 36602**  
**4:00 P.M. – 8:00 P.M.**

**Monday, September 29, 2014**  
**5 Rivers – Alabama’s Delta Resource Center**  
**30945 Five Rivers Blvd.**  
**Spanish Fort, Alabama 36527**  
**4:00 P.M. - 8:00 P.M.**

The same information will be presented in both meetings. The meeting format will be an open house from 4:00 p.m. to 8:00 p.m., where the public may review project information and exhibits of all the alternatives including the preferred on display. The public forum is scheduled to start at 5:30 p.m. Persons wishing to provide comments to ALDOT representatives during the public forum should register at the sign-in table. There is a two (2) minute time limit for each person to speak. These comments as well as written comments will be recorded and reviewed and will become a part of the public record. Representatives of ALDOT will be available to answer questions throughout the meeting.

Free parking will be available at each meeting.

Comment sheets will be provided, which you may fill out and turn back in at the meeting, submit online at [www.mobileriverbridge.com](http://www.mobileriverbridge.com), e-mail to us at [hossaina@dot.state.al.us](mailto:hossaina@dot.state.al.us), fax to (251) 473-3624 or send by mail by 5:00 p.m. Friday, November 7, 2014. The mailing address is:

**Vincent E. Calametti, P.E.**  
**Region Engineer, Southwest Region**  
**Alabama Department of Transportation**  
**1701 I-65 West Service Road N**  
**Mobile, Alabama 36618**

For additional information visit [www.mobileriverbridge.com](http://www.mobileriverbridge.com). For individuals requiring special assistance due to physical impairments or disability, please call (251) 470-8267 or contact the Region Engineer at the above-listed address. Special assistance should be requested at least five (5) business days prior to the meeting.





Public Meeting Notices for the meetings at the Cruise Terminal and Five Rivers were left at the following places in Baldwin County:

Wilson's Service Center, US-98  
BP, US-98  
Rite Aid, US-98  
Spanish Fort Post Office, US-98  
Shell Station, US-98  
Bass Pro Shops, US-98  
Spanish Fort Gold & Coin

Tyndall Federal Credit Union, US-98  
BBVA Compass Bank, US-98  
Rouses, US-31  
Wells Fargo, US-98  
Starbucks, US-90, Daphne  
Home Depot, US-90, Daphne

Public Meeting Notices for the meetings at the Cruise Terminal and Five Rivers were left at the following places on the Causeway:

Captain's Table  
Bay Transmission  
Oyster House  
Ed's Seafood Shed  
Felix's  
R&R Seafood  
Laps  
Bluegill  
Tacky Jack's

Public Meeting Notices for the meetings at the Cruise Terminal and Five Rivers were left at the following places in Mobile County:

City/County Complex – 6<sup>th</sup> Floor  
City/County Complex – 3<sup>rd</sup> Floor  
Exploreum, Government St.  
Olensky Brothers Store, Royal Street  
Hancock Bank, Dauphin St.  
Subway, Dauphin St.  
Mostly Muffins, Dauphin St.  
BBVA Compass Bank, corner of Royal and Dauphin St.  
Serda, Royal St.  
Alabama Power, St. Joseph Street  
Post Office, St. Joseph Street  
Moorer YMCA, St. Michael Street  
Regions Bank, St. Joseph Street  
LoDa Bier Garten, Dauphin St.  
TP Crockmiers, Dauphin St.  
Three Georges, Dauphin St.

Cabo, Dauphin St.  
Pita Pit, Dauphin St.  
Wells Fargo, St. Joseph Street  
Joe Cain's in the Battle House, Royal St.  
Panini Pete's, Dauphin St.  
PNC Bank, Royal St.  
Royal Scam, Royal St.  
Mount Pleasant Missionary Baptist Church, Texas St.  
International Ministry, Texas Place  
Bell & Co., Conception St  
Wal Tech, Conception St  
Marine & Industrial Supply, Virginia St.  
Southern Fish & Oyster, Eslava St.  
Prism Systems, Virginia St.  
Shell, Virginia St.  
Texas Street Community Center, Texas St.



**ALABAMA  
DEPARTMENT OF TRANSPORTATION**



SOUTHWEST REGION  
**OFFICE OF REGION ENGINEER**  
1701 I-65 WEST SERVICE ROAD NORTH  
**MOBILE, ALABAMA 36618-1109**  
TELEPHONE: (251) 470-8200  
FAX (251) 473-3624

*Robert Bentley*  
GOVERNOR

*John R. Cooper*  
TRANSPORTATION DIRECTOR

August 12, 2014

Sir or Madam  
Alabama Department of Tourism & Travel  
401 Adams Avenue, Suite 126  
Montgomery AL 36104

Dear Sir or Madam:

Re: **Project No. DPI-0030 (005)**  
**I-10 Mobile River Bridge and Bayway Widening, Mobile and Baldwin Counties, Alabama.**

You are invited to attend and participate in either of the following Corridor Hearings scheduled by the Alabama Department of Transportation (ALDOT) on the proposed project listed above:

**Tuesday, September 23, 2014**  
**Alabama Cruise Terminal**  
**201 South Water Street**  
**Mobile, Alabama 36602**  
**4:00 P.M. – 8:00 P.M.**

**Monday, September 29, 2014**  
**5 Rivers – Alabama’s Delta Resource Center**  
**30945 Five Rivers Blvd.**  
**Spanish Fort, Alabama 36527**  
**4:00 P.M. - 8:00 P.M.**

The same information will be presented in both meetings. The meeting format will be an open house from 4:00 p.m. to 8:00 p.m., where the public may review project information and exhibits of all the alternatives including the preferred on display. A public forum is scheduled to start at 5:30 p.m. and persons wishing to provide comments during the public forum should register at the sign-in table. There is a two (2) minute time limit for each person to speak.

ALDOT personnel will be on hand to discuss the project and answer questions. Comment sheets will be provided, which you may fill out and turn in at the meeting, submit online at [www.mobileriverbridge.com](http://www.mobileriverbridge.com), e-mail to [hossaina@dot.state.al.us](mailto:hossaina@dot.state.al.us), fax to (251) 473-3624, or send by mail by 5:00 p.m. Friday, November 7, 2014 to:

**Vincent E. Calametti, P.E.**  
**Region Engineer, Southwest Region**  
**Alabama Department of Transportation**  
**1701 I-65 West Service Road N**  
**Mobile, Alabama 36618**

All comments will receive due consideration in the development of this project.

We hope to see you at the meeting.

Sincerely,

Vincent E. Calametti, P.E.  
Region Engineer

VEC/ DCP/ELP/abh/KDL  
C: Environmental File

Public Involvement Meeting  
Project No. DPI-0030 (005), I-10 Mobile River Bridge  
Mobile and Baldwin Counties

Sir or Madam  
U. S. Environmental Protection Agency  
Environmental Assessment NEPA Review Staff  
Atlanta GA 30303-3104

Commissioner  
Department of Agriculture & Industries  
1445 Federal Drive  
Montgomery AL 36107

Director  
State of Alabama Geological Survey  
P.O. Box 869999  
Tuscaloosa AL 35401

Alabama Department of Housing  
and Urban Development  
950 22nd Street, North Suite 900  
Birmingham AL 35203-5302

Sir or Madam  
U.S.D.A.-Soil Conservation Service  
Post Office Box 311  
Auburn AL 36830

Sir or Madam  
Downtown Redevelopment Commission  
Post Office Box 1827  
Mobile AL 36633

Sir or Madam  
National Estuary Program  
440 Fairhope Avenue  
Fairhope AL 36532

Sir or Madam  
Dauphin Island Sea Lab  
101 Bienville Boulevard  
Dauphin Island AL 36528

Sir or Madam  
Alabama Wildlife Federation  
3050 Lanark Road  
Milbrook AL 36054

Environmental Coordinator  
Alabama Department of Conservation  
and Natural Resources  
64 North Union Street  
Montgomery AL 36130

Sir or Madam  
Dir Law Enforcement/Traffic Safety Div-ADECA  
404 Adams Avenue/P. O. Box 5690  
Montgomery AL 36130-5690

Sir or Madam  
U. S. Forest Service-U.S.D.A.  
2946 Chestnut Street  
Montgomery AL 36107

Sir or Madam  
Alabama State Docks  
Post Office Box 1588  
Mobile AL 36633

Sir or Madam  
Mobile Civic Center  
401 Civic Center Drive  
Mobile AL 36602

Sir or Madam  
Mobile Chamber of Commerce  
Post Office Box 2187  
Mobile AL 36652

Sir or Madam  
Mobile Convention Center  
1 Water Street  
Mobile AL 36602

Sir or Madam  
Associated General Contractors  
754 Downtowner Loop West  
Mobile AL 36609

Sir or Madam  
National Marine Fisheries Service  
9721 Executive Center Drive, North  
St Petersburg FL 33702

Director  
U.S.D.A.-Natural Resource Conservation Services  
Post Office Box 311  
Auburn, Alabama 36831-0311

Project Manager  
FAA/Airports District Office  
100 West Cross Street, Suite B  
Jackson MS 39208-2307

Sir or Madam  
The Nature Conservancy  
3280 Dauphin Street  
Mobile AL 36606

Sir or Madam  
Mobile County Emergency Management Agency  
348 McGregor Avenue  
Mobile AL 36608

Director  
Soil and Water Conservation  
P.O. Box 304800  
Montgomery AL 36130-4800

Cindy J. House-Pearson  
U.S. Army Corps of Engineers  
Mobile District  
Post Office Box 2288  
Mobile AL 36628-0001

Ms. Lee Anne Woffard  
Deputy Historic Preservation Officer  
Alabama Historical Commission  
468 South Perry Street  
Montgomery AL 36130-0900

Connie Hudson, President  
Mobile County Commission  
Post Office Box 1443  
Mobile AL 36633

Sir or Madam  
Baldwin County Emergency Management  
Agency  
23100 McAuliffe  
Robertsdale AL 36567

Public Involvement Meeting  
Project No. DPI-0030 (005), I-10 Mobile River Bridge  
Mobile and Baldwin Counties

Sir or Madam  
Mobile Bay National Estuary Program  
Bay Front Road  
Mobile AL 36615

Sir or Madam  
Mobile City Council  
Post Office Box 1827  
Mobile AL 36633

Sir or Madam  
Daphne City Council  
Post Office Box 400  
Daphne AL 36526

Sir or Madam  
Atlantic Marine, Inc.  
Post Office Box 3202  
Mobile AL 36652

Sir or Madam  
Star Shipping (NY) Inc.  
109 North Conception Street  
Mobile AL 36602

Sir or Madam  
ADEM-Air & Water Quality Program  
1890 Coliseum Boulevard  
Montgomery AL 36130-1463

Sir or Madam  
Mobile Bay Watch, Inc.  
3280 Dauphin Street  
Mobile AL 36606

Honorable Frank Burt, Jr.  
Office of Vice Chairman  
Baldwin County Commission  
312 Courthouse Square, Suite 12  
Bay Minette AL 36507

Director  
Alabama Development Office  
401 Adams Avenue  
6<sup>th</sup> Floor  
Montgomery AL 36130-4106

Mr. Nick Ambergber  
City of Mobile  
Post Office Box 1827  
Mobile AL 36633

The Honorable Alan Baker  
Alabama House of Representatives  
1416 Elizabeth Avenue  
Pollard AL 36441

Mr. Mark D. Bartlett  
Federal Highway Administration  
9500 Wynlakes Place  
Montgomery AL 36117-8515

The Honorable Margie Wilcox  
Alabama House of Representatives - District 104  
11 S. Union Street  
Montgomery, AL 36130

Mrs. Ann Bedsole  
6 St. Joseph Street  
Mobile AL 36602

Ntale Kajumba  
U.S. EPA Region 4  
NEPA Program Office  
Office of Policy and Management  
61 Forsyth Street  
Atlanta, GA 30303

The Honorable Michael M. McMillian  
Mayor of Spanish Fort  
Post Office Box 7226  
Spanish Fort AL 36577

The Honorable Bradley Byrne  
Alabama Congressman District 1  
U.S. House of Representatives  
2236 Rayburn HOB  
Washington, DC 20515

The Honorable Bill Hightower  
Alabama Senate District 35  
Room 733  
11 South Union Street  
Montgomery, AL 36130

Mr. John T. Burrough  
Central Texas Street Neighborhood Association  
751 South Warren Street  
Mobile AL 36603

The Honorable James E. Buskey  
Alabama House of Representatives - District 99  
2207 Barretts Lane  
Mobile AL 36617

The Honorable Jeff Collier  
Mayor of Dauphin Island  
1011 Bienville Boulevard  
Dauphin Island AL 36528

The Honorable Spencer Collier  
Alabama House of Representatives - District 105  
Post Office Box 550  
Irvington AL 36544

The Honorable Robert Craft  
Mayor of Gulf Shores  
Post Office Box 299  
Gulf Shores AL 36547

Mr. Alan Curtis  
Or Occupant  
500 Polo Trace  
Daphne AL 36526

The Honorable Ron Davis  
Mayor of Prichard  
Post Office Box 10427  
Prichard AL 36610

The Honorable Randy Davis  
Alabama House of Representatives - District 96  
6590 Thompson Lane  
Daphne AL 36526

Mr. A. Bailey Du Mont  
Mobile City Urban Development  
Post Office Box 1827  
Mobile AL 36633-1827

Public Involvement Meeting

Project No. DPI-0030 (005), I-10 Mobile River Bridge

Mobile and Baldwin Counties

The Honorable Vivian Davis Figures  
Alabama Senate District 33  
2054 Clemente Court  
Mobile AL 36617

The Honorable Joseph Mitchell  
Alabama House of Representatives  
District 103  
465 Dexter Avenue  
Mobile, AL 36604

The Honorable Billy J. Middleton  
Mayor of Loxley  
Post Office Box 9  
Loxley, AL

Dr. Tommy Bice, Superintendent  
Alabama Department of Education  
50 N. Ripley Street  
Post Office Box 302101  
Montgomery, AL 36104

Director  
U.S. Department of Energy  
1000 Independence Ave., SW  
Washington, DC 20585

U. S. Fish & Wildlife Service  
Daphne ES Field Office  
1208-B Main Street  
Daphne, AL 36526

Soil and Water Conservation Committee  
1445 Federal Drive/Beard Building  
Montgomery, AL 36109

Director  
Headquarters, US Army Corps of Engineers  
441 G. Street, NW  
Washington, DC 20314-1000

Chairman  
Advisory Council on Historic Preservation  
401 F Street NW, Suite 308  
Washington, DC 20001-2637

Commissioner Jerry Carl  
District 3  
Mobile County Commission  
Post Office Box 1443  
Mobile AL 36633

Director  
U.S. Department of Agriculture  
1400 Independence Ave., S.W.  
Washington, DC 20250

Director  
Alabama Department of Economic and  
Community Affairs  
P.O. Box 5690  
Montgomery, AL 36103-5690

Commissioner Merceria Ludgood  
District 1  
Mobile County Commission  
Post Office Box 1443  
Mobile AL 36633

Director  
U.S. Department of Commerce  
1401 Constitution Ave., NW  
Washington, DC 20230

Director  
Alabama Department of Environmental  
Management  
P.O. Box 301463  
Montgomery, Alabama 36130-1463

Sierra Club  
1330 21st Way S Ste 100  
Birmingham, AL 35205-3912

Director  
U.S. Department of Defense  
1400 Defense Pentagon  
Washington, DC 20301-1400

Honorable Chris Elliott  
Office of County Commissioner, District 2  
Baldwin County Commission  
1100 Fairhope Ave  
Fairhope, AL 36532

Ms. Ethel Smith  
Environmental Protection Specialist  
Office of Environmental Policy and Compliance  
(OEPC)  
Office of the Secretary [Room MS-2462-MIB]  
U.S. Department of the Interior [DOI]  
1849 C Street, NW  
Washington, DC 20240-0001  
Commander  
Eighth Coast Guard District  
Hale Boggs Federal Building  
500 Poydras Street  
New Orleans, LA 70130

Director  
U.S. Department of Health and Human Services  
200 Independence Avenue, S.W.  
Washington, D.C. 20201

Honorable Tucker Dorsey  
Office of County Commissioner, District 3  
Baldwin County Commission  
22251 Palmer Street  
Robertsdale, AL 36567

Director  
U.S. Department of Housing and Urban Development  
451 7th Street S.W.  
Washington, DC 20410

Honorable Charles F. "Skip" Gruber  
Office of Chairman  
Office of County Commissioner, District 4  
Baldwin County Commission  
201 East Section Ave  
Foley, AL 36535

Director  
Federal Emergency Management Agency  
U.S. Department of Homeland Security  
500 C Street SW, Washington, D.C. 20472

Director  
U.S. Department of the Interior  
Fish and Wildlife Service  
1849 C Street NW  
Washington, DC 20240

Public Involvement Meeting  
Project No. DPI-0030 (005), I-10 Mobile River Bridge  
Mobile and Baldwin Counties

The Honorable Chad Fincher  
Alabama House of Representatives, District 102  
9695 Alabama Court  
Semmes AL 36575

Mr. David M. Frank  
US Coast Guard  
501 Magazine Street  
New Orleans LA 70130-3396

The Honorable Victor Gaston  
Alabama House of Representatives  
District 100  
1136 Hillcrest Crossing, West  
Mobile AL 36695

Mr. Gary D. Giberson  
Atlantic Land Corporation  
660 Dunlap Drive  
Mobile AL 36601

The Honorable Rusty Glover  
Alabama Senate  
Post Office Box 2175  
Semmes AL 36575

The Honorable James Gordon  
Alabama House of Representatives  
District 98  
7818 Lukoli Lane  
Saraland AL 36571

Mr. Bob Hanks  
Church Street East Historical District  
200 South Warren Street  
Mobile AL 36603

The Honorable Jamie Ison  
Alabama House of Representatives - District 101  
57 Byrnes Blvd  
Mobile AL 36608

The Honorable Thomas E. Jackson  
Alabama House of Representatives  
District 68  
Post Office Box 656  
Thomasville AL 36784

Ms. Myrt Jones  
Mobile Bay Audubon Society  
Post Office Box 850611  
Mobile AL 36685

The Honorable Sandy Stimpson  
Mayor of Mobile  
Post Office Box 1827  
Mobile AL 36633

The Honorable Timothy M. Kant  
Mayor of Fairhope  
Post Office Drawer 429  
Fairhope AL 36533

The Honorable Marc Keahey  
Alabama Senate  
2350 Allen Road  
Grove Hill AL 36451

The Honorable Adline Clarke  
Alabama House of Representatives - District 97  
856 Canal Street  
Mobile AL 36602

The Honorable Tony Kennon  
Mayor of Orange Beach  
Post Office Box 458  
Orange Beach AL 36561

The Honorable John E. Koniar  
Mayor of Foley  
Post Office Drawer 400  
Foley AL 36536-0400

Mr. Gregory L. Leatherbury, Jr.  
Hand Arendall, L. L. C.  
Post Office Box 123/Suite 3000  
Mobile AL 36601

Mr. Ted Lee  
Navios Ship Agencies, Inc.  
261 North Joachim Street  
Mobile AL 36603

The Honorable Jerry Lundy  
Mayor of Mount Vernon  
Post Office Box 309  
Mount Vernon AL 36560

Mr. Cal Markert  
Baldwin County Engineer  
Post Office Box 220  
Silverhill AL 36576

Mr. Norman Pitman  
President  
Mobile Historical Development  
Commission  
Post Office Box 1827  
Mobile AL 36633-1827

The Honorable Stephen McMillan  
Alabama House of Representatives - District 95  
Post Office Box 337  
Bay Minette AL 36507

Mr. Brian P. Metcalfe  
Metcalfe & Company, Inc.  
Post Office Box 2903  
Mobile AL 36601

Mr. Bill Metzger  
City of Mobile  
Post Office Box 1827  
Mobile AL 36633

Dr. Robert H. Mount  
Or Occupant  
331 Funchess Hall  
Auburn AL 36849

The Honorable Charles H. Murphy  
Mayor of Robertsdale  
Post Office Box 429  
Robertsdale AL 36567

Mr. Joe W. Ruffer  
Mobile County Engineer  
Mobile Government Plaza  
205 Government Street  
Mobile, AL 36644-1600

Public Involvement Meeting  
Project No. DPI-0030 (005), I-10 Mobile River Bridge  
Mobile and Baldwin Counties

The Honorable Donald Nelson  
Mayor of Creola  
Post Office Box 490  
Creola AL 36525

The Honorable Charles O. Newton  
Alabama House of Representatives - District 90  
1216 South Conecuh Street  
Greenville AL 36037

Mr. E. B. Peebles, III  
Maritime Museum  
Post Office Box 290  
Mobile AL 36601

The Honorable Byron Pittman  
Mayor of Chickasaw  
Post Office Box 11307  
Chickasaw AL 36671

The Honorable Trip Pittman  
Alabama Senate District 32  
Post Office Box 1812  
Daphne AL 36526

The Honorable Loretta Presnell  
Mayor of Citronelle  
19135 South Main Street  
Citronelle AL 36522

Director  
Eastern States Office  
Bureau of Land Management  
U.S. Department of Interior  
411 Briarwood Drive  
Suite 404  
Jackson, MS 39206

The Honorable Hank Sanders  
Alabama Senate, District 23  
Post Office Box 1305  
Selma AL 36702

The Honorable Jeff Sessions  
Alabama Senator  
United States Senate  
326 Russell Senate Office Building  
Washington, DC 20510

The Honorable Richard Shelby  
Alabama Senator  
United States Senate  
304 Russell Senate Office Building  
Washington, DC 20510

The Honorable Harry Shiver  
Alabama House of Representatives - District 64  
46007 Sunset Drive  
Bay Minette AL 36507

Ms. Violetta Simpson  
Central Texas Street Neighborhood  
Association  
603 Delaware Street  
Mobile AL 36606

Mr. Bill Skinner  
Alabama Shipyard, Inc.  
Post Office Box 3202, Dunlap Drive  
Mobile AL 36652

The Honorable Dane Haygood  
Mayor of Daphne  
Post Office Box 400  
Daphne AL 36526

Central Texas Street Neighborhood  
Association  
750 South Cedar Street  
Mobile AL 36603

The Honorable William Stewart III  
Mayor of Satsuma  
Post Office Box 517  
Satsuma AL 36572

USS Alabama Commission  
Battleship Memorial Park  
Post Office Box 65  
Mobile AL 36601-0065

Mr. David Underhill  
Mobile Bay Group  
Post Office Box 2682  
Mobile AL 36652

The Honorable Ken Williams  
Mayor of Saraland  
716 U. S. Hwy 43, South  
Saraland AL 36571

The Honorable Marvin Williams  
Mayor of Elberta  
Post Office Drawer 277  
Elberta AL 36530

The Honorable Timothy Wilson  
Mayor of Silverhill  
Post Office Box 309  
Silverhill AL 36576

The Honorable David Wilson  
Mayor of Summerdale  
Post Office Box 148  
Summerdale AL 36580

Mr. Chris Miller  
South Alabama Regional Planning Commission  
Post Office Box 1665  
Mobile AL 36633

The Honorable Stan Wright  
Mayor of Bayou LaBatre  
13785 South Wintzell Avenue  
Bayou LaBatre AL 36509



Public Involvement Meeting  
Project No. DPI-0030 (005), I-10 Mobile River Bridge  
Mobile and Baldwin Counties

Mr. Hal Zimmerman  
Whilhelmsen Lines USA, Inc.  
109 North Conception Street  
Mobile AL 36602

Mr. B. J. Seigle Eskew  
Or Occupant  
1008 North Peters Street  
New Orleans LA 70116

The Honorable Joe Faust  
Alabama House of Representatives  
District 94  
20452 Beecher Street  
Fairhope AL 36532

Director  
U.S. Department of the Interior  
U.S. Geological Survey  
1849 C Street NW  
Washington, DC 20240

Director  
U.S. Department of Transportation  
Federal Aviation Administration  
800 Independence Avenue, SW  
Washington, DC 20591

Director  
Alabama Department of Industrial Relations  
649 Monroe Street  
Montgomery, Alabama 36131

Director  
Alabama Emergency Management Agency  
P.O. Drawer 2160  
Clanton, AL 35046-2160

Director  
Alabama Forestry Commission  
513 Madison Avenue  
Montgomery, AL 36130-2550

Director  
Alabama State Council on Arts and Humanities  
201 Monroe Street  
Montgomery, AL 36130-1800

**Office of the Attorney General**  
**500 Dexter Avenue**  
**Montgomery, AL 36130**

Mr. Mark Crosswhite, President  
Alabama Power Company  
600 North 18th Street  
Birmingham, AL 35291

Alabama Conservancy  
2100 1st Avenue North, Suite 500  
Birmingham, AL 35203

Dr. William E. Powell  
Executive Vice President  
Alabama Cattlemen's Association  
201 S. Bainbridge  
Montgomery, AL 36104

Supervisor  
US Forest Service  
USDA  
2946 Chestnut Street  
Montgomery, AL 36107

Alabama Tourism Department  
401 Adams Avenue, Suite 126  
P.O. Box 4927  
Montgomery, AL 36103-4927

Dr. Alan T. Lee  
Superintendent of Education  
Baldwin County Schools  
2600-A North Hand Avenue  
Bay Minette, AL 36507

Mrs. Martha L. Peek  
Superintendent  
Mobile County Schools  
P.O. Box 180069  
Mobile, AL 36618

**Honorable Tim Russell**  
**Baldwin County Probate Judge**  
**P.O. Box 459**  
**Bay Minette, AL 36507**

**Honorable Don Davis**  
**Mobile County Probate Judge**  
**P.O. Box 7**  
**Mobile, AL 36601**

Mr. Sam Cochran  
Mobile County Sheriff  
P.O. Box 113  
Mobile, AL 36601

Mr. Huey Hoss Mack  
Baldwin County Sheriff  
310 Hand Avenue  
Bay Minette, AL 36507

Mobile City Hall  
205 Government Street  
Mobile, AL 36602-0001

Daphne City Hall  
P.O. Box 400  
Daphne, AL 36526

Spanish Fort City Hall  
7581 Spanish Fort Boulevard  
Spanish Fort, AL 36527

Ms. Janette Curry  
Manager  
Mobile County Public Library  
Parkway Branch  
1924-B Dauphin Island Parkway  
Mobile, AL 36605-3004

Ms. Margie Calhoun  
Manager  
Mobile County Public Library  
Ben May Main Branch  
701 Government Street  
Mobile, AL 36602

Ms. Toja Young  
Director  
Daphne Public Library  
2607 U.S. Highway 98  
Daphne, AL 36526

Public Involvement Meeting  
Project No. DPI-0030 (005), I-10 Mobile River Bridge  
Mobile and Baldwin Counties

Ms. Elizabeth Merritt  
Deputy General Counsel  
National Trust for Historic Places  
1785 Massachusetts Avenue NW  
Washington, DC 20036-2117

Mr. John Hildreth  
Vice President  
Eastern Field Office  
National Trust for Historic Places  
William Aiken House  
456 King Street  
3rd Floor  
Charleston, SC 29403  
Mr. Walter Meigs  
General Counsel  
BAE Systems/Southeast Shipyards Alabama, LLC  
P.O. Box 3202  
Mobile, AL 36652

Ms. Carol Legard  
FHWA Liaison  
Advisory Council on Historic Places  
401 – F Street NW, Suite 308  
Washington, DC 20001-2637

Spanish Fort City Council  
Spanish Fort City Hall  
7581 Spanish Fort Boulevard  
Spanish Fort, AL 36527

Mr. Ray Harris  
Facilities Manager  
Signal Ship Repair  
601 Royal Street  
Mobile, AL 36602

Ms. Mary L. Cousar  
6 St. Joseph Street  
Mobile, AL 36602

Ms. Elizabeth S. Sanders  
Downtown Mobile Alliance  
P.O. Box 112  
Mobile, AL 36601

Ms. Caroline Etherton  
Colonial Dames and Conde-Charlotte Museum House  
Post Office Box 1968  
Mobile, AL 36633

Mr. Douglas Burtu Kearley  
Ten Wisteria Avenue  
Mobile, AL 36607

Mr. Herdon Inge  
P.O. Box 40188  
Mobile, AL 36640

Reverend Canon Beverly F. Gibson  
Christ Church Cathedral  
115 South Conception Street  
Mobile, AL 36602

Ms. Lauren Vanderbijl  
Historic Mobile Preservation Society  
300 Oakleigh Place  
Mobile, AL 36604

Ms. Katherine Frangos  
Friends of the Museum  
Post Office Box 2068  
Mobile, AL 36602

Mr. Jarrod J. White  
Restore Mobile, Inc.  
c/o Cabaniss Johnston, LLP  
Mobile Office, Riverview Plaza  
63 South Royal Street  
Suite 700  
Mobile, AL 36652

Luann Cutshaw  
424 N. Calhoun Street  
Tallahassee, FL 32301



Robert Bentley  
GOVERNOR

# ALABAMA DEPARTMENT OF TRANSPORTATION

SOUTHWEST REGION  
OFFICE OF REGION ENGINEER  
1701 I-65 WEST SERVICE ROAD NORTH  
MOBILE, ALABAMA 36618-1109  
TELEPHONE: (251) 470-8200  
FAX: (251) 473-3624



John R. Cooper  
TRANSPORTATION DIRECTOR

August 25, 2014

## NEWS RELEASE

### **I-10 Mobile River Bridge and Bayway Widening, Mobile and Baldwin Counties, Alabama.**

The Alabama Department of Transportation (ALDOT) will hold Corridor Hearings on the proposed improvement of I-10 from west of Broad Street in Mobile County to a location East of the existing U.S. Highway 98/I-10 Interchange at Daphne/Spanish Fort. All the alternatives including the preferred corridor will be presented to the public for consideration and comments at this meeting.

The same information will be presented in both meetings. The dates and locations of the meetings are as follows:

**Tuesday, September 23, 2014**  
**Alabama Cruise Terminal**  
**201 South Water Street**  
**Mobile, Alabama 36602**  
**4:00 P.M. – 8:00 P.M.**


**Monday, September 29, 2014**  
**5 Rivers – Alabama’s Delta Resource Center**  
**30945 Five Rivers Blvd.**  
**Spanish Fort, Alabama 36527**  
**4:00 P.M. - 8:00 P.M.**

The meeting format will be an open house from 4:00 p.m. to 8:00 p.m., where the public may review project information and exhibits of all the alternatives including the preferred on display. A public forum is scheduled to start at 5:30 p.m. and persons wishing to provide comments during the public forum should register at the sign-in table. There is a two (2) minute time limit for each person to speak. Representatives of the Alabama Department of Transportation (ALDOT) will be available to answer questions throughout the meeting. Citizens are invited to attend anytime between the above-stated hours. Public views and comments are welcomed.

There are several ways to submit a comment for review in the final Environmental Impact Statement (EIS):

- In person, at either one of the meetings (2 minute time limit)
- At the meeting by filling out a comment sheet or using one of the provided court reporters

- Take home a comment sheet and mail to ALDOT's Mobile Office by 5:00 pm Friday, November 7, 2014
- Fill out a comment sheet online at [MobileRiverBridge.com](http://MobileRiverBridge.com)
- Fax a comment sheet to (251) 473-3624
- Email a comment sheet to [hossaina@dot.state.al.us](mailto:hossaina@dot.state.al.us)

  
Vincent E. Calametti, P.E.  
Region Engineer

## NEWS MEDIA LIST

### TELEVISION STATIONS

WKRG  
555 Broadcast Drive  
Mobile, Alabama 36606  
Telephone: 251-479-5555  
Fax: 251-662-3071  
e-mail: [tv5@wkrq.com](mailto:tv5@wkrq.com)

WALA-TV 10  
1501 Satchel Paige Drive  
Mobile, Alabama 36606  
Telephone: 251-434-1010  
Fax: 251-434-1023  
e-mail: [aghassemi@fox10tv.com](mailto:aghassemi@fox10tv.com)  
[bcashen@fox10tv.com](mailto:bcashen@fox10tv.com)

WEAR  
Mobile Highway  
Pensacola, Florida  
and  
200 Government Street  
Mobile, Alabama 36602  
Telephone: 251-433-3333  
Fax: 850-455-8972  
e-mail: [wmakin@wear.sbgnet.com](mailto:wmakin@wear.sbgnet.com)

Tri-City Ledger  
P.O.Box 1916  
Flomaton, Alabama 36441  
Telephone (251) 296-3491  
Fax (251) 296-0010  
e-mail: [jcooper@tricityledger.com](mailto:jcooper@tricityledger.com)

WPMI  
661 Azalea Road  
Mobile, Alabama 36609  
Telephone: 251-602-1544  
Fax: 251-602-1550  
e-mail: [nbc15@wpmi.com](mailto:nbc15@wpmi.com)

WMPV  
1668 S Beltline Hwy  
Mobile, Alabama 36606  
Telephone: 251-661-2101  
Fax: 251-661-7121  
e-mail: [agoins@tbn.org](mailto:agoins@tbn.org)

## **RADIO STATIONS**

WAVH

3725 Airport Boulevard

Mobile, Alabama 36608

Telephone: 251-344-1065

Fax: 251-438-5462

e-mail: [news@106.5thepirate.com](mailto:news@106.5thepirate.com)

WNTM/ WMXC/ WRKH/ WKSJ/ Classic  
Country 104/ IMPACT 1270

555 Broadcast Drive

Mobile, Alabama 36606

Telephone: 251-450-0100

Fax: 251-479-3418

e-mail: [sandacoyle@clearchannel.com](mailto:sandacoyle@clearchannel.com)  
[cctvinfo@clearchannel.com](mailto:cctvinfo@clearchannel.com)

WBHY

2621-B Ralston Rd

Mobile, Alabama

Telephone : 251-473-8488

e-mail : [news@goforth.org](mailto:news@goforth.org)

WNSP/WZEW

1100 E Dauphin St

Mobile, Alabama

Telephone : 251-438-5460

Fax : 251-438-5462

e-mail: [clintc@wnsp.com](mailto:clintc@wnsp.com)

WABF 1220 AM

460 Section St. S

Fairhope, Alabama

Telephone : 251-928-9228

Fax : 251-928-9229

e-mail: [wabf1220@bellsouth.net](mailto:wabf1220@bellsouth.net)

# APPENDIX B



**Mobile River Bridge Public Hearing Fact Sheet**

**Project Number:** DPI-0030 (005)

**Description:** I-10 Mobile River Bridge and Bayway Widening  
Mobile and Baldwin County

**Purpose & Need:** Increase the capacity of I-10 to meet existing and future traffic volumes and to provide a more direct route for vehicles transporting hazardous materials, while minimizing impacts to Mobile's maritime industry.

**Presented Alternates:** Five Feasible (5) (A, B, B' Preferred, C and No Build)  
14 Alternatives originally, others didn't meet Purpose and Need

**Traffic Count (AADT)**

Route	Demand (AADT)**	
	Existing 2010	2030
Cochrane-Africatown USA Bridge	14,463	23,699
I-10 Wallace Tunnels (Design 36,000)	79,997	131,082
Bankhead Tunnel	16,884	27,664
<b>Total</b>	<b>111,344</b>	<b>182,445</b>

\*\*Average Annual Daily Traffic

**Alternatives Comparison Matrix**

Description	Alt A	Alt B	Alt B' Preferred	Alt C
Total Cost (\$M)	\$782.6	\$771.2	\$773.1	\$791.0
Roadway Widening Length (miles)	1.1	0.9	0.8	1.2
Bridge Length (miles)	2.5	2.7	2.8	2.9
Bayway Widening Length (miles)	6.6	6.5	6.4	6.3
Total Length (miles)	10.2	10.1	10.0	10.4
Economic Loss (\$M)	\$5.6	\$6.1	\$6.1	\$200
Economic Benefits (\$M)	\$537-1,054	\$549-1,066	\$549-1,066	\$560-1,077
Residential Relocations (each)	0	0	0	4
Business Relocations (each)	0	13	12	13
Wetlands (acres)	2.2	1.7	1.7	6.6
Essential Fish Habitat (acres)	76.25	67.15	67.15	65.35
Traffic Noise Impacts (each)	275	274	275	392
Hazardous Material Sites (each)	3	8	7	9
Archaeological Sites (each)	0	1	1	4
Potential Visual Impacts	Highest	Moderate	Moderate	Low
Direct Historic Impact	None	Union Hall	None	Maritime District
Section 4f Properties* (each)	0	1	0	1

\*Section 4f includes: Public Parks, Recreation Areas, Wildlife and Waterfowl Refuge, and Historical

- The existing I-10 Wallace Tunnel will remain as a connector to Downtown Mobile
- Stress on the where we are in the process .... selecting from the five feasible alternates
- Direct Bike and Pedestrian questions to Bike and Pedestrian Displays
- Cable Stayed Bridge with Air Draft minimum clearance of 215 ft above Navigation Channel
- Speed, Slope and Curve will meet current AASHTO interstate design standards
- Estimated construction time 6-8 years based on similar projects.
- Next Step Final EIS, estimated completion late 2015
- Deadline for Comments is 5:00 pm Friday, November 7<sup>th</sup>
- Funding to be determined
- Comment formats: Comment Form, Speaking at Public Forum, or Speak to Court Reporter



REGISTRATION

COMMENT FORM DROP-OFF

REGISTRATION





City of Mobile

Alternative B' (Preferred Alternative)  
I-10 Mobile River Bridge and Bayway Widening  
Project No. DP1-0030(005)



City of Mobile

Alternative B' (Preferred Alternative)  
I-10 Mobile River Bridge and Bayway Widening  
Project No. DP1-0030(005)





# BICYCLE AND PEDESTRIAN ALTERNATIVES



Mobile River



Identifying & Potential Bike & Pedestrian Routes for Mobile River Bridge and Bayway Widening Project No. DP150500255



Mobile River



City of Mobile













**Agenda**

- Public Comment Period
- Review the Draft Environmental Impact Statement
- Addressing Stakeholder Comments
- Final Environmental Impact Statement
- Approval of the Final EIS
- Construction Methods
- All Other RUCDT Committee Actions

**Agenda**

- Public Comment Period
- Review the Draft Environmental Impact Statement
- Addressing Stakeholder Comments
- Final Environmental Impact Statement
- Approval of the Final EIS
- Construction Methods
- All Other RUCDT Committee Actions





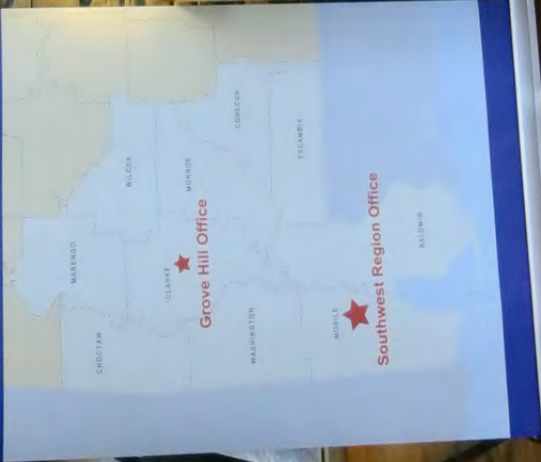




## Alabama Department of Transportation Southwest Region

1701 I-65 West Service Road North • Mobile, AL 36618  
(251) 470-8200

Find us on the web at [dot.state.al.us](http://dot.state.al.us) and [@ALDOTMobileArea](https://twitter.com/ALDOTMobileArea)



## COMMENT FORM DROP-OFF



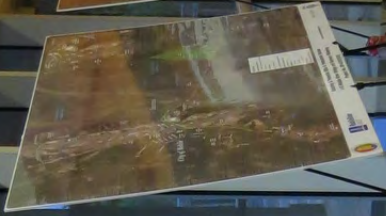
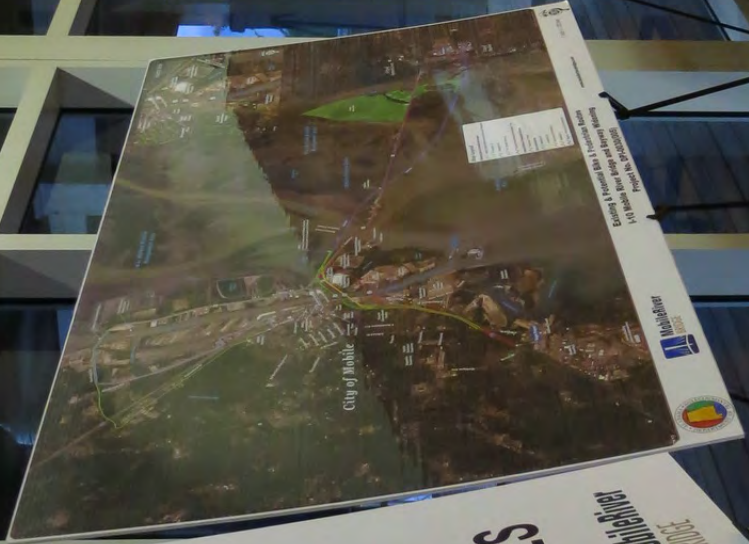


# BICYCLE

# AND

# PEDESTRIAN

# ALTERNATIVES







EXIT

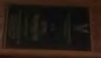


# VIRTUAL DESIGN AND

# VISUALIZATION



Photo-Match of MRB Looking South







SIGN IN

MobileFirst







Avalanche Risk / 1994-2014  
No Signal

FOREVER WILD  
FOREVER YOURS.  
Facebook.com/foreverwild  
http://www.foreverwild.com

EXIT





Robert Bentley  
GOVERNOR

# ALABAMA DEPARTMENT OF TRANSPORTATION

SOUTHWEST REGION  
OFFICE OF REGION ENGINEER  
1701 I-65 WEST SERVICE ROAD NORTH  
MOBILE, ALABAMA 36618-1109  
TELEPHONE: (251) 470-8200  
FAX: (251) 473-3624



John R. Cooper  
TRANSPORTATION DIRECTOR

Dear Citizen:

Welcome to the Corridor Hearing for Project Number DPI-0030 (005). This project consists of improvement of I-10 from west of Broad Street in Mobile County to a location East of the existing U.S. Highway 98/I-10 Interchange at Daphne/Spanish Fort.

As you enter the room, please notice the large project layouts on exhibit for your viewing. These layouts will show all the alternatives including the preferred. ALDOT personnel will be stationed at the displays to answer your questions and discuss the project with you. These individuals can be identified by their name tags. Please take time to discuss the project with them.

A public forum will be held at 5:30 p.m., and persons wishing to provide comments to ALDOT representatives during the public forum should register at the sign-in table. There is a two (2) minute time limit for each person to speak.

We encourage you to fill out and return the comment sheet attached to the back of this brochure. You may fill it out here tonight and turn it in before you leave, or you may mail it by 5:00 p.m. Friday, November 7, 2014 to:

Mr. Vincent E. Calametti, P.E., Region Engineer  
Alabama Department of Transportation  
1701 I-65 West Service Road N  
Mobile, Alabama 36618-1109

Comment Forms may also be submitted online at  
[www.mobileriverbridge.com](http://www.mobileriverbridge.com), faxed to (251) 473-3624 or e-mailed  
to [hossaina@dot.state.al.us](mailto:hossaina@dot.state.al.us)

A Corridor Hearing Report will be compiled after the comment period expires on November 7, 2014. All comments received will be given due consideration in the final plan development of this project.

Thank you for your attendance and interest in this project.

Sincerely,

Vincent E. Calametti, P.E.  
Region Engineer





Source: USGS, Digital Numbers, Satellite Imagery, National Earthquake Information Center, California CGS, USGS, USGS, ARCA, California, Virginia, etc. 2011, Mississippi, and Bay State University

Note: This map is for presentation use only and not to be used for construction purposes.



0-47

**Figure ES**  
**I-10 Mobile River Bridge and Bayway Widening**  
**Build Alternatives with Bayway Widening**  
**Project No. DPI-0030(005)**

Path: V:\PROJECTS\0030\005\Map\_Summary.dwg, 05/11/2011 11:51:11 AM, Scale: 1:1000, Units: Feet, Projection: NAD83



**CORRIDOR HEARING COMMENT SHEET**  
**PROJECT NUMBER: DPI-0030 (005)**  
**I-10 MOBILE RIVER BRIDGE AND BAYWAY WIDENING**  
**MOBILE AND BALDWIN COUNTIES, ALABAMA**

**NAME:** \_\_\_\_\_ **DATE:** \_\_\_\_\_

**ADDRESS:** \_\_\_\_\_

**Personal information will not be released.**

Are you one of the following?      Property Owner       Area Resident       Commuter   
                                                        Business Owner       Work in the Vicinity of the Project

Name of the Organization (if applicable) \_\_\_\_\_

*How often do you use the existing I-10 Wallace Tunnel?*

**None**    **Daily**    **Weekly**    **Occasionally**

*How often would you use a bike/pedestrian crossing for Mobile River?*

**None**    **Daily**    **Weekly**    **Occasionally**

*How did you hear about this meeting?*

**Letter**    **E-mail**    **Newspaper**    **Flyer**    **Word of Mouth**    **TV**    **Other** \_\_\_\_\_

**COMMENTS:**

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**(CONTINUE ON BACK IF NECESSARY)**

One item of special importance to us is the identification of sites on our projects where contamination may have occurred due to the storage, handling, or disposal of hazardous materials. It is important that we identify such sites prior to acquiring right-of-way. Otherwise, taxpayers may incur responsibility for the cleanup of any such contamination. We appreciate any information given us on sites adjacent to this project where the following materials may have been stored, handled, spilled/leaked, or disposed of: plating waste, solvents and petroleum products, dry cleaner fluids, fertilizers, pesticides, foundry waste, paints, or other suspect materials. We especially appreciate help in identifying old gasoline station sites.

*Please give any comments you have concerning hazardous wastes:*

\_\_\_\_\_  
\_\_\_\_\_

**(CONTINUE ON BACK IF NECESSARY)**

**SIGNED** \_\_\_\_\_

**PLEASE DROP IN COMMENT SHEET BOX AT PUBLIC MEETING  
OR MAIL BY 5:00 P.M. FRIDAY NOVEMBER 7<sup>th</sup>, 2014 TO:**

Mr. Vincent E. Calametti, P.E., Region Engineer  
Southwest Region  
Office of Region Engineer  
1701 West I-65 Service Road  
Mobile, Alabama 36618

**YOU MAY ALSO FAX OR E-MAIL US YOUR COMMENTS:      FAX: (251) 473-3624 , E-MAIL: [hossaina@dot.state.al.us](mailto:hossaina@dot.state.al.us)**

## **APPENDIX P**

### **Disposition of Substantive Comments on Signed DEIS**



**Disposition of Substantive Comments Received on the I-10 Mobile River Bridge and Bayway Widening Draft Environmental Impact Statement, Signed July 22, 2014**

Commenter	Comment	Response
<p>Advisory Council on Historic Preservation Letter Dated October 9, 2014</p>	<p>It is our opinion that several historic properties will be directly and indirectly adversely affected. At this time, we recommend that FHWA and ALDOT revise the effect determination for the undertaking and proceed to the next step in the Section 106 process.</p>	<p>See Section 4.13 of the Supplemental DEIS for a discussion of effects on historic properties. By letter dated May 18, 2015, FHWA issued a letter stating the proposed project would be have adverse visual effects on Church Street East and Lower Dauphin Historic Districts.</p>
	<p>Noting that there are multiple National Historic Landmarks (NHLs) identified within the Area of Potential Effects (APE) for the undertaking, we also recommend that you contact the National Park Service (NPS) and invite them to enter the consultation pursuant to 36 C.F.R. §800.10(c) of our regulations.</p>	<p>The National Park Service accepted the invitation to serve as a Section 106 Consulting Party on July 10, 2018.</p>
	<p>Develop a Memorandum of Agreement (MOA) or Programmatic Agreement (PA) as appropriate.</p>	<p>The Draft Section 106 MOA is included in Appendix L of the Supplemental DEIS. A Final Section 106 MOA will be executed and included in the FEIS/ROD.</p>
<p>Mobile Historic Development Commission Letter Dated October 27, 2014</p>	<p>ALDOT has not yet made available the vibrations study or the ongoing Phase 3 archaeological investigations which are referenced in the Draft EIS. Therefore, at this time the MHDC cannot agree with ALDOT’s finding of no adverse effect, and we request additional time to comment on the Draft EIS following release of the vibrations study and the Phase 3 archeological investigations.</p>	<p>The Vibrations Study is contained in Appendix M of the Supplemental DEIS. The effect determination was revised to adverse visual effects on the Church Street East and Lower Dauphin Historic Districts by letter dated May 18, 2015. The status of archaeological surveys has been updated and is discussed in Section 4.14.6.</p>
	<p>However, the MHDC recognizes the urgent need to move this project forward, and requests that ALDOT enter into a written programmatic agreement to address mitigation of negative impacts on historic resources, including both currently identified impacts and those not yet identified.</p>	<p>The Draft Section 106 MOA is included in Appendix L of the Supplemental DEIS. The Final Section 106 MOA will be included in the FEIS/ROD.</p>
	<p>With the bridge still to be designed, the MHDC believes that most of its concerns could be handled through a programmatic agreement that will allow the bridge to proceed. These are:</p>	

Commenter	Comment	Response
	<p>1. ALDOT will continue to consult with the MHDC throughout the design and construction of the bridge. The historic significance of the bridge location and the impact on the viewshed from historic resources should be considered in the design process.</p>	<p>The Draft Section 106 MOA in Appendix L outlines a process for involving the Consulting Parties in the design process. ALDOT has developed an Aesthetics Steering Committee that has provided input on the Aesthetic Guidelines for the project and provided feedback to ALDOT on the preliminary aesthetic and landscaping plans from the proposing teams. Section 6.8 of the Supplemental DEIS contains more detailed information on the role of the Aesthetic Steering Committee and its role in the design process.</p>
	<p>2. Vibrations from construction may harm historic resources. Such harm would be a direct adverse effect on historic resources. Constant monitoring during construction and beyond must be done to ensure there will be no damage to historic resources. Monitoring must be performed within Oakdale and East Church Street Historic Districts and at nearby buildings of historic importance. Examples of buildings requiring monitoring are: St. Matthews Church; Council Elementary School; Prince of Peace Church; the Conde Charlotte House and the Union Hall. Other sites and criteria for site monitoring will be finalized during creation of the programmatic agreement. Specific measures, testing procedures and methods of documenting and monitoring the condition of critical structures must be outlined in the programmatic agreement.</p>	<p>The Draft Section 106 MOA specifies requirements and commitments for vibration monitoring before, during, and after construction. It also specifies the requirements for repairing any damage that may occur related to vibrations during construction. See additional information contained in Section 4.14.3 and Appendix M.</p>
	<p>3. The programmatic agreement must include a plan of action in the event that vibrations are detected at potentially harmful levels. The plan must include steps to prevent or halt any damage to historic structures. ALDOT must repair any damage done to historic resources.</p>	



Commenter	Comment	Response
	<p>4. The Commission is concerned about environmental issues that would diminish the integrity and value of various historic resources, both during and after construction: noise; particulates; stormwater; viewshed and lighting. The mitigating measures proposed in the Draft EIS must be documented in the programmatic agreement and implemented before, during and after construction.</p>	<p>The Draft Section 106 MOA specifies mitigation measures to be implemented as part of the proposed project. Additional details on impacts and mitigation can be found in Section 4.8.2 Stormwater Runoff, Section 4.10 Noise Analysis, Section 4.11 Air Quality, Section 4.12 Lighting Conditions, and Section 5.0 Draft Section 4(f) Evaluation.</p>
	<p>5. Actual, “as built” measurements must be documented to insure that measurable impacts (noise, lighting, particulates, etc.) are within the levels stated in the Draft EIS. If the impacts exceed the estimates in the Draft EIS, additional mitigation measures will be implemented.</p>	<p>The potential impacts in the environmental document are based on the anticipated conditions in the 2040 design year and tend to be overestimated when more detailed analyses are not available. The Supplemental DEIS and Draft Section 106 MOA include mitigation measures to be implemented as part of the proposed project. If design changes result in impacts that exceed the estimates evaluated in the FEIS/ROD, a re-evaluation of or supplement to the approved NEPA document will be required.</p>
	<p>6. Lighting components of the project should not be finalized until near the end of the design and engineering process in order to take advantage of the latest technological developments.</p>	<p>As noted in Section 4.13 of the Supplemental DEIS and the Draft Section 106 MOA, lighting fixtures will not be selected until late in the design process in order to use the most current technology available to help minimize light pollution resulting from roadway, bridge, and aesthetic lighting.</p>
	<p>7. The programmatic agreement must contain a specific plan for minimizing adverse effects to historic resources during construction, including specific plans for minimizing noise, particulate, stormwater and lighting impacts.</p>	<p>The Draft Section 106 MOA specifies mitigation measures to be implemented as part of the proposed project. Anticipated construction impacts for the project as a whole are discussed in Section 4.14.</p>
	<p>8. The viewshed from historic resources will be compromised, particularly where open spaces or vistas occur and where there are views from multi-story buildings. This will diminish the integrity of the various historic resources’ setting and feeling, which will lead to a direct adverse economic impact on the property values of historic resources. The programmatic agreement must contain a</p>	<p>The Draft Section 106 MOA describes measures to avoid, minimize, and/or mitigate for potential effects on historic properties. An Aesthetic and Landscape Management Plan will be required as part of the design process and will be implemented by the Concessionaire. Potential negative economic impacts to historic resources have not been identified.</p>

Commenter	Comment	Response
	<p>plan for diminishing or offsetting this impact on the viewshed. A detailed landscaping plan could alleviate some of this effect. Any landscaping or tree plantings implemented by ALDOT or identified in the Draft EIS as blocking the view of the bridge from historic resources must be maintained or replaced during the four years following completion of the bridge and plantings, whichever is later in order to ensure their ongoing viability.</p>	
	<p>9. The MHDC takes no position on pedestrian or bike access on the bridge. If a walkway/bikeway is included, the increased auto, bike and pedestrian traffic will impact historic resources. Proper planning is needed to assure the minimization of any negative impacts. The MHDC must be included in the planning, locating and design of any parking area or integrated pathway plan.</p>	<p>Comment noted. Section 3.8 and Appendix B of the Supplemental DEIS discuss the proposed bicycle/pedestrian facilities.</p>
	<p>10. Additional signage will offset any loss of tourism and other negative economic impact on historic resources. The MHDC requests that a signage plan be developed to alert motorists of the historic districts and attractions, and the signage plan will be included in the programmatic agreement.</p>	<p>A signage plan will be prepared as part of the design process. A draft signage plan has been prepared and is included in the Draft IMR. Section 4.3.1 of the DEIS and Section 4.4.3 of the Supplemental DEIS discuss potential impacts on tourism, and Section 4.4.8 of the DEIS discusses heritage tourism. Potential negative economic impacts to historic resources have not been identified.</p>
	<p>11. The bridge will traverse areas of historic significance, including identified historic resources. The programmatic agreement must include a plan and resources to redevelop the area under and surrounding the bridge to mitigate negative economic impact to historic resources.</p>	<p>The only area of historic significance that would be traversed (passed over, along, or through) is the BAE Maritime Historic District with Alternative C. The remainder of the Build Alternatives would not traverse areas of historic significance. Potential negative economic impacts to historic resources have not been identified. Development of ALDOT's right-of-way underneath and surrounding the bridge will be part of the design process.</p>

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	<p>The Commission believes a programmatic agreement would be of benefit to all concerned parties. By allowing the Commission and other Consulting Parties to be part of the design process, many of the concerns expressed in this letter could be minimized.</p>	<p>A Draft Section 106 MOA has been developed in consultation with the Section 106 Consulting Parties and is included in Appendix L of the Supplemental DEIS. The MOA contains mitigation measures to address concerns raised by Consulting Parties. A Final Section 106 MOA will be executed and included in the FEIS/ROD.</p>
<p>Mobile Baykeeper Letter Dated November 3, 2014</p>	<p>We recommend in depth study of potential stormwater and spill runoff impacts from the Bridge and Bayway to Mobile Bay and Mobile River to be included in project development as well as in the Final Environmental Impact Statement. Management of stormwater runoff on roadways and bridges is of great importance to Mobile Baykeeper.</p>	<p>A Bridge Stormwater Runoff Treatment Technical Memorandum has been prepared and is included in Appendix H. It includes references and citations to NCHRP Report 778 of NCHRP Project 25-42 and other scientific studies. NCHRP Report 778 is a comprehensive report and guide for managing bridge runoff to protect environmental quality and meet regulatory requirements. The Technical Memorandum addresses the concerns of this comment and includes Alabama Department of Transportation (ALDOT) Environmental Stewardship Commitments summarizing ALDOT's Environmental Stewardship Commitments and mitigation measures for stormwater impacts for this project. Section 4.8.2 of the Supplemental DEIS includes more details on potential impacts and plans to address stormwater runoff.</p>
	<p>The proposed project will create 105 acres of impervious surfaces, with 95% of the constructed area over water and wetlands. We categorically disagree with the statement, on page 99 of the DEIS, that this constructed impervious surface will not create additional runoff. Additionally, the statements on page 99 of the DEIS, that "with improved traffic flow, it is anticipated that the amount of pollutants deposited on the roadway, which result from normal traffic, should be reduced due to improved engine fuel burning efficiency and a decrease in the potential for oil or other contaminants that leak from vehicles during traffic delays" are overly optimistic, at the very least. Though the bridge and Bayway widening as proposed is intended to cause an</p>	<p>Section 4.8.2 of the Supplemental DEIS includes more details on potential impacts and plans to address stormwater runoff. A Bridge Stormwater Runoff Treatment Technical Memorandum has been prepared and is included in Appendix H. It includes references and citations to NCHRP Report 778 of NCHRP Project 25-42 and other scientific studies. NCHRP Report 778 is a comprehensive report and guide for managing bridge runoff to protect environmental quality and meet regulatory requirements. The Technical Memorandum addresses the concerns of this comment and includes Alabama Department of Transportation (ALDOT) Environmental Stewardship Commitments summarizing ALDOT's Environmental</p>

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	<p>end to traffic delays on I-10, the fact remains that contaminants, such as dirt, dust, rubber, antifreeze, engine oil, and litter, from vehicles and roadway construction are generated and washed from all roadways. Additionally, there is the matter of projected increased numbers of vehicles on the roadway which will mean a greater number of potential sources of pollution crossing Mobile River and Mobile Bay.</p>	<p>Stewardship Commitments and mitigation measures for stormwater impacts for this project.</p> <p>Section 4.8.1 of the Supplemental DEIS includes an update regarding 303(d) impaired waterbodies. The portion of Mobile River that is listed on the 2018 303(d) impaired waterbodies list would not be crossed by the proposed project. A Bridge Stormwater Runoff Treatment Technical Memorandum has been prepared for this project and is included in Appendix H. It includes references and citations to NCHRP Report 778 of NCHRP Project 25-42 and other scientific studies. NCHRP Report 778 is a comprehensive report and guide for managing bridge runoff to protect environmental quality and meet regulatory requirements. The Technical Memorandum addresses the concerns of this comment and includes Alabama Department of Transportation (ALDOT) Environmental Stewardship Commitments summarizing ALDOT's Environmental Stewardship Commitments and mitigation measures for stormwater impacts for this project.</p>
<p>Pollution prevention structures as well as pollution collection and management systems should be evaluated as integral parts of the development of this project.</p>		
<p>We recommend incorporation of stormwater runoff capture and containment methods into Bridge design, construction, and operation to reduce runoff pollution to Mobile River and Mobile Bay. We support significant study and implementation of stormwater capture and runoff containment methods in transportation project design, construction practices, and the final build.</p>		
<p>As noted on page 98 of the DEIS, the project area contains three water bodies, Mobile River, Joes Branch and D'Olive Creek, that are listed as impaired on the Alabama Department of Environmental Management's 2012 303(d) list. The updated ADEM 2014 303(d) list still contains these waterways. Mobile River has been found to be polluted with metals (specifically mercury from atmospheric deposition), and a pollutant limit (TMDL) determination is due to be completed in 2020. Joes Branch has pollution impacts from siltation due to land development and is scheduled to have a TMDL established in 2018. D'Olive Creek is significantly impacted from siltation from development with a TMDL date of 2018. Two of the three of these waterways' impairments are due to runoff and stormwater pollution. In order to not exacerbate the pollution issues in these waterways, runoff capture and containment from the Mobile River Bridge and</p>		

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	Bayway should be an integral part of project evaluation and final construction.	
	Runoff containment infrastructure could also aid in management of major spills from entering the Mobile River and Mobile Bay.	<p>Runoff containment infrastructure is not proposed on the bridges.</p> <p>NCHRP Report 778 discusses bridge spill frequency specifically and states that spills of hazardous materials with discharge to waterbodies are extremely rare, less than 0.01 percent of all reported spills for the period of 2003 to 2012. Risks from spills vary depending on the types and quantities of materials released during the spill. While the probability of a spill happening is limited, ALDOT has committed to providing resources to be mobilized and utilized to contain spills that could occur on the main span and/or Bayway bridges and other portions of the proposed project. A Spill Containment Plan will be developed to outline a plan to assist first responders in any containment/cleanup efforts should a spill occur. The plan will be reviewed and updated annually to incorporate advances in technological developments related to spill containment measures.</p>
	If the recommended route is approved, construction of this roadway will impact 1.7 acres of wetlands and roughly 67 acres of essential fish habitat. Specifically, 63.25 acres of shallow water habitat, and associated submerged aquatic vegetation, will be impacted from shading of the waters of Mobile Bay due to Bayway widening. Habitat loss will occur in the Bay due to constructing a wider Bayway, and the direct impact of the project should be mitigated as close as possible to the impacted area.	The Draft Mitigation Plan prepared in consultation with the agencies having jurisdiction over the potentially impacted resources identifies a mitigation site north of the Bayway in close proximity to the proposed project. See Appendix F of the Supplemental DEIS.
	Mitigation of such impacts are critical and Mobile Baykeeper strongly advocates for bridging areas of the Mobile Causeway to allow more water flow between the Delta and Mobile Bay. Mobile Baykeeper has long advocated to restore historic	Bridging areas of the Causeway is beyond the scope of the Mobile River Bridge project. Mitigation measures developed in consultation with the resource and regulatory agencies for impacts to wetlands, SAV, and EFH resulting

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	<p>hydrologic connectivity between the Mobile/Tensaw Delta and Mobile Bay. Reconnecting the tidal exchange will ensure the productivity of the estuary. The exchange will have significant ecological benefits to the water, flora and fauna that live within Alabama's significant estuary, which will be impacted by construction of this waterway. Addressing upstream and downstream modifications to Mobile Bay that have altered ecological productivity can create habitat for wildlife impacted by the construction and operation of the widened Bayway.</p>	<p>from this project are contained in the Draft Mitigation Plan in Appendix F.</p>
	<p>We also join many other voices in advocating for pedestrian and bicycle friendly transportation projects.</p>	<p>Section 3.8 and Appendix B of this Supplemental DEIS discuss the bicycle/pedestrian facilities that will be included in the proposed project.</p>
<p>United States Coast Guard Letter Dated November 4, 2014</p>	<p>In order for the Coast Guard to adopt the bridge-related portions of the FEIS as part of the Coast Guard bridge permitting process, please address the following comments:</p>	
	<p>Please include in the FEIS a description of stormwater management plans for the bridge.</p>	<p>Section 4.8 discusses stormwater runoff for roadways and bridges. This discussion has been expanded since the DEIS. Appendix H of the Supplemental DEIS contains a technical memorandum regarding stormwater plans for the main span and the Bayway bridges. Section 4.14.1 contains a discussion about construction sediment and runoff.</p>
	<p>Section 4.12.3.3 mentions the fact that several of the submerged aquatic vegetation (SAV) species are invasive. Additionally, <i>Phragmites australis</i> is also likely an invasive plant. Executive Order 13112 directs federal agencies to prevent the introduction and spread of invasive species. Please describe in the FEIS the steps that will be used in this project to ensure that these species do not spread.</p>	<p>Section 2.2 of the Draft Monitoring Plan in the Draft Mitigation Plan states that invasive plants will be physically removed if found, although common cane found along the fringing shoreline may be left in place if it contributes to erosion control.</p>
	<p>In Section 4.12.4, the impacts to wetlands from the bridge/Bay way piers are termed "much less" than the overall width of new roadway that they will be supporting. Please state the total area of SAV and wetlands that will be lost due to the bridge/Bayway piers.</p>	<p>Section 4.7 of the Supplemental DEIS states the total acres of wetlands and SAV that would be impacted by the proposed project as of surveys conducted in 2016. Updated surveys will be performed prior to construction.</p>

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	<p>In Section 4.12.5, shading impacts are discussed solely in terms of area. What are the expected biological and ecological function impacts? Will existing SAV die from lack of sun? Will species be less able to make use of such areas, especially if the SAV is important habitat for them?</p>	<p>For the purposes of the Supplemental DEIS and the basis of the Draft Mitigation Plan, it is assumed that all of the SAV underneath the proposed bridges will be completely lost. Some species will continue to use the area as habitat. To mitigate for adverse impacts, a marsh island will be constructed to replace the habitat that is lost as a result of construction. Appendix F of the Supplemental DEIS contains more details on the proposed mitigation, which has been prepared in consultation with the agencies having jurisdiction over wetlands, SAV, and EFH. The USCG has been involved in the development of the Draft Mitigation Plan presented in Appendix F.</p>
	<p>In Section 4.12.5, it is stated that EFH impacts from the bridge are considered minimal, but the total area of habitat permanently lost to bridge/Bayway piers is not stated. Please include this information in the FEIS.</p>	<p>Section 4.7.3 of the Supplemental DEIS states the total acres of EFH that would be impacted by the proposed project.</p>
	<p>Section 4.12.5 needs a fuller discussion of EFH impacts. Impacts on aquatic animals from construction noise and suspended sediments are not discussed, and this is not currently sufficient for the USCG to adopt the document.</p>	<p>Pile driving operations may result in impacts to aquatic species, including fish, that inhabit the area where construction will take place. Coordination with the USFWS indicates that these impacts would be temporary in nature, and special mitigation measures are not required (Appendix I). In order minimize potential impacts on aquatic species, the Concessionaire will be required to use a ramp-up pile driving procedure will be used during the installation of piles in water. This procedure allows for a gradual increase in noise levels so that species have time to leave the area prior to full noise levels being released by pile driving. Additionally, the Concessionaire will be required to implement an Erosion and Sediment Control Plan to minimize the suspension of sediments in the water during construction to minimize impacts on aquatic species. See Section 4.14.2 of the Supplemental DEIS.</p>



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	<p>In Section 4.12.6, it is stated that in the FWS letter of 14 May 2003 providing the Service's Biological Opinion, there were four listed species identified by the FWS as possibly occurring in the project area. However, their letter only discusses the effects on the Alabama red-bellied turtle and the Gulf sturgeon, and does not mention any other species. Section 4.12.6 also states that the manatee was brought in as a species to be considered on 11 January 2007, but meeting notes dated a year earlier, on 18 January 2006 (Appendix A), noted the manatees at that time. Please include the correct citation for the four species and update dates as necessary.</p>	<p>The USFWS Biological Opinion only discusses the Alabama red-bellied turtle and the Gulf sturgeon because those are the only two that may experience an incidental take as part of the proposed project. Appendix I of this Supplemental DEIS contains a Biological Assessment for the manatees, as well as additional correspondence with the USFWS that has occurred since the DEIS was approved.</p>
	<p>Ensure that migratory bird impacts are thoroughly discussed in the FEIS in section 4.12.6, with all appropriate USFWS input included and coordination completed.</p>	<p>As noted in the telephone memo with the USFWS in Appendix I and in the environmental commitments, USFWS acknowledges that the project must be designed to meet FAA regulations. ALDOT's commitment to request the maximum allowable duration of strobe (beacon) lighting on the bridge tower as part of the FAA permitting process will satisfy USFWS's request regarding migratory birds. No further input or requirements from the USFWS is expected on this issue.</p>
	<p>The section on GHG and Climate Change seems to be copied from some other document since it refers to the DOT as "we." Recommend changing to the third person since DOT isn't the author of this DEIS, and it is confusing to the reader.</p>	<p>New language in Supplemental DEIS has been included to describe more current studies and regulations; however, the DEIS language remains as it was at the time the document was signed.</p>
	<p>Section 4.16.1 refers to the FHWA as "their" and gives the impression that FHWA is not the responsible agency. Recommend changing the language to "we" and "our" as necessary or deleting the pronouns and only referring to "the FHWA".</p>	<p>New language in Supplemental DEIS has been included to describe more current studies and regulations; however, the DEIS language remains as it was at the time the document was signed.</p>
	<p>Recommend inserting the Wetland Determination and SAV Survey as a separate appendix for easy reference.</p>	<p>The Draft Mitigation Plan, which contains updated wetland and SAV survey results, is included in this Supplemental DEIS as Appendix F.</p>

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	Recommend inserting the EFH Assessment as a separate appendix for easy reference or combine with the Wetland Determination into one appendix.	More information regarding EFH has been added to Section 4.7 of the Supplemental DEIS to address comments received from USCG.
	Please include the Biological Assessment provided to the USFWS in 2002 in an appendix.	Biological Assessment will be submitted to USCG under separate cover with the permit application to the USCG.
	Please update the status of NMFS consultation in the FEIS and include all correspondence.	NMFS consultation is included in Section 4.7 and Appendix F of the Supplemental DEIS.
	Appendix K could be renamed to highlight the fact that it also includes the shadow study.	The Supplemental DEIS did not revise the shadow study from the DEIS. Therefore, no changes to this appendix in the DEIS were made.
	If mitigation will take place as suggested by FWS in 2001/2002, please describe it in the FEIS.	The Draft Mitigation Plan is included in Appendix F of the Supplemental DEIS.
	USACE's jurisdictional determination expired 5 years after issuance, which occurred on 28 February 2002 (Appendix A). Has a new JD been procured from USACE?	A new jurisdictional determination will be required with the updated wetland and SAV surveys that will be performed as part of the Final Mitigation Plan once more detailed design plans are available. The Final Mitigation Plan will be prepared prior to obtaining permits to begin construction.
	Please provide the MOA or PA with the Alabama SHPO when finalized.	The Draft Section 106 MOA is included as Appendix L of the Supplemental DEIS. A Final MOA will be included in the FEIS/ROD.
	Ensure that the proper waterways are annotated as there are two waterways that this bridge crosses; the Tensaw River, mile 0.3, the Apalachee River, mile 0.0 and the Blakeley River, 0.4.	Per USCG comment, references to waterway crossings have been added to Section 4.2 of the Supplemental DEIS.
	After attending the September 23, 2014, Consulting Parties meeting, and after touring the area with representatives of the Advisory Council on Historic Preservation, the Mobile Historic Development Commission, and the National Trust for Historic Preservation, the Alabama Historical Commission continues to express our concerns over potential visual, auditory, and vibratory adverse effects on historic properties.	By letter dated June 11, 2015, the Alabama Historical Commission concurred with FHWA's finding of adverse visual effects on the Church Street East and Lower Dauphin Historic Districts.

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Alabama Historical Commission Letter Dated November 4, 2014	While the DEIS addresses certain environmental commitments, we would prefer that FHWA and ALDOT identify and address all potential adverse effects to historic properties in either a Programmatic Agreement and/or a Memorandum of Agreement.	The Draft Section 106 MOA is included in Appendix L of the Supplemental EIS.
	We also understand that Phase II Archaeology will be coordinated with this office and performed as part of the investigation of the Preferred Alternative in the FEIS.	Archaeological surveys completed to date have been coordinated with the SHPO and will continue to be coordinated with the SHPO when they are all completed. The FEIS/ROD will include a summary of the findings of all archaeological surveys performed at the time the FEIS/ROD is completed. The Draft Section 106 MOA in Appendix L includes a discussion of future plans for archaeological investigations.
Alabama Power Company Letter Dated November 5, 2014	APC does not have an opinion as to whether such a [pedestrian and bicycle] path should be incorporated into the project, other than to note that such a path is not part of the project's stated purpose. Nevertheless, it would be prudent for ALDOT to carefully consider this issue. To that end, whereas the financial costs of including such a path can be objectively estimated, the purported benefits (economic and otherwise) of such a path should also be objectively identified and considered. On a related note, such a path, with the scenic visits that it will present, could provide an opportunity (through informational panels and kiosks and the like) for mitigation opportunities to offset the project's potential impact to historic resources.	ALDOT's decision to include bike/ped facilities on the project was based on a variety of factors discussed in Appendix B. There is the potential for economic benefits by including bike/ped facilities on the project, but that is not part of the purpose and need of the project, nor is it required in this analysis. ALDOT's bike/ped decision is responsive to public input, economic, and engineering analyses as discussed in Section 3.8 and Appendix B. ALDOT's bike/ped commitment includes construction of a belvedere (observation platform) on the new bridge on the west side of the river, which will provide scenic views.
	It has been APC's experience that the proper implementation of Best Management Practices, coupled with meeting the conditions of the Alabama Department of Environmental Management's Clean Water Act Section 401 Water Quality Certification, will successfully address water quality concerns (to include stormwater runoff) associated with the Project.	Comment noted.

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	<p>Technically speaking, the DEIS' discussion of the critical habitat designation for the Gulf sturgeon on page 116 is incorrect. The DEIS states that critical habitat for the Gulf sturgeon was proposed on March 19, 2003, by the U.S. Fish &amp; Wildlife Service; in actuality, critical habitat was, in fact, designated at that time and, in that same action, the U.S. Fish &amp; Wildlife Service affirmatively rejected suggestions that Mobile Bay be included in the Gulf sturgeon's critical habitat.</p>	<p>Comment noted. Appendix A of the DEIS contains the USFWS's Biological Opinion and Incidental Take Permit for Gulf sturgeon. No change is required.</p>
	<p>Section 4.4 of the DEIS addresses the socio-economic environment. A more robust analysis of this environment would explore whether improved commute capabilities between Mobile and Baldwin County will accelerate "suburban flight" from Mobile to the communities on the Eastern Shore, and the impact that such migration will have on communities, schools, tax bases, and the like in Mobile and Baldwin Counties.</p>	<p>Section 4.19 of the DEIS contains an evaluation of indirect and cumulative impacts associated with the proposed project. The Traffic and Revenue study (available on the <a href="http://www.mobileriverbridge.com">www.mobileriverbridge.com</a> website) performed for the proposed project and does not indicate that additional "suburban flight" will occur.</p>
	<p>To the extent that the GulfQuest Maritime Museum is cited as a change in the environmental setting that impacts the alternatives analysis, the Final EIS should candidly and expressly reflect the current status of that facility. For example, the DEIS is likely inaccurate on page 46 when it states that the GulfQuest Maritime Museum "is scheduled to open in late 2014." Such inaccuracies may not, from a practical sense, be relevant to ALDOT's decision making with respect to the Project, but they will likely be cited by opponents of the Project as symptomatic of flawed analysis. By scrubbing and updating the DEIS to reflect the current status of Mobile's waterfront, ALDOT will deny such arguments to the Project's opponents.</p>	<p>The status of GulfQuest has been updated in Section 4.1 of the Supplemental DEIS.</p>
	<p>APC encourages ALDOT and other interested parties to recognize that the historic integrity of the Mobile viewshed has been compromised by modern projects; thus, to argue that the existing viewshed possesses integrity capable of being adversely impacted is not particularly compelling.</p>	<p>FHWA has issued a determination of adverse visual effects on the Church Street East Historic District and Lower Dauphin Historic District. The Draft Section 106 MOA presented in Appendix L of the Supplemental DEIS contains a description of the mitigation measures that have been</p>

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		developed in consultation with the Section 106 Consulting Parties having jurisdiction over or interest in historic properties that could be affected by the proposed project.
	ALDOT is correct to address environmental justice concerns in the DEIS. Perhaps a follow-on meeting in the Central Texas Street Neighborhood community is warranted as part of the ongoing EIS process (given that the last meeting reportedly occurred on August 31, 2010).	Section 4.6 and Appendix E of the Supplemental DEIS contain updates to the Environmental Justice Assessment and a summary of EJ community workshops conducted in June 2018.
	Understandably, the DEIS, in Section 4.9.5, focuses on the project's potential negative impacts to the local community. At the same time, however, relieving traffic congestion between Mobile and Baldwin Counties could have positive impacts on this community as well (i.e., reduction in vehicle-sourced ozone and improved mobility/access to job opportunities in Baldwin County). If ALDOT agrees, then such positive impacts should be factored into the environmental justice calculus as well.	The Supplemental DEIS considers both the positive and negative impacts that could result from the proposed project.
USEPA Letter Dated November 6, 2014	Based on a review of the project impacts, EPA provided detailed comments regarding air quality, cultural resource, water resources, noise and community impacts. The selection of B' relative to the other proposed build alternatives assessed in the DEIS minimizes impacts to communities, cultural resources and the environment. Nevertheless, we recommend that the FEIS describe efforts to further avoid, minimize, mitigate and/or clarify noise, water resource, and cultural resources impacts described in the detailed comments. EPA rates this DEIS as EC-2 i.e., EPA has "Environmental Concerns" and "Additional Information" is requested. EPA's rating system can be found online at: <a href="http://www.epa.gov/oecaerth/nepa/comments/ratings.html">http://www.epa.gov/oecaerth/nepa/comments/ratings.html</a> .	Comment noted.
	Appropriate best management practices should be implemented and described in the FEIS to ensure that impacts to sensitive aquatic resources or species are	A Draft Mitigation Plan for impacts to wetlands, SAV, and EFH is included in Appendix F of the Supplemental DEIS. The USEPA was consulted in the development of the Draft Mitigation Plan. Discussions of BMPs to be implemented

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	<p>minimized and/or appropriately mitigated. EPA should be consulted, when appropriate.</p>	<p>and commitments to minimize impacts on water quality are contained in Sections 4.8 and 4.14.</p>
	<p>The DEIS provides a description of the air impacts that will result from both the construction and the use of the bridges proposed in the project. Mobile and Baldwin are currently in attainment for all National Ambient Air Quality Standards (NAAQS). The DEIS also includes a Carbon Monoxide hotspot analysis despite the area's attainment status. For section 4.17.3, Air Quality Impacts, during Construction, EPA recommends that the project implement overall diesel emission reduction activities through various measures such as: switching to cleaner fuels, retrofitting current equipment with emission reduction technologies, repowering older engines with newer cleaner engines, replacing older vehicles, and reducing idling through operator training and/or contracting policies. EPA can assist in the future development or implementation of these options.</p>	<p>As stated in Section 4.17.3 of the DEIS, the Concessionaire will be required to adhere to Article 107.22 of the State of Alabama Highway Department Standard Specifications, which requires compliance with all state, Federal, and local laws and regulations controlling pollution of the environment, including air pollution.</p>
	<p>Based on the DEIS, concerns remain regarding impacts to cultural resources in Mobile. Cultural resource surveys were conducted and an initial determination of no adverse indirect effects on cultural resources has be[en] made. The preferred alternative is the alternative that is furthest away from historic resources in downtown Mobile and avoids direct impacts to Old Union Hall, which is eligible for the National Register of Historic Places and avoids the use of a Section 4(f) resource. However, EPA notes that FHWA and ALDOT continues to consult with the State Historic Preservation Officer (SHPO) and Section 106 Consulting Parties regarding their persistent concerns and ALDOT will need to conduct additional archeological surveys on some of the alternatives. EPA recommends that the FEIS should document the results of the consultation process and any remaining survey results.</p>	<p>The Draft Section 106 MOA is included in Appendix L of the Supplemental DEIS. A Final MOA will be included in the FEIS/ROD. Additional consultation is documented in Section 6.4 and Appendix L of the Supplemental DEIS.</p>

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	<p>In section 4.12, Water Quality and Biological Resources, the DEIS indicates that lower congestion rates and reduced low-speed and idled traffic are likely to result in decreased leaks of oil and combustion-related pollutants because inefficient combustion at low speeds would be reduced. However, the DEIS should also consider that a widened Bayway is also likely to result in an increased volume of traffic, increasing the number of emission sources contributing to emission-related products in runoff to Mobile Bay and River.</p>	<p>An updated discussion regarding water quality and stormwater runoff is contained in Section 4.9 of the Supplemental DEIS. Appendix H contains a technical memorandum specifically related to bridge stormwater runoff.</p>
	<p>Section 4.12.4 states, "Calculations of wetland impacts have been derived based on the structural dimensions and projected as ground impacts," and refers to Figures 8a and 8b. EPA recommends that the text provide quantified impacts. In addition, the rationale for the "wetlands avoided" area marked on Figure 8a and 8b is unclear. The DEIS should indicate why the wetland avoided areas are much wider than the proposed expanded Bayway.</p>	<p>As noted in Section 4.7 of the Supplemental DEIS, the proposed project would impact approximately 6 acres of wetlands. A total of approximately 55 acres of wetlands have been identified within ALDOT's existing right-of-way, which extends outside of the existing footprint of the Bayway bridges. Wetland impacts have been minimized by avoiding construction outside of the existing footprint of the Bayway bridges, except for in locations in proximity to the interchanges where new ramps are expected to be required to maintain traffic during construction.</p>
	<p>Section 4.12.4.6 indicates that "Further discussion of mitigation alternatives will be conducted with ADEM, USACE, USFWS, and NMFS during development of the mitigation plan." "The draft and final mitigation plans will be developed in consultation with the USACE, USFWS, NMFS, ADEM, and local agencies, as appropriate." EPA has a critical role in reviewing compensatory mitigation proposals. ALDOT should also consult with EPA.</p>	<p>The USEPA has been included in the development of the Draft Mitigation Plan, as noted in Section 4.7 of this Supplemental DEIS.</p>
	<p>The DEIS indicates that construction would occur across impaired waterbodies, the Mobile River, Mobile to Spanish River, Joe's Branch from its source to D' Olive Creek, and D'Olive Creek from its source to D'Olive Bay. Joe's Branch and D'Olive Creek are both listed for siltation. Section 4.17, Construction Impacts, states that, "Best Management Practices will be utilized to control sedimentation and</p>	<p>Section 4.8.2 of the Supplemental DEIS discusses roadway and bridge stormwater runoff, including the requirement for special BMPs due to the project crossing 303(d) impaired waterbodies, which are considered priority construction sites. Specific BMPs will not be determined until the final design phase prior to obtaining construction NPDES permits.</p>



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	stormwater runoff during construction." EPA requests that additional information be provided for review in the Final EIS.	
	Section 4.23 Environmental Commitments states re: Wetland and SAV Surveys: "Appropriate mitigation measures will be developed in consultation with resource agencies including US Corps of Engineer (USCOE), USFWS, NMFS, and ADEM." EPA has a critical role in reviewing compensatory mitigation proposals. Therefore, we recommend that ALDOT should consult with EPA, as well.	The USEPA has been included in the development of the Draft Mitigation Plan, as noted in Section 4.7 of the Supplemental DEIS.
	Section 6.0 makes no mention of EPA as an agency that should be or has been coordinated with.	Coordination with the USEPA is included in Chapter 6.0 of the Supplemental DEIS.
	Section 4.15, Lighting, describes directing lighting such that it does not fall outside of the right of ways. EPA recommends also considering how to limit light pollution that may affect wildlife, such as shielding light that would be directed upward, disrupting owls and other birds active at dusk and night. Also, lighting that would shine over water such that it could disrupt aquatic species activities should be minimized. All of the above should be addressed for construction and permanent lighting.	Section 4.12 of the Supplemental DEIS contains measures specifically required to minimize light spill that may affect birds and aquatic species, as coordinated with the USFWS.
	Section ES-10, Traffic Noise Impacts, indicates that 1,065 noise-sensitive receptors are within the vicinity of the build alternatives. The preferred alternative may result in noise impacts to 275 receptors. These sites either approach or exceed the noise abatement criteria. Although noise barriers were evaluated as part of the EIS, no noise minimization strategies are proposed. Given the magnitude of the impacts, EPA recommends that the FEIS include viable strategies to minimize noise related impacts to local communities.	Noise abatement measures were evaluated as part of the original Traffic Noise Analysis and the Addendum to the Traffic Noise Analysis, which is contained in Appendix J of the Supplemental DEIS. The traffic noise analysis found that noise abatement measures are not reasonable or feasible in accordance with ALDOT's Noise Policy.
	EPA notes the inclusion of environmental justice (EJ) as part of the assessment process. The DEIS indicates that there are census blocks (11, 12 and 13.02 and 15.02) in and around the Central Texas Neighborhood with higher minority and low-	Section 4.6 and Appendix E of the Supplemental DEIS specifically address potential impacts to EJ populations, including a comparison of existing and predicted noise levels and concerns raised by the EJ communities.

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	<p>income populations. In addition, there are two minority citizen organizations - the Central Texas Neighborhood Association and the Down the Bay Community Organization. The preferred alternative avoids relocating minority residents, but the closing to the Texas Street interchange will be inconvenient for residents that use this interchange, and the high noise levels currently experienced by residents in the Central Texas Neighborhood will increase slightly. The DEIS identifies an alternate interchange and provides information regarding various public engagement and outreach activities within the affected communities. EPA notes that some of those targeted outreach events occurred over ten years ago. We recommend that the EJ section of the FEIS summarize the existing noise levels in the EJ communities and summarize any concerns expressed by the community in relationship to the proposed project.</p>	
	<p>While the need for additional vehicle capacity has been clearly demonstrated, EPA strongly encourages the implementation of a "Complete Streets" design in order to provide citizens—within the project limits—with safe and user-friendly facilities to support transit, bicycle, and pedestrian modes of transportation for accessing places along the corridor. These accommodations could also help reduce Mobile Source Air Toxics (MSATs). A recently published study by the Transportation Research Board concluded that buffered bicycle lanes encourage cyclists to ride outside the door zone (see <a href="#">Recommended Bicycle Lane Widths for Various Roadway Characteristics</a> (NCHRP Report 766) and <a href="#">Bicycles 2013: Planning, Design, Operations, and Infrastructure</a> (Journal of the TRB, No. 2387)). EPA encourages the inclusion of this scientific research in developing appropriate urban roadway design. EPA notes ALDOT's commitment to providing pedestrian access across the Mobile River. The DEIS states that this may be via the Cochran Bridge or the Bankhead Tunnel. EPA also notes that</p>	<p>As noted in Sections 3.8 and 6.6 and Appendix B of the Supplemental DEIS, ALDOT has coordinated with stakeholders regarding bicycle and pedestrian facilities to reach a commitment to provide bicycle and pedestrian facilities as part of the proposed project.</p>

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	<p>the public was also interested in having access to the proposed I-10 Mobile Bridge via bicycle. We encourage ongoing coordination with the appropriate stakeholders to ensure that an appropriate solution is reached.</p>	
<p>United States Department of the Interior Letter Dated December 1, 2014</p>	<p>Based on the information provided in the DEIS/Section 4(f) evaluation, the preferred alternative would adversely affect one known National Register of Historic Places (NRHP) eligible resource within the Area of Potential Effect.</p>	<p>As noted in Section 5.0, Alternative C would result in impacts to one Section 4(f) property, the BAE Maritime Historic District. Alternative B would have impacted the Union Hall, but it was demolished by its owner.</p>
	<p>The identified preferred alternative would adversely affect historic properties listed on or eligible for listing for the NRHP and identified as National Historic Landmarks (NHL). Visual adverse effects were identified by the SHPO for the Church Street East Historic District, Lower Dauphin Street Historic District, and the Union Hall. Visual adverse effects were also identified by the SHPO for one NHL; the Old Southern Market and City Hall. Access adverse impacts were identified for one NHL; the USS Alabama Memorial Park.</p>	<p>As noted in Section 4.14 of the Supplemental DEIS and the Draft MOA in Appendix L, Alternative B' (Preferred) would result in adverse visual effects on the Church Street East and Lower Dauphin Historic Districts. Adverse impacts to the USS ALABAMA Battleship Memorial Park have not been identified. Access to the Battleship will remain as it currently exists, and supplemental signs will be placed along the I-10 corridor to direct travelers to Battleship Memorial Park.</p>
	<p>The United States Fish and Wildlife Service (USFWS) entered into Endangered Species Act, Section 7 formal consultation for this proposal in 2003 and issued a biological opinion for identified impacts to the Alabama redbelly turtle and the Gulf sturgeon. Since that time another species, the West Indian manatee (<i>Trichechus manatus</i>) has become more prevalent in Mobile Bay and should be considered in an additional consultation. USFWS has been in contact with the Federal Highway Administration (FHWA) Alabama Division and recommended re-initiation of formal section 7 consultation based on new data. USFWS received FHWA's request, dated September 15, 2014, on October 3, 2014. USFWS has requested FHWA provide a biological assessment for the anticipated impacts to the West Indian manatee.</p>	<p>A Biological Assessment for the manatee is included as Appendix I of the Supplemental DEIS. Additional coordination with the USFWS regarding construction requirements to avoid impacts to the manatee is summarized in Section 4.10 and included in Appendix I of the Supplemental DEIS.</p>

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	The Department does not concur with Section 4(f) approval of this project at this time. We would be pleased to reconsider this position upon completion of the Section 106 process and the resubmission of the final Section 4(f) Evaluation.	An updated Section 4(f) Evaluation is included in Chapter 5.0 of the Supplemental DEIS.
Public Hearing Comments	We would also join the Alabama State Port Authority who has asked that the bridge be a minimum of 215 feet. This height is required to accommodate already existing vessels that could call Mobile in the future. We support the maximum practical air draft clearance and northernmost location practicable. Make sure that the height will work with today's ships, as well as ships tomorrow.	Comment noted. The proposed air draft clearance remains 215 feet above the navigation channel.
	I would like to hear some discussion and possibly something set in stone for repurposing the Wallace Tunnel before construction begins.	The Wallace Tunnel will remain open to vehicular traffic with the proposed project.
	The project planning and design should include both increased need for advanced pathfinder signage, tourism vehicular impacts for on-off ramp access, and growth in the area.	A draft signage plan is included in the Draft IMR for the proposed project, which is available on the project website at <a href="http://www.mobileriverbridge.com">www.mobileriverbridge.com</a> . A final signage plan will be required as part of the design process and will include supplemental signs as necessary.
	Early identification of specific environmentally sensitive areas (ESAs) beyond that built into the current Area Contingency Plan should be conducted. Protection of ESA's must be part of design, monitoring, and budgeting.	The DEIS and Supplemental DEIS identify environmentally sensitive areas that could be included in Federal, state, and local Area Contingency Plans, should the agencies responsible for those plans choose to do so. Including those environmentally sensitive areas in current Area Contingency Plans is beyond the scope of this project.
	Coordination with existing and proposed OPA-90 facility response plans is a consideration.	The proposed improvements associated with this project should have no adverse effect on existing or proposed Oil Pollution Act of 1990 (OPA-90) facility response plans, as access to both roadways and waterways will be provided during construction.
	Mitigation planning should include partnering with the City of Mobile regarding its water quality management programs for the Mobile River and upper Northwest Mobile Bay coastal	The Draft Mitigation Plan for wetlands, EFH, and SAV has been developed in consultation with the agencies having jurisdiction over natural resources and is included in

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	<p>areas, as well as partnering with Mobile and Baldwin Counties, the City of Spanish Fort and Daphne, and with ADEM and ADCNR. Considerations should be made for replenishing historic oyster beds and aquatic spawning areas, opening additional flow regimes for delta through the causeway associated with additional access off &amp; on the expanded Bayway.</p>	<p>Appendix F of the Supplemental DEIS. Additionally, ALDOT has committed to partner with local organizations and/or municipalities in environmental stewardship projects in order to improve water quality in and around Mobile Bay, as discussed in Sections 4.7 and 4.9 of the Supplemental DEIS.</p>
	<p>Runoff of fluid material from the proposed bridge and any expansion of the Bayway should be strictly controlled to prevent additional deleterious chemical mixing of its runoff with the waters of the US. Rubbish and trash from both use and construction should likewise be strictly managed to prevent wind-blown carriage out into inaccessible areas of the marshes and delta and into the waters of the US. There needs to be consideration of untreated roadway stormwater runoff to avoid worsening water quality.</p>	<p>A Bridge Stormwater Runoff Treatment Technical Memorandum has been prepared and is included in Appendix H. It includes references and citations to NCHRP Report 778 of NCHRP Project 25-42 and other scientific studies. NCHRP Report 778 is a comprehensive report and guide for managing bridge runoff to protect environmental quality and meet regulatory requirements. The Technical Memorandum addresses the concerns of this comment and includes Alabama Department of Transportation (ALDOT) Environmental Stewardship Commitments summarizing ALDOT's Environmental Stewardship Commitments and mitigation measures for stormwater impacts for this project.</p>
	<p>During construction, travel and access will be impacted. ALDOT must be prepared to add additional manpower and communication capacity to provide public outreach, immediate emergency response, constant multi-platform communications on routing conditions and construction impacts. Assuring that traffic conditions remain safe is of course paramount- but economically, slowdowns and interruptions and re-routing can cost the greater Mobile area and the State of Alabama BILLIONS.</p>	<p>A Comprehensive Public Information and Communications Plan that informs, engages, and responds to the traveling public will be implemented during the design phase and will continue through the construction and post-construction phases. Various forms of media outreach will be used to communicate with the public.</p>
	<p>Little has been presented regarding impacts of the project's Bayway modifications on commuter loadings and general traffic flow for the eastern shore landing. Already a challenging interchange, shifting the "bottleneck" issues from the Wallace Tunnel to the transition at the interchange zone</p>	<p>The IMR examines the potential traffic impacts along roadways, at interchanges, and at local intersections in detail. Interchange modifications required to address traffic at the US-90/US-98 Eastern Shore Interchange are discussed in Section 3.3 of the Supplemental DEIS and in</p>

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	for the eastern shore will exacerbate a growing issue of traffic management at this location.	the IMR, which is available on the project website at <a href="http://www.mobileriverbridge.com">www.mobileriverbridge.com</a> .
	Consideration should be given for growth back toward Loxley and the traffic flow impacts created or mitigated by the addition of the Beach Express interchange, Baldwin County Rd 13 interchange, residential and business growth in Daphne, Spanish Fort and Loxley.	Section 4.19 of the DEIS addresses indirect and cumulative impacts associated with this project and looks at areas in Baldwin County, including Daphne, Spanish Fort, and Malbis. Expanding this analysis beyond the Daphne/Spanish Fort/Malbis area is beyond the scope of this project.
	Grade considerations for the Mobile River Bridge along the B-Prime alignment with the approach beginning at Virginia St are of concern to me in the use by entering truck traffic from the maritime and port businesses. Entering eastbound and immediately dealing with a 4% or better grade situation may create risk with oncoming vehicles already at highway speeds. I have a concern about the horizontal alignments of the alternatives. They are very similar to the existing I-10 tunnel, which has very sharp curves in its east and west entrance. I suggest ALDOT consider highway safety in the alternative selection criteria. ALDOT may use Interactive Highway Safety Design Model (IHSDM) by FHWA.	The proposed Virginia Street Interchange has been discussed with the trucking association and the ASPA to develop a concept that accommodates access required for trucks entering and exiting at Virginia Street. The proposed project will be designed to meet design criteria for safe roadways.
	I believe the preferred placement of the proposed bridge would be detrimental to the historic nature of the downtown area. It would increase noise for the Fort Conde area and other nearby neighborhoods, and the view from many areas would be obstructed. Also, I believe the underside of the bridge where footings would be would become a blighted area.	Noise studies indicate that noise in neighborhoods along the I-10 corridor in proximity to downtown Mobile would actually be reduced with the proposed project (see Sections 4.10 and 5.5.3 and Appendix J of the Supplemental DEIS). The Draft Section 106 MOA in Appendix L and environmental commitments in Section 4.18 of the Supplemental DEIS specify mitigation measures for potential adverse visual effects on downtown historic districts. The Section 106 MOA and environmental commitments require aesthetic and landscaping plans to treat the area underneath the bridge to be developed as part of the design process.
	Please consider closing down the inlet ramp from Water Street during peak times (summer/Thurs-Sat mid-afternoon).	The existing ramp from Water Street to the Wallace Tunnel eastbound has been closed by ALDOT. Additionally, the

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	Remove interchange at tunnel giving this land back to city for development opportunities for our citizens. The on-ramps at the mouth of the tunnel at both the east and west entrances are a major hazard and need to be removed as soon as possible. It needs to have much more gentle curve than the tunnel has so that traffic will not slow down. The speed limit needs to be the same as it is on the interstate highway.	west entrance to the Wallace Tunnel will be improved as part of the proposed project. A description of the proposed Canal Street/Water Street interchange and connectivity to the Wallace Tunnels is included in Section 3.3 of the Supplemental DEIS and is shown on Figure 9.
	We could drastically reduce I-10 congestion without building anything if we seriously addressed behavioral components of traffic jams, namely inconsistent driving speed.	Traffic studies show that the Wallace Tunnels and the Bankhead Tunnel are currently at maximum capacity; therefore, as traffic increases, congestion is expected to continue to increase unless additional capacity to carry traffic is provided. Addressing behavioral issues and inconsistent travel speeds will not address the need of the project which is to increase the capacity of I-10 to meet existing and predicted future traffic volumes.
	I would suggest that sound barriers be placed on the east side of I-10 Virginia St (etc.).	The Traffic Noise Analysis in the DEIS (Section 4.13 and Appendix H) and the addendum to the Supplemental DEIS (Section 4.10 and Appendix J) did not identify any reasonable or feasible noise abatement measures, including noise barrier walls, per the ALDOT Noise Policy.
	Recommend hospitality rest stop at the bridge shoreline complete with state of the art bathrooms, fishing pier, gazebos, benches, parking and running water. Consider putting a tourist attraction observation tower as part of the bridge structure with elevators, parking, etc. nearby.	ALDOT has committed to provide a belvedere (observation platform) on the new bridge on the west side of Mobile River. It will be accessible via elevator and stair tower and will have bicycle and pedestrian facilities to connect to the area underneath the bridge. Additional amenities may be identified as part of the design process. Further development of ALDOT's right-of-way underneath and surrounding the bridge will be part of the design process.
	Please add right turn lanes at exit 38 of I-10. While this is not part of the bridge project, this addition would help traffic flow like the new Water Street diamond. The timing of these improvements will help with the long wait for the bridge.	ALDOT has a separate project (NHF-I010(330)) to construct interchange improvements at Exit 38 (SR-181) on I-10 and to widen I-10 from the east end of the Bayway bridge to 0.5 mile east of SR-181 in Baldwin County. The project is scheduled to be let for construction in 2020.



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	Water body modifications state that a rock armored pier will be built in the Mobile River channel. If these areas were to be disturbed and/or damaged, would there be a potential effect on the naturally occurring Jubilees?	The proposed project will not place piers in the Mobile Harbor navigation channel. There is no evidence to suggest that construction of the project would affect naturally-occurring jubilees.
	Divert the Water St. east bound entrance of the Wallace tunnel to Virginia St. or back around Bienville Sq. to the Bankhead tunnel. This certainly is not a fix to the problem but it would help eliminate the bottle neck at the Wallace entrance.	The existing ramp from Water Street to the Wallace Tunnels eastbound has been closed by ALDOT.
	Primarily I am concerned that the favored route proposed (B preferred) will link into existing bridges, which are at risk in the event of a strong storm. Historically, these types of bridges have not fared well in hurricanes. The existing lanes should be enhanced for storm resistance. Concerned about the Bayway flooding and the difficulty for truckers, especially the ones with hazardous cargo. It's also too low to the water. Please explain to me the logic in the current plan to expand the Bayway and not raise it or completely replace it with a new six or eight lane superstructure.	As discussed in Section 3.3 of this Supplemental DEIS, the Bayway will now be replaced with 8 lanes at an elevation higher than the 100-year storm event.
	There is a curve in B' just like the tunnel. Combined with the incline, traffic will slow down. Truckers will be shifting gears, creating a dangerous situation.	The curves in Alternative B' will meet 60 MPH and higher design speeds. The radii for the curves at the west entrance to the Wallace Tunnels will be modified (increased) during the design of this project.
	We have long asked for an exit ramp coming off the proposed bridge that would be at ground level prior to reaching USS Alabama Battleship Memorial Park. I would like to see an off ramp coming off the east bound lanes of the new proposed bridge to exit somewhere between the Wallace Tunnel exit and the Battleship Memorial Park.	ALDOT evaluated potential ramps to provide more direct access to USS ALABAMA Battleship Memorial Park. These ramps would not meet Federal design criteria for safe roadways and therefore are not proposed. As discussed in Sections 4.13 and 6.0 and Appendix L, coordination with the USS ALABAMA Battleship Memorial Park Commission has occurred, and ALDOT has committed to provide supplemental signage to help motorists navigate to the USS ALABAMA Battleship Memorial Park from I-10.

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	Access to the Battleship USS Alabama can be addressed by signs on I-10 telling motorists to use tunnel routes to visit Battleship Park.	As noted in the Draft Section 106 MOA and the environmental commitments for the proposed project, ALDOT has committed to provide supplemental signs to help motorists navigate to USS ALABAMA Battleship Memorial Park.
	Has there been a traffic study on the amount of traffic to the tunnel/bridge that comes from the north (southbound) on I-65?	The Draft IMR and Traffic and Revenue Study prepared for the proposed project look at where the traffic to the Mobile River crossings originates, including the amount of traffic from I-65.
	Walking and biking trails are imperative! Could a pedestrian facility be attached to the outside of the bridge rather than a bike/pedestrian lane? Close off the old tunnel on weekends and holidays and make it bicycle and pedestrian accessible. Put the pedestrian bike path on the causeway. If pedestrian/bicycle path is included in the project, I think it should be caged to prevent suicide attempts, jumps, dare-devils, and any possibility for the escape of materials that would reach the ground or water below. The walkway and bikeway must have sufficient and convenient (for parking their cars) staging areas on the east and west areas of the bridge where people will start and end their trek across the bridge. If the bridge is ever built and it does not include room for bicyclists and pedestrians to alleviate their long detour through Prichard, then decide which of the three tunnels will be permanently shut down to motor vehicle traffic.	Section 3.8 of the Supplemental DEIS contains a description of ALDOT's commitment to provide bicycle and pedestrian facilities.
	I think access to surface roads is important. Perhaps have multiple exits to either surface roads or existing I-10. Please make downtown accessible.	Downtown will continue to be accessible via I-10 Business with interchanges at Broad Street, Virginia Street, and Canal/Water Streets. The Wallace and Bankhead Tunnels will continue to serve traffic accessing downtown.
	Demonstrate that traffic projections used to justify the construction of the bridge are reasonable and peer-reviewed, based on accurate data and including continuing trends up to the present. Release all data used in those traffic	The Draft IMR and Traffic and Revenue Study prepared for the proposed project are available on the project website at <a href="http://www.mobileriverbridge.com">www.mobileriverbridge.com</a> .

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	projections. Show that you have carefully examined the lowest cost and most cost effective measures to address traffic issues, and have implemented them where possible.	
	I request that you look to the long term and design and build a bridge with 8 lanes.	Constructing an eight-lane bridge has been evaluated. By providing shoulders on the inside and outside of the new bridge, the bridge could be restriped to accommodate more than six lanes in the future if traffic warrants.
	On the Bayway allow for one or more crossovers between eastbound and westbound lanes for emergency vehicles. Add emergency lane to Bayway.	The new Bayway will include inside and outside shoulders as well as emergency crossovers to accommodate emergency vehicles.
	Consider ways to baffle the wind in an effort to keep random wind gusts from toppling 18-wheeler. Some thought needs to go towards wind protection for vehicles (higher guardrails?), specifically for motorcycles. The crosswinds on the Bayway can be treacherous for high-profile motorcycles (touring bikes) and, at a minimum, signs should be erected advising drivers of these vehicles to consider using the causeway.	The project will include an emergency Response Plan that contains detailed operational procedures for the coordination, advance preparation, response, and recovery associated with Emergencies including severe weather events such as fog, high winds, flood, hurricane, fire, ice, sleet, and/or snow. Guard rails of appropriate heights will be included at appropriate locations as required by AASHTO criteria.
	Consider alternative that goes straight across the bay using the north end of McDuffie Island and by Little Sand Island that ends up in the Daphne or Fairhope area, where ALDOT can design an appropriate connection to the existing I-10 on the Eastern Shore. Figure out a way to build a bridge between Theodore and Fairhope. Connect direct from I-10 to Cochrane Bridge direct connect to I-65. Direct trucks to use the Cochran Bridge. Convert the Cochrane bridge connector to a limited access I-210 and fix the Wallace Tunnel approaches. Plan a route further north, if necessary, utilize I-65, Cochrane Bridge, etc. with a better chance of less road damage due to hurricanes. Truck traffic should be routed to I-65, I-165, Bay Bridge Road, then to causeway.	A full range of reasonable alternatives, including 14 different alignments, was evaluated as part of the Alternative Screening Evaluation. The results of the evaluation are discussed in Appendix B of the DEIS. The screening process included northern routes that would use the Cochrane-Africatown USA Bridge, I-110, and I-65, as well as other alignments that would be located north and south of the proposed Alternative B'. The other alternatives were not carried forward for further evaluation for a variety of environmental, economic, and engineering considerations.
	The appearance of the bridge is important. I would like for the design of the bridge to be from an open design competition. Challenge the urban/architectural community	An Aesthetic Steering Committee comprised of members representing the Mobile and Baldwin County communities and the Section 106 Consulting Parties has been developed

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	to provide a more suitable design. Please give serious thought, consideration, and effort in designing an aesthetically pleasing bridge.	and is providing guidance to ALDOT and FHWA on the aesthetics of the proposed project.
	Consider some version of public transit connecting eastern shore communities to the City. The proposed Mobile River Bridge should be designed for MultiModal Transport (as with many new road projects of the future). Install monorail or light rail system that could run on the bridge decking and take commuters back and forth across the bay.	Approximately 60% of the traffic crossing the Mobile River and Mobile Bay in Mobile and Baldwin Counties is through traffic, which means that public transit would not alleviate the congestion problem in this area. Transit was discussed in Sections 3.2.4.1 through 3.2.4.3 in the DEIS.
	Build the bypass from I-10 in Baldwin County to I-65 and build the bypass from I-65 westward Mobile to I-65. We request ALDOT and the Federal Highway Administration review current data on the “northern route’s” capability to address the project’s stated purposed and need, especially given recent economic developments, including the pending openings of Mobile Container Terminal, ThyssenKrupp, Berg Steel and Northrop Grumman/EADS. Why is a northern bypass that connects to I-165 not being considered as an option? It seems that a northern bypass would be less expensive and could possibly be done as “Phase I” to see how much effect it has on relieving the tunnel congestion.	Northern routes were evaluated in the Alternatives Screening Evaluation in the DEIS. These routes do not meet the purpose and need of the project because they would not reduce congestion on I-10 and would not provide additional capacity across the Mobile River. The results of the Alternatives Screening Evaluation in the DEIS are still valid.
	The method of paying tolls (if any) should be done like the new Oakland – San Francisco Bay Bridge where you either must have a Fast-Pay electronic detector in your car, or the system takes your license plate picture and mails you the bill. I urge you to construct this project quickly even if “Toll-By-Tag” is needed to fund it.	The proposed project will include all electronic tolling, which will use toll-by-plate and transponders.
	Recommend Pay-As-You-Go program which is how taxpayers fund road construction and rehabilitation in Mobile County. This project will require a significant drawdown in precious dollars from Highway Trust Fund or other federal and state funding sources, money that might be used for other projects. I am opposed to a toll road. They never go away once the project is paid for and a toll booth would still create	As noted in Section 3.7 of the Supplemental DEIS, FHWA has determined that ALDOT has the authority to toll the Wallace Tunnels. Proposed funding sources for the proposed project are discussed in Section 3.7 of the Supplemental DEIS. Tolling only out of state users would not generate sufficient revenue to fund enough of the

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	<p>a bottleneck as the tunnel does now. A better solution is regulating the traffic flow, such as prohibiting trucks on the bridge during rush hours. It has been speculated that if a toll is put on the bridge, then a toll will also have to be put on the Wallace Tunnel; otherwise most drivers will continue using the tunnel for free. It is unacceptable to put a toll on the Wallace Tunnel no matter the rationale. If you absolutely must have a toll on the bridge to pay for its construction make it for out-of-state licenses plate user only, reserving free express lanes for Alabama drivers.</p>	<p>project to make it viable. Toll booths would not be required, as the project would use all-electronic tolling.</p>
	<p>Move it to an industrial area that does not affect historic districts and would avoid possible social justice litigation.</p>	<p>A full range of reasonable alternatives, including 14 different alignments, was evaluated as part of the Alternative Screening Evaluation. The results of the evaluation are discussed in Appendix B of the DEIS. Alternative B' was identified as the preferred alternative because it moves the alignment further away from historic resources in downtown Mobile while minimizing impacts on the maritime industry.</p>
	<p>Implement staggered work schedules at local maritime cluster industries to decrease existing traffic congestion.</p>	<p>FHWA and ALDOT do not have the authority to require maritime industries to implement staggered work schedules. Additionally, approximately 60% of traffic crossing the Mobile River is through traffic, which would not be subject to the staggered work schedules.</p>
	<p>Make repairs to the west end of the Wallace Tunnel. Restripe the existing Bayway for three east-bound lanes during peak traffic hours with a lighting system used to designate lane cautions and closures. Correct the dangerous east-bound approach and entry into the George Wallace Tunnel by re-designing the angle of that approach and curve. Mr. Gardner believes that eliminating the ability to merge at the tunnel's entrance would greatly reduce traffic bottlenecks and congestion. To address congestion the westbound entrance speed limit can be increased to 55 mph to eliminate the bottleneck that takes place at the middle of the tunnel.</p>	<p>The west (eastbound) entrance to the Wallace Tunnels will be improved as part of the proposed project; however, this improvement will not provide additional capacity across the Mobile River, which is needed to accommodate existing and projected traffic volumes. The existing Bayway has been determined to be vulnerable to storm surge; therefore, it will be replaced at an elevation higher than the 100-year storm event.</p>

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	<p>Postponing the action to build this bridge would increase the incidences of severe traffic congestion and/or gridlock conditions. I-10 is a major transportation connector that fails in Mobile. This failure must be remedied to provide safe, effective and reliable transportation for all users. The failure of I-10 in Mobile causes major impact to us locally. Summer vacation and event traffic backed up on I-10 greatly affects our life and safety. In favor of the bridge for both regional and local reasons.</p>	<p>Comment noted.</p>
	<p>Name the bridge in honor of Cpl. Christopher Edward Mason from Mobile, Alabama.</p>	<p>ALDOT has received the petition in favor of naming the bridge in honor of Cpl. Christopher Edward Mason. Section 23-1-8.1 of the Code of Alabama states that the cost of manufacturing and installing memorial signs cannot be charged to the state of Alabama. Funding a memorial sign is handled with an SWA (Special Work Authorization). Most highway memorial designations come about by way of Legislative Act signed by the Governor, from a resolution prepared by a local representative.</p>
	<p>Regarding Hazardous Materials – look into the City of Mobile parcel key numbers 00945179 and 00945160 at 1051 Ledyard Street. In the past the site has had issues with hazardous materials and was supposed to be cleaned up in 2005.</p>	<p>The proposed project is not expected to acquire property from 1051 Ledyard Street. Therefore, hazardous materials investigations have not been conducted at this parcel as part of the proposed project.</p>
	<p>ALDOT has already purchased land at what I believe to be Alternative B / B' prime location along the west bank of the river. Has the ultimate decision for the bridge location already been made and has expanding road capacity ever proven to be the long-term solution to traffic congestion/highway death rates anywhere in the U.S.?</p>	<p>As discussed in Section 4.5.1 of the Supplemental DEIS, ALDOT has made protective purchases using state funds in order to preserve the proposed corridor of the preferred alternative. In addition, ALDOT has begun to acquire right-of-way for the project in order to streamline the project process to allow demolition and construction to occur as soon as possible after the FEIS/ROD. ALDOT is using state funds to acquire these properties. A Selected Alternative will not be identified until the FEIS/ROD is approved.</p>
	<p>Proper implementation of Best Management Practices, coupled with meeting the conditions of ADEM's Clean Water</p>	<p>BMPs are discussed in Sections 4.9, 4.15, and Appendix H of the Supplemental DEIS.</p>

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	Act Section 401 Water Quality Certification, will successfully address water quality concerns (to include stormwater runoff) associated with the project.	
	The Final EIS should update the current status of the GulfQuest Maritime Museum.	The status of GulfQuest has been updated in the Supplemental DEIS.
	Concerned about impacts to historic resources and impacts have not been given due consideration. The planning does not adequately meet the requirements of federal law that are intended to protect historic resources from unnecessary harm caused by the construction of federal highways. Build Alternative C poses the least threat to historic resources. ALDOT and the USDOT need to consider their obligations under federal law to protect historic resources and to eliminate options A, B, and B' from further consideration. The historic significance of the bridge location and the impact on the viewshed from historic resources should be considered in the design process. Concerned about impacts to the historic Bankhead Tunnel and the effect tolling will have on it.	Alternative C is the only Build Alternative that would result in direct impacts to a NRHP-eligible historic district. Alternative B' would not result in direct impacts to historic structures or historic district, and indirect impacts resulting from Alternative B' are limited to adverse visual effects on the Church Street East Historic District and the Lower Dauphin Historic District. Mitigation measures have been identified in the Draft MOA in consultation with the Section 106 Consulting Parties having jurisdiction over or interest in the potentially-affected historic properties. The viewshed impacts have been and will continue to be considered in the design process, as noted in the Draft MOA. Bankhead Tunnel will continue to remain open to vehicular traffic. It will not be tolled, and traffic projections indicate it will continue to function similar to how it functions today.
	The City of Mobile, City of Daphne, Mobile County, Mobile Area Chamber of Commerce, City of Satsuma, Poarch Band of Creek Indians, Build the I-10 Bridge Coalition, South Alabama Regional Planning Commission, Mobile Metropolitan Planning Organization, Alabama State Port Authority, Cintas, Mobile County Road Builders Association, Delta Bike Project, and Partners for Environmental Progress voiced their support for the project and the Preferred Alternative.	Comment noted.
	Do not tie into the existing Bayway, which is vulnerable to storm surge.	As discussed in Section 3.4 of the Supplemental DEIS, the Bayway will be replaced at an elevation higher than the 100-year storm event.
	ALDOT needs to acknowledge the adverse effects on historic properties and develop ways to modify the project in order	As noted in Section 4.14 of the Supplemental DEIS and the Draft MOA in Appendix L, FHWA has issued a



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	to avoid, minimize, and mitigate those adverse effects. The NTHP is especially concerned about visual impacts and vibration impacts from construction.	determination of adverse visual effects on the Church Street East Historic District and Lower Dauphin Historic District. The Draft Section 106 MOA presented in Appendix L of the Supplemental DEIS contains a description of the mitigation measures for potential adverse visual effects and vibrations. The Draft Section 106 MOA has been developed in consultation with the Section 106 Consulting Parties having jurisdiction over or interest in historic properties that could be affected by the proposed project. The Final Section 106 MOA will be completed and signed prior to the FEIS/ROD.
	Incorporate a high line from Dauphin Street across Water Street around the waterfront into Fort Conde.	This is beyond the scope of this project. However, the proposed project will include opportunities to connect different types of development to the waterfront by removing the existing elevated I-10 structures and providing bicyclist and pedestrian facilities to the riverfront in proximity to the proposed Mobile River Bridge.
	I think the proposed bridge would make downtown Mobile less desirable for both residents and tourists. Anything that will discourage our recent progress and development of downtown and discourage tourism is not conducive to the quality of life. To say that there will be no adverse impact to the B Prime route is irresponsible.	The proposed project has the support of local municipalities, Chamber of Commerce, and numerous area businesses. The proposed project is not expected to discourage progress in downtown nor is it expected to discourage tourism. The potential adverse effects that may result from the Preferred Alternative are documented in the 2014 DEIS and Chapters 4 and 5 of the Supplemental DEIS.
	Recommend a rest stop hospitality center at the bridge shoreline.	The Section 106 MOA and environmental commitments require aesthetic and landscaping plans to treat the area underneath the bridge to be developed as part of the design process. Additional amenities may be identified as part of the design process. Further development of ALDOT's right-of-way underneath and surrounding the bridge will be part of the design process and will be compatible with the City of Mobile's proposed land use and zoning plans for the waterfront area.

Commenter	Comment	Response
	<p>Discussions with local business persons, residents, and government officials indicate all are aware of the need for a resolution to the current traffic problems associated with I-10 and the existing tunnels. However, a few were in favor of the previous proposed bridge plans. The current plans have been received more open-mindedly. Several still refer to a coalition of local businesses called "Keep Mobile Moving." References were made to plans presented by a consultant hired by this group. All requested serious consideration of the plans presented by "Keep Mobile Moving."</p>	<p>A full range of reasonable alternatives, including 14 different alignments, was evaluated as part of the Alternative Screening Evaluation. The results of the evaluation are discussed in Appendix B of the DEIS. The screening process included northern routes that would use the Cochrane-Africatown USA Bridge, I-110, and I-65, and was supported by the Keep Mobile Moving group. The northern route was determined to not meet the purpose and need of the proposed project because it would not increase the capacity of I-10 and it would not draw sufficient traffic off of I-10 to reduce congestion.</p>
<p>Comment received during update to ROW-RA-1 Form</p>	<p>Since I do not believe the public comment period for the Environmental Impact Statement (EIS) has opened and it is now known the Alabama Department of Transportation (ALDOT) must build a new more elevated bayway, could we modify the current plans and build the new I-10 Bridge and Bayway south of the planned route (Attachment 1)? If we follow a more southern route, the existing I-10 could be designated the I-210 for predominately local Mobile traffic with heavier use during high traffic events, maintenance and wrecks. There would be enhanced traffic flow across the Mobile River and Mobile Bay with five lanes of interstate quality road going each direction and save the tax payers \$200-300 million which is an estimated cost to demolish the current I-10 Bayway.</p> <p>I admit I'm not an engineer but the redundancy of five lanes going east and west will make Mobile a safer, healthier community and provide more opportunity for economic growth in both Mobile/Baldwin Counties. The current river front property owned by ALDOT may be more valuable to the maritime industry bringing in more high paying jobs. Trucks leaving the Alabama Port on Virginia Street could take the "Airbus Route" to Brookley and go north on the new Michigan Ave. to I-10 and then turn east to cross the new</p>	<p>As part of the environmental studies conducted for the Draft Environmental Impact Statement (DEIS), an alternatives screening evaluation looked at four alternatives that would cross Mobile Bay on new location south of the Preferred Alternative and tie into Baldwin County. One of those alternatives, Alternative 7, was located on virtually the same alignment you attached to your letter. Alternative 7 was eliminated from further consideration for a variety of reasons, including the following:</p> <ul style="list-style-type: none"> <li>- In order to provide adequate horizontal and vertical clearances to span the Federally-authorized 1,500-foot by 1,500-foot turning basin, the main span length of the bridge would be more than twice the maximum span of the other alternatives considered. A bridge of this magnitude would represent a major engineering challenge and would be very expensive. Shifting the bridge location to the south to avoid the authorized turning basin would require spanning an even greater horizontal distance. Any shifts further to the south would impact flight patterns at the Brookley Airport.</li> <li>- Part of the purpose and need of the project is to</li> </ul>

Commenter	Comment	Response
	<p>bridge. Also, the slope of the bridge could be less on both sides, since there would be more length to rise and fall. Maybe the Alabama Port Authority will consider trading the ALDOT owned property for an easement across the proposed southern route or purchase it for your original investment cost. The Mobile Chamber of Commerce may be able to provide insight into the future of the coal industry, but I believe current data predicts a steady decline.</p>	<p>minimize adverse effects on the maritime industry along the Mobile River. Constructing a new bridge that crosses the McDuffie Coal Terminal would result in adverse impacts on Alabama State Port Authority operations.</p> <p>Obtaining environmental permits to construct a new bridge across Mobile Bay over previously undisturbed open water and water bottoms would be challenging.</p>
<p>January 18, 2018, Letter from Mr. Burt Eichold, Mobile County Public Health</p>	<p>Since I do not believe the public comment period for the Environmental Impact Statement (EIS) has opened and it is now known the Alabama Department of Transportation (ALDOT) must build a new more elevated bayway, could we modify the current plans and build the new I-10 Bridge and Bayway south of the planned route (Attachment 1)? If we follow a more southern route, the existing I-10 could be designated the I-210 for predominately local Mobile traffic with heavier use during high traffic events, maintenance and wrecks. There would be enhanced traffic flow across the Mobile River and Mobile Bay with five lanes of interstate quality road going each direction and save the tax payers \$200-300 million which is an estimated cost to demolish the current I-10 Bayway.</p> <p>I admit I'm not an engineer but the redundancy of five lanes going east and west will make Mobile a safer, healthier community and provide more opportunity for economic growth in both Mobile/Baldwin Counties. The current river front property owned by ALDOT may be more valuable to the maritime industry bringing in more high paying jobs. Trucks leaving the Alabama Port on Virginia Street could take the "Airbus Route" to Brookley and go north on the new Michigan Ave. to I-10 and then turn east to cross the new bridge. Also, the slope of the bridge could be less on both sides, since there would be more length to rise and fall. Maybe the Alabama Port Authority will consider trading the</p>	<p>As part of the environmental studies conducted for the Draft Environmental Impact Statement (DEIS), an alternatives screening evaluation looked at four alternatives that would cross Mobile Bay on new location south of the Preferred Alternative and tie into Baldwin County. One of those alternatives, Alternative 7, was located on virtually the same alignment you attached to your letter. Alternative 7 was eliminated from further consideration for a variety of reasons, including the following:</p> <ul style="list-style-type: none"> <li>- In order to provide adequate horizontal and vertical clearances to span the Federally authorized 1,500-foot by 1,500-foot turning basin, the main span length of the bridge would be more than twice the maximum span of the other alternatives considered. A bridge of this magnitude would represent a major engineering challenge and would be very expensive. Shifting the bridge location to the south to avoid the authorized turning basin would require spanning an even greater horizontal distance. Any shifts further to the south would impact flight patterns at the Brookley Airport.</li> <li>- Part of the purpose and need of the project is to minimize adverse effects on the maritime industry along the Mobile River. Constructing a new bridge that crosses the McDuffie Coal Terminal would</li> </ul>

Commenter	Comment	Response
	<p>ALDOT owned property for an easement across the proposed southern route or purchase it for your original investment cost. The Mobile Chamber of Commerce may be able to provide insight into the future of the coal industry, but I believe current data predicts a steady decline.</p>	<p>result in adverse impacts on Alabama State Port Authority operations.</p> <ul style="list-style-type: none"> <li>- Obtaining environmental permits to construct a new bridge across Mobile Bay over previously undisturbed open water and water bottoms would be challenging.</li> </ul>

## **DEIS Comment Letters**



# ALABAMA DEPARTMENT OF TRANSPORTATION

Design Bureau, Consultant Management Section  
1409 Coliseum Boulevard, Montgomery, Alabama 36110  
P. O. Box 303050, Montgomery, Alabama 36130-1050  
Phone: 334-242-6178 FAX: 334-353-6513



Bob Riley  
Governor

Joe McInnes  
Transportation Director

July 30, 2014

Representative Cam Ward  
Room 625-C  
11 S. Union Street  
Montgomery, AL 36130

**RE: Draft Environmental Impact Statement**  
Project No. DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening  
Mobile and Baldwin Counties

In accordance with Section 102(2)(c) of the National Environmental Policy Act of 1969 (P.L. 91-190) and Federal Highway Administration guidelines, we are enclosing (1) one hard copy of the referenced Draft Environmental Impact Statement (DEIS) along with (1) CD containing the DEIS appendices for your review and comment.

We would appreciate any comments you may have concerning the proposed alternatives, both positive and negative, on the social, economic, environmental effects, etc. Please let us know if you would like a copy of the Final Environmental Impact Statement or you need a hard copy of this DEIS.

Please mail comments to: Mr. William F. Adams, State Design Engineer, ATTN: Ms. Alfredo Acoff, Alabama Department of Transportation, 1409 Coliseum Boulevard, Montgomery, Alabama 36110.

We request that your comments be returned by November 7, 2014. If no comment or request for an extension has been received by that date, we will assume that you have no comment.

If a hard copy of the document is necessary for your review, please contact this office immediately at email address [dunnh@dot.state.al.us](mailto:dunnh@dot.state.al.us).

Sincerely,

William F. Adams, P.E.  
State Design Engineer

By:   
Alfredo Acoff, Coordinator  
Environmental Technical Section

WFA/AA/ hmd

July 30, 2014

2 | Page

Enclosure

c: DB File (w/o att.)  
ETS File (w/o att.)



**MAILOUT**  
**(For Hard Copies of DEIS**  
**Narrative with Appendix**  
**CD)**

Chairman  
Advisory Council on Historic Preservation  
401 F Street NW, Suite 308  
Washington, DC 20001-2637

Environmental Coordinator  
Alabama Department of Conservation  
and Natural Resources  
64 N. Union Street  
Montgomery, Alabama 36130

Director  
Alabama Department of Economic and  
Community Affairs  
P.O. Box 5690  
Montgomery, AL 36103-5690

Dr. Joseph B. Morton, Superintendent  
Alabama Department of Education  
P.O. Box 302101  
Montgomery, AL 36104

Director  
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Montgomery, Alabama 36130-1463

Cindy J. House-Pearson  
U.S. Army Corps of Engineers  
Mobile District  
P.O. Box 2288  
Mobile, AL 36628-0001

U.S. Fish and Wildlife Service  
Daphne ES Field Office  
1208-B Main Street  
Daphne, AL 36526

Ms. Lee Anne Wofford  
Deputy Historic Preservation Officer  
Alabama Historical Commission  
468 South Perry Street  
Montgomery, AL 36130

Sierra Club  
1330 21st Way S Ste 100  
Birmingham, AL 35205-3912

Mr. Chris Miller  
South Alabama Regional Planning  
Commission  
P.O. Box 1665  
Mobile, AL 36633

Ms. Ethel Smith  
Environmental Protection Specialist  
Office of Environmental Policy and  
Compliance (OEPC)  
Office of the Secretary [Room MS-2462-  
MIB]  
U.S. Department of the Interior [DOI]  
1849 C Street, NW  
Washington, DC 20240-0001

Ntale Kajumba  
U.S. EPA Region 4  
NEPA Program Office  
Office of Policy and Management  
61 Forsyth Street  
Atlanta, GA 30303

Commander  
Eighth Coast Guard District  
Hale Boggs Federal Building  
500 Poydras Street  
New Orleans, LA 70130

## MAILOUT (For CD'S ONLY)

Director  
Federal Emergency Management Agency  
U.S. Department of Homeland Security  
500 C Street SW, Washington, D.C. 20472

Director  
Headquarters, US Army Corps of Engineers  
441 G. Street, NW  
Washington, DC 20314-1000

Commandant CG-00  
US Coast Guard  
Headquarters  
2100 2nd St SW Stop 7000  
Washington DC 20593-7000

Director  
U.S. Department of Agriculture  
1400 Independence Ave., S.W.  
Washington, DC 20250

Director  
U.S. Department of Commerce  
1401 Constitution Ave., NW  
Washington, DC 20230

Director  
U.S. Department of Defense  
1400 Defense Pentagon  
Washington, DC 20301-1400

Director  
U.S. Department of Energy  
1000 Independence Ave., SW  
Washington, DC 20585

Director  
U.S. Department of Health and Human  
Services  
200 Independence Avenue, S.W.  
Washington, D.C. 20201

Director  
U.S. Department of Housing and Urban  
Development  
451 7th Street S.W.  
Washington, DC 20410

Director  
U.S. Department of the Interior  
Fish and Wildlife Service  
1849 C Street NW  
Washington, DC 20240

Director  
Eastern States Office  
Bureau of Land Management  
U.S. Department of Interior  
411 Briarwood Drive  
Suite 404  
Jackson, MS 39206

Director  
U.S. Department of the Interior  
U.S. Geological Survey  
1849 C Street NW  
Washington, DC 20240

Director  
U.S. Department of Transportation  
Federal Aviation Administration  
800 Independence Avenue, SW  
Washington, DC 20591

Project Manager  
FAA/Airports District Office  
100 West Cross Street  
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Jackson, MS 39208-2307

Director  
Alabama Department of Industrial  
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649 Monroe Street  
Montgomery, Alabama 36131

Director  
Alabama Development Office  
401 Adams Avenue  
6<sup>th</sup> Floor  
Montgomery, AL 36130-4106

Director  
Alabama Emergency Management Agency  
P.O. Drawer 2160  
Clanton, AL 35046-2160

Director  
Alabama Forestry Commission  
513 Madison Avenue  
Montgomery, AL 36130-2550

Director  
Alabama Geological Survey  
P.O. Box 869999  
Tuscaloosa, AL 35486-6999

Director  
Alabama State Council on Arts and  
Humanities  
201 Monroe Street  
Montgomery, AL 36130-1800

Director  
Soil and Water Conservation  
Committee  
P.O. Box 304800  
Montgomery, AL 36130-4800

Director  
Natural Resources Conservation Service  
P.O. Box 311  
Auburn, Alabama 36831-0311

Office of the Attorney General  
500 Dexter Avenue  
Montgomery, AL 36130

FOR INFORMATION ONLY:  
Director  
State of Alabama Oil and Gas Board  
P.O. Box 869999  
Tuscaloosa, AL 35486-6999  
(This address is not needed. Should instead use Alabama  
Geological Survey. Ivey Burgess @ 205-247-3677 said  
this 2-2-12. Oil and Gas Board doesn't review these  
documents. Instead the director's office for Geological  
Survey does.)

Mr. Mark Crosswhite, President  
Alabama Power Company  
600 North 18th Street  
Birmingham, AL 35291

Alabama Conservancy  
2100 1st Avenue North, Suite 500  
Birmingham, AL 35203

Dr. William E. Powell  
Executive Vice President  
Alabama Cattlemen's Association  
201 S. Bainbridge  
Montgomery, AL 36104

Alabama Department of Housing and  
Urban Development  
950 22nd St North  
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Supervisor  
US Forest Service  
USDA  
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Commissioner  
Alabama Department of Agriculture and  
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1445 Federal Drive  
Montgomery, AL 36107

Mayor Sandy Stimpson  
City of Mobile  
P.O. Box 1827  
Mobile, AL 36633-1827

Mayor Michael M. McMillian  
City of Spanish Fort  
P.O. Box 7226  
Spanish Fort, AL 36527

Mayor Dane Haygood  
City of Daphne  
P.O. Box 400  
Daphne, AL 36526

Connie Hudson, President  
Mobile County Commission  
205 Government Street  
Mobile, AL 36644-1001

Commissioner Jerry Carl  
District 3  
Mobile County Commission  
205 Government Street  
Mobile, AL 36644-1001

Commissioner Merceria Ludgood  
District 1  
Mobile County Commission  
205 Government Street  
Mobile, AL 36644-1001

Commissioner Tucker Dorsey  
Baldwin County Commission  
1100 Fairhope Avenue  
Fairhope, AL 36532

Commissioner Chris Elliott  
Baldwin County Commission  
1100 Fairhope Avenue  
Fairhope, AL 36532

Commissioner Frank Burt, Jr.  
Baldwin County Commission  
1100 Fairhope Avenue  
Fairhope, AL 36532

The Honorable Margie Wilcox  
District 104  
AL House of Representatives  
11 S. Union Street  
Montgomery, AL 36130

The Honorable Adline Clarke  
District 97  
AL House of Representatives  
856 Canal Street  
Mobile, AL 36602

The Honorable Joe Faust  
District 94  
AL House of Representatives  
20452 Beecher Street  
Fairhope, AL 36532

The Honorable Randy Davis  
District 96  
AL House of Representatives  
6590 Thompson Lane  
Daphne, AL 36526

The Honorable Trip Pittman  
District 32  
AL Senate  
Room 730  
11 S. Union Street  
Montgomery, AL 36130

The Honorable Bill Hightower  
District 35  
AL Senate  
Room 733  
11 South Union Street  
Montgomery, AL 36130

Dr. Alan T. Lee  
Superintendent of Education  
Baldwin County Schools  
2600-A North Hand Avenue  
Bay Minette, AL 36507

Mrs. Martha L. Peek  
Superintendent  
Mobile County Schools  
P.O. Box 180069  
Mobile, AL 36618

Mr. Joe W. Ruffer  
Mobile County Engineer  
Mobile Government Plaza  
205 Government Street  
Mobile, AL 36644-1600

Mr. Cal Markert  
Baldwin County Engineer  
P.O. Box 220  
Silverhill, AL 36576

Honorable Tim Russell  
Baldwin County Probate Judge  
P.O. Box 459  
Bay Minette, AL 36507

Honorable Don Davis  
Mobile County Probate Judge  
P.O. Box 7  
Mobile, AL 36601

Mr. Sam Cochran  
Mobile County Sheriff  
P.O. Box 113  
Mobile, AL 36601

Mr. Huey Hoss Mack  
Baldwin County Sheriff  
310 Hand Avenue  
Bay Minette, AL 36507

Mobile City Hall  
205 Government Street  
Mobile, AL 36602-0001

Daphne City Hall  
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Spanish Fort City Hall  
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Spanish Fort, AL 36527

Ms. Janette Curry  
Manager  
Mobile County Public Library  
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1924-B Dauphin Island Parkway  
Mobile, AL 36605-3004

Ms. Margie Calhoun  
Manager  
Mobile County Public Library  
Ben May Main Branch  
701 Government Street  
Mobile, AL 36602

Ms. Toja Young  
Director  
Daphne Public Library  
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Daphne, AL 36526

Mr. Norman Pitman  
President  
Mobile Historic Development Commission  
City of Mobile  
P.O. Box 1827  
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Deputy General Counsel  
National Trust for Historic Places  
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Mr. John Hildreth  
Vice President  
Eastern Field Office  
National Trust for Historic Places  
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3rd Floor  
Charleston, SC 29403

Ms. Carol Legard  
FHWA Liaison  
Advisory Council on Historic Places  
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Spanish Fort City Council  
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Mr. Walter Meigs  
General Counsel  
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Alabama, LLC  
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Mobile, AL 36652

Mr. Ray Harris  
Facilities Manager  
Signal Shipyard/Bender Shipbuilding &  
Repair Company  
601 Royal Street  
Mobile, AL 36602

Ms. Mary L. Cousar  
6 St. Joseph Street  
Mobile, AL 36602

Ms. Elizabeth S. Sanders  
Downtown Mobile Alliance  
P.O. Box 112  
Mobile, AL 36601

Ms. Caroline Etherton  
Colonial Dames and Conde-Charlotte  
Museum House  
Post Office Box 1968  
Mobile, AL 36633

Mr. Douglas Burtu Kearley  
Ten Wisteria Avenue  
Mobile, AL 36607

Mr. Herdon Inge  
P.O. Box 40188  
Mobile, AL 36640

Ms. Ann Bedsole  
6 St. Joseph Street  
Mobile, AL 36602

Reverend Canon Beverly F. Gibson  
Christ Church Cathedral  
115 South Conception Street  
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Ms. Lauren Vanderbijl  
Historic Mobile Preservation Society  
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Mobile, AL 36604

Ms. Katherine Frangos  
Friends of the Museum  
Post Office Box 2068  
Mobile, AL 36602

USS Alabama Commission  
Battleship Memorial Park  
Post Office Box 65  
Mobile, AL 36601-0065

Mr. Jarrod J. White  
Restore Mobile, Inc.  
c/o Cabaniss Johnston, LLP  
Mobile Office, Riverview Plaza  
63 South Royal Street  
Suite 700  
Mobile, AL 36652

The Honorable Richard Shelby  
Alabama Senator  
United States Senate  
304 Russell Senate Office Building  
Washington, DC 20510

The Honorable Jeff Sessions  
Alabama Senator  
United States Senate  
326 Russell Senate Office Building  
Washington, DC 20510

The Honorable Bradley Byrne  
Alabama Congressman District 1  
US House of Representatives  
2236 Rayburn HOB  
Washington, DC 20515

Luann Cutshaw  
424 N. Calhoun Street  
Tallahassee, FL 32301

**\*\*\*THIS ONE TO BE HANDLED BY  
FHWA (DON'T SEND COPY TO)  
(NEED CD FROM VOLKERT FOR  
THOUGH)**

Mr. Jefferson Keel, Lt. Governor  
Chickasaw Nation  
P.O. Box 1548  
Ada, OK 74821-1548



# THAMES BATRÉ

INSURANCE • SINCE 1891

July 30, 2014

Mr. John R. Cooper, Director  
Alabama Department of Transportation  
Post Office Box 303050  
Montgomery, Alabama 36130-3050



Re: Interstate 10 (I-10) Mobile River Bridge/Bay Way Widening Project

Dear Mr. Cooper:

On behalf of Thames Batré, we are writing to express our support for the proposed I-10 Mobile River Bridge project. As South Alabama wrestles with ever increasing traffic congestion along the Interstate 10 corridor, this major bottleneck continues to threaten public safety, as well as the vitality of our local and regional economy.

Thames Batré has been serving the insurance needs of South Alabama for over 120 years. With clients, employees and offices located on both sides of this bridge, we know first hand the serious economic, environmental and transportation problems caused by this I-10 bottleneck.

There are few decisions more critical to the region's future than those related to planning and funding for the I-10 Mobile River Bridge. Therefore, it our desire to join you and other regional partners in support this important transportation project.

Sincerely yours,

A handwritten signature in black ink, appearing to read 'Bo Mattei', is written over the 'Sincerely yours,' text.

Bo Mattei, CIC, CLU  
President

HWM/spb

cc: Mobile Area Chamber of Commerce  
Attention: Ginny Russell  
Post Office Box 2187  
Mobile, Alabama 36652-2187





## Continental Motors

Powering Aircraft Since 1929

September 16, 2014

Mr. John R. Cooper, Director  
Alabama Department of Transportation  
P.O. Box 303050  
Montgomery, Alabama 36130-3050



RE: Interstate 10 (I-10) Mobile River Bridge / Bay Way Widening Project

Dear Mr. Cooper:

On behalf of Continental Motors, Inc., we are writing to express our support for the proposed I-10 Mobile River Bridge project. As South Alabama wrestles with ever increasing traffic congestion along the Interstate 10 corridor, this major bottleneck continues to threaten public safety, as well as the vitality of our local and regional economy.

While many years have been spent assessing the need for the I-10 transportation enhancement, it is vitally important that we continue working in pursuit of this needed infrastructure improvement. Interstate 10 serves the southern tier of the United States by providing the main east-west link in the Interstate Highway System. Viewed by many as a regional asset, we must solve the serious economic, environmental and transportation problems caused by the overwhelmed I-10 Bridge.

Continental Motors, Inc. employs over 450 people at two sites in Mobile and Fairhope (~425 at Brookley Aeroplex and ~25 at Fairhope's Sonny Callahan Airport). Our employees come from Florida, Alabama and Mississippi and travel on I-10 for their commutes.

Solving the gridlock caused by the existing I-10 Mobile Tunnel will reduce commutes for our employees, reduce congestion that slows shipments from suppliers and to customers, and support reduced travel costs between company locations in Mobile and Baldwin counties.

Further, we believe that starting this long overdue improvement will also help Mobile develop a vibrant waterfront that will draw business and tourists to the community, while increasing the quality of life for all residents.

There are few decisions more critical to the region's future than those related to planning and funding for the I-10 Mobile River Bridge. Therefore, it is our desire to join you and other regional partners in support of this important transportation project.

Sincerely,

Rhett Ross  
President

---

**Rhett Ross - President**

2039 South Broad Street Mobile AL 36615 Phone: 251.436.8360 Fax : 251.438.1811





## Alabama Chapter

September 17, 2014

Mr. John R. Cooper, Director  
Alabama Department of Transportation  
P.O. Box 303050  
Montgomery, Alabama 36130 - 3050

RE: Interstate 10 (I-10) Mobile River Bridge/Bay Way Widening Project

Dear Mr. Cooper:

On behalf of Associated Builders & Contractors of Alabama, I wanted to share our thoughts regarding the proposed I-10 Mobile River Bridge project. This is not a Mobile issue. In fact, this is not a Baldwin County issue or a Mobile County issue. This is an entire State of Alabama issue. True enough, the traffic congestion and public safety needs will be sufficed once this project is complete. However, from a construction and economic development view, we must not sit by and allow opportunities like this to pass us by that create jobs and increase revenue for our state while improving infrastructure and our state's ability to attract industry.

I have been involved and have seen close attention being paid to those parties that feel the project is not needed. Fortunately, we now have a solid plan that protects and preserves while advances. We actively solicit ALDOT, The Mobile Chamber and the Build a Bridge Coalition to call on us for assistance as needed.

We stand ready to help but wanted to make sure you knew as an association, we stand for "Build the Bridge".

Sincerely,

Jay Reed  
President

c.c. Ginny Russell, Mobile Chamber of Commerce  
Jeff Rode, SJ & L, Inc.  
Mike Rogers, Rogers & Willard  
John Baker, Thompson Engineering







Preserving America's Heritage

October 9, 2014

Mr. Mark Bartlett, P.E.  
Division Administrator  
Federal Highway Administration  
Alabama Division  
9500 Wynlakes Place  
Montgomery, AL 36117

Ref: *Determination of Effects for ALDOT Project DPI-0030(005)  
I-10 Mobile River Bridge and Bayway Widening EIS  
Mobile and Baldwin Counties, Alabama*

Dear Mr. Bartlett:

Thank you for hosting the September 23, 2014 consulting parties meeting to discuss the proposed I-10 Mobile River Bridge and Bayway Widening Project. This meeting was held in accordance with our regulations, "*Protection of Historic Properties*" (36 C.F.R. Part 800). We found this meeting productive and relevant to developing a path forward.

We have reviewed the Draft Environmental Impact Statement (DEIS) and conducted a site visit to the project area on September 22<sup>nd</sup> along with the members of the Alabama State Historic Preservation Office (SHPO), Mobile Historic Development Commission, and the National Trust for Historic Preservation (NTHP). Based on our tour and a review of the DEIS, we do not agree with the determination of effects proposed by the Alabama Department of Transportation (ALDOT). We also noted there was overwhelming agreement among consulting parties in attendance at the September 23<sup>rd</sup> meeting that this undertaking will result in an "adverse effect" due to visual, auditory, and vibratory effects on historic properties. 36 C.F.R. § 800.5(a)(1) of our regulations states that "An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association." It is our opinion that several historic properties will be directly and indirectly adversely affected.

We regret that ALDOT did not involve the Advisory Council on Historic Preservation (ACHP) in project planning sooner. We want to remind you that ACHP intends to participate actively in the Section 106 consultation along with other recognized consulting parties from this point forward. At this time, we recommend that FHWA and ALDOT revise the effect determination for the undertaking and proceed to the next step in the Section 106 process. As set forth in 36 C.F.R. § 800.6, FHWA and ALDOT should consult with the Alabama SHPO and other consulting parties to develop and evaluate alternatives or modifications to the undertaking that could avoid, minimize, or mitigate adverse effects on historic

ADVISORY COUNCIL ON HISTORIC PRESERVATION

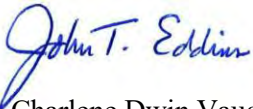
401 F Street NW, Suite 308 • Washington, DC 20001-2637  
Phone: 202-517-0200 • Fax: 202-517-6381 • [achp@achp.gov](mailto:achp@achp.gov) • [www.achp.gov](http://www.achp.gov)

P-45

properties and develop a Memorandum of Agreement (MOA) or Programmatic Agreement (PA) as appropriate. Noting that there are multiple National Historic Landmarks (NHLs) identified within the Area of Potential Effects (APE) for the undertaking, we also recommend that you contact the National Park Service (NPS) and invite them to enter the consultation pursuant to 36 C.F.R. § 800.10(c) of our regulations.

We request that you share our letter with the other consulting parties. We also encourage you to solicit from these parties any historic preservation issues that need to be addressed as ALDOT and FHWA proceed with the Section 106 review process. If you have any questions, please contact Mr. Chris Wilson at 202-517-0229 or via e-mail at [cwilson@achp.gov](mailto:cwilson@achp.gov).

Sincerely,



*for* Charlene Dwin Vaughn, AICP  
Assistant Director  
Federal Permitting, Licensing and Assistance Section  
Office of Federal Agency Programs



## Mobile Historic Development Commission

P.O. Box 1827  
Mobile, AL 36633-1827



October 27, 2014

Ms. Heather Dunn  
Alabama Department of Transportation  
Environmental Technical Section  
1409 Coliseum Boulevard  
Montgomery, AL 36110

Mr. Vincent E. Calametti, P.E.  
Region Engineer Southwest Region  
Office of Region Engineer  
1701 West I-65 Service Road  
Mobile, AL 36618

Re: Comments on Draft Environmental Impact Statement (“Draft EIS”) for the I-10 River Bridge

Dear Ms. Dunn and Mr. Calametti:

On behalf of the Mobile Historic Development Commission (MHDC), I would like to thank ALDOT for the work on the Draft EIS for the I-10 River Bridge. We are particularly appreciative of the Phase 3 archaeological investigations currently underway and the meetings recently held in Mobile and Baldwin Counties.

The comment period on the Draft EIS ends November 7, 2014, but ALDOT has not yet made available the vibrations study or the ongoing Phase 3 archeological investigations which are referenced in the Draft EIS. Therefore, at this time the MHDC cannot agree with ALDOT’s finding of no adverse effect, and we request additional time to comment on the Draft EIS following release of the vibrations study and the Phase 3 archeological investigations. However, the MHDC recognizes the urgent need to move this project forward, and requests that ALDOT enter into a written programmatic agreement to address mitigation of negative impacts on historic resources, including both currently identified impacts and those not yet identified.

The MHDC recognizes the need for a bridge and agrees with the choice of the “B Prime” route as the preferred route. The MHDC specifically objects to Route A and Route B as shown in the Draft EIS due to their adverse effects on historic resources. The MHDC



recognizes the need to narrow the focus of the project in order to move forward with finalization of the route selection and design and engineering of the bridge. Thus, the MHDC does not object to the elimination of alternative routes and bridge and tunnel designs from the Draft EIS and future studies.

With the bridge still to be designed, the MHDC believes that most of its concerns could be handled through a programmatic agreement that will allow the bridge to proceed. These are:

1. ALDOT will continue to consult with the MHDC throughout design and construction of the bridge. The historic significance of the bridge location and the impact on the viewshed from historic resources should be considered in the design process.
2. Vibrations from construction may harm historic resources. Such harm would be a direct adverse effect on historic resources. Constant monitoring during construction and beyond must be done to ensure there will be no damage to historic resources. Monitoring must be performed within Oakdale and East Church Street Historic Districts and at nearby buildings of historic importance. Examples of buildings requiring monitoring are: St. Matthews Church; Council Elementary School; Prince of Peace Church; the Conde Charlotte House and the Union Hall. Other sites and criteria for site monitoring will be finalized during creation of the programmatic agreement. Specific measures, testing procedures and methods of documenting and monitoring the condition of critical structures must be outlined in the programmatic agreement.
3. The programmatic agreement must include a plan of action in the event that vibrations are detected at potentially harmful levels. The plan must include steps to prevent or halt any damage to historic structures. ALDOT must repair any damage done to historic resources.
4. The Commission is concerned about environmental issues that would diminish the integrity and value of various historic resources, both during and after construction: noise; particulates; stormwater; viewshed and lighting. The mitigating measures proposed in the Draft EIS must be documented in the programmatic agreement and implemented before, during and after construction.
5. Actual, "as built" measurements must be documented to insure that measurable impacts (noise, lighting, particulates, etc.) are within the levels stated in the Draft EIS. If the impacts exceed the estimates in the Draft EIS, additional mitigation measures will be implemented.
6. Lighting components of the project will have a direct impact on nearby historic resources. Lighting specifications should not be finalized until near the end of the design and engineering process in order to take advantage of the latest technological developments.
7. The programmatic agreement must contain a specific plan for minimizing adverse effects to historic resources during construction, including specific plans for minimizing noise, particulate, stormwater and lighting impacts.



8. The viewshed from historic resources will be compromised, particularly where open spaces or vistas occur and where there are views from multi-story buildings. This will diminish the integrity of the various historic resources' setting and feeling, which will lead to a direct adverse economic impact on the property values of historic resources. The programmatic agreement must contain a plan for diminishing or offsetting this impact on the viewshed. A detailed landscaping plan could alleviate some of this effect. Any landscaping or tree plantings implemented by ALDOT or identified in the Draft EIS as blocking the view of the bridge from historic resources must be maintained or replaced during the four years following completion of the bridge and plantings, whichever is later, in order to ensure their ongoing viability.
9. The MHDC takes no position on pedestrian or bike access on the bridge. If a walkway/bikeway is included, the increased auto, bike and pedestrian traffic will impact historic resources. Proper planning is needed to assure the minimization of any negative impacts. The MHDC must be included in the planning, locating and design of any parking area or integrated pathway plan.
10. Additional signage will offset any loss of tourism and other negative economic impact on historic resources. The MHDC requests that a signage plan be developed to alert motorists of the historic districts and attractions, and the signage plan be included in the programmatic agreement.
11. The bridge will traverse areas of historic significance, including identified historic resources. The programmatic agreement must include a plan and resources to redevelop the area under and surrounding the bridge to mitigate negative economic impact to historic resources.

The MHDC reserves the right to comment and request additional mitigating measures as further information becomes available. In particular, additional mitigating measures may be required following release of the vibration study and the Phase 3 archaeological studies.

The design of the bridge will have a significant impact on Mobile as a whole, especially the nearby historic districts. As the most visible structure in the City, it will become a symbol of Mobile. The MHDC looks forward to working with ALDOT during the design phase. The Commission notes that:

- A. The overall design of the bridge must be architecturally appealing; and
- B. Since the location of the bridge will be in one of the most historic parts of the City, the treatment of the large area below the bridge is a great concern.

The Commission believes a programmatic agreement would be of benefit to all concerned parties. By allowing the Commission and other Consulting Parties to be part of the design process, many of the concerns expressed in this letter could be minimized.

The Mobile Historic Development Commission welcomes the opportunity to work with ALDOT on this project. In so doing, we can all take pride in this accomplishment.

Sincerely,

A handwritten signature in cursive script that reads "Jean Dodge". The signature is written in black ink and is positioned above the printed name and title.

Jean Dodge  
President

cc:

William S. Stimpson

Dianne Irby

Elizabeth Merritt

Chris Wilson



STATE OF ALABAMA  
ALABAMA HISTORICAL COMMISSION  
468 SOUTH PERRY STREET  
MONTGOMERY, ALABAMA 36130-0900

FRANK W. WHITE  
EXECUTIVE DIRECTOR

November 4, 2014

TEL: 334-242-3184  
FAX: 334-240-3477

Mr. Mark Bartlett  
Federal Highway Administration  
Alabama Division  
9500 Wynlakes Place  
Montgomery, AL 36117

RE: AHC 00-1934  
I-10 Mobile River Bridge and Bayway Widening DEIS  
Mobile and Baldwin Counties, Alabama

Dear Mr. Bartlett:

We appreciate the opportunity to review the Draft Environmental Impact Statement for the above-referenced project. After attending the September 23, 2014, Consulting Parties meeting, and after touring the area with representatives of the Advisory Council on Historic Preservation, the Mobile Historic Development Commission, and the National Trust for Historic Preservation, the Alabama Historical Commission continues to express our concerns over potential visual, auditory, and vibratory adverse effects on historic properties.

While the DEIS addresses certain environmental commitments, we would prefer that FHWA and ALDOT identify and address all potential adverse effects to historic properties in either a Programmatic Agreement and/or a Memorandum of Agreement.

We also understand that Phase II Archaeology will be coordinated with this office and performed as part of the investigation of the Preferred Alternative in the FEIS. We agree that if sites cannot be avoided, FHWA will consult with our office.

Thank you for your continued work on this complicated project. We are committed to working with FHWA and ALDOT to move this project forward. If you have any questions, please feel free to contact Amanda McBride at 334.230.2692 or [Amanda.McBride@preserveala.org](mailto:Amanda.McBride@preserveala.org) or Lee Anne Wofford at 334.230.2659 or [LeeAnne.Wofford@preserveala.org](mailto:LeeAnne.Wofford@preserveala.org).

Sincerely,

Lee Anne Wofford  
Deputy State Historic Preservation Officer

LAW/ALM/RJG/law







16591A  
November 4, 2014

Alabama Department of Transportation  
Attn: Mr. William F. Adams, P. E.  
Post Office Box 303050  
Montgomery, AL 36130-1050



Dear Mr. Williams:

This letter is in reference to your request for comments on the Draft Environmental Impact Statement (DEIS) for the widening of the I-10 Bridge and Bayway over the Tenesaw River, mile 0.3, the Apalachee River, mile 0.0 and the Blakley River, 0.4. The Coast Guard has completed our review of the DEIS and have the following comments:

1. We have completed our review of the EIS prepared by the Alabama Department of Transportation and have the following comments. Please send a letter to the applicant, with copy to CG-BRG-2, including comments from both D8 (dpb) and CG-BRG-2.
2. In order for the Coast Guard to adopt the bridge-related portions of the FEIS as part of the Coast Guard bridge permitting process, please address the following comments.
3. Please include in the FEIS a description of stormwater management plans for the bridge.
4. Section 4.12.3.3 mentions the fact that several of the submerged aquatic vegetation (SAV) species are invasive. Additionally, *Phragmites australis* is also likely an invasive plant. Executive Order 13112 directs federal agencies to prevent the introduction and spread of invasive species. Please describe in the FEIS the steps that will be used in this project to ensure that these species do not spread.
5. In Section 4.12.4, the impacts to wetlands from the bridge/Bayway piers are termed "much less" than the overall width of new roadway that they will be supporting. Please state the total area of SAV and wetlands that will be lost due to the bridge/Bayway piers.
6. In Section 4.12.5, shading impacts are discussed solely in terms of area. What are the expected biological and ecological function impacts? Will existing SAV die from lack of sun? Will species be less able to make use of such areas, especially if the SAV is important habitat for them?
7. In section 4.12.5, it is stated that EFH impacts from the bridge are considered minimal, but the total area of habitat permanently lost to bridge/Bayway piers is not stated. Please include this information in the FEIS.
8. Section 4.12.5 needs a fuller discussion of EFH impacts. Impacts on aquatic animals from construction noise and suspended sediments are not discussed, and this is not currently sufficient for the USCG to adopt the document.

November 4, 2014

9. In Section 4.12.6, it is stated that in the FWS letter of 14 May 2003 providing the Service's Biological Opinion, there were four listed species identified by the FWS as possibly occurring in the project area. However, their letter only discusses the effects on the Alabama red-bellied turtle and the Gulf sturgeon, and does not mention any other species. Section 4.12.6 also states that the manatee was brought in as a species to be considered on 11 January 2007, but meeting notes dated a year earlier, on 18 January 2006 (Appendix A), noted the manatees at that time. Please include the correct citation for the four species and update dates as necessary.
10. Ensure that migratory bird impacts are thoroughly discussed in the FEIS in section 4.12.6, with all appropriate USFWS input included and coordination completed.
11. The section on GHG and Climate Change seems to be copied from some other document since it refers to the DOT as 'we'. Recommend changing to the third person since DOT isn't the author of this DEIS, and it is confusing to the reader.
12. Section 4.16.1 refers to the FHWA as "their" and gives the impression that FHWA is not the responsible agency. Recommend changing the language to "we" and "our" as necessary or deleting the pronouns and only referring to "the FHWA".
13. Recommend inserting the Wetland Determination and SAV Survey as a separate appendix for easy reference.
14. Recommend inserting the EFH Assessment as a separate appendix for easy reference, or combine with the Wetland Determination into one appendix.
15. Please include the Biological Assessment provided to the USFWS in 2002 in an appendix.
16. Please update the status of NMFS consultation in the FEIS and include all correspondence.
17. Appendix K could be renamed to highlight the fact that it also includes the shadow study.
18. If mitigation will take place as suggested by FWS in 2001/2002, please describe it in the FEIS.



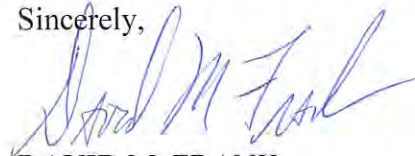
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November 4, 2014

19. USACE's jurisdictional determination expired 5 years after issuance, which occurred on 28 February 2002 (Appendix A). Has a new JD been procured from USACE? Please provide the MOA or PA with the Alabama SHPO when finalized.

20. Ensure that the proper waterways are annotated as there are two waterways that this bridge crosses; the Tenesaw River, mile 0.3, the Apalachee River, mile 0.0 and the Blakley River, 0.4.

If you have any questions or need further assistance, please don't hesitate to contact our office.

Sincerely,



DAVID M. FRANK  
Chief, Bridge Administration Branch  
U.S. Coast Guard  
By direction

Copy: CG-BRG-2, Shelly Sugarman  
FHWA, Mark Bartlett, P.E.

BAE Systems Southeast Shipyards Alabama LLC  
 Main Gate, Dunlap Drive  
 P.O. Box 3202  
 Mobile, AL 36652



November 5, 2014

Mr. Vincent E. Calametti, P.E., Region Engineer  
 Alabama Department of Transportation  
 1701 I-65 West Service Road N  
 Mobile, AL 36618-1109



RE: Section 106 Consulting Parties Consultation Meeting  
 ALDOT Project DPI-0030(005)  
 I-10 Mobile River Bridge and Bayway Widening EIS  
 Mobile and Baldwin Counties, Alabama

Gentlemen:

Please accept this letter as the comments of BAE Systems Southeast Shipyards Alabama LLC and its affiliated companies. BAE Systems operates the shipyards which more previously known as Atlantic Marine, Inc, Atlantic Marine Alabama, LLC, and Alabama Shipyard. BAE Systems and its predecessor companies have operated these yards on Pinto Island since the 1800's employing a large number of employees. Of the three routes under consideration Route C as previously documented by BAE Systems directly has an adverse impact on the yard's business and would also impact certain properties which may have historical and cultural values. Route B has an indirect effect and Routes A and B prime, a lesser effect. Additionally, because of the ever increasing air draft of vessels, we support the maximum practical air draft clearance and northernmost location practicable.

Please accept these comments as those of an Consulting Party under Section 106 and generally. Previously in the record we have submitted documentation to support our position, which we trust we do not have to replicate in detail.

Very truly yours,

Victor Rhoades  
 Director

SECTION	INFO	ACTION	FILE
REGIONAL ENGINEER			
ASST REGION ENGINEER			
ADMINISTRATION			
OPERATIONS-MOBILE			
OPERATIONS-GROVE HILL			
CONSTRUCTION			
COUNTY TRANSPORTATION			
EQUIPMENT			
EEO			
MATERIALS			
PRE-CONSTRUCTION			
SPECIAL PROJECTS			
DISTRICT MANAGERS			P-55



**Michael Saxon**  
Vice President  
Mobile Division

150 St. Joseph Street (36602)  
Post Office Box 2247 (36652-2247)  
Mobile, Alabama

Tel 251.694.2301  
Fax 251.694.2309  
rmsaxon@southernco.com



November 5, 2014

Mr. Williams F. Adams, P.E.  
State Design Engineer  
ATTN: Ms. Alfredo Acoff  
Alabama Department of Transportation  
1409 Coliseum Boulevard  
Montgomery, AL 36110



Re: Comments on Draft Environmental Impact Statement, Project No. DPI-0030(005), I-10 Mobile River Bridge and Bayway Widening, Mobile and Baldwin Counties

Dear Mr. Adams and Ms. Acoff:

Thank you for the opportunity to comment upon the above-referenced Draft Environmental Impact Statement ("DEIS"). The Alabama Department of Transportation's ("ALDOT") efforts to relieve existing (and worsening) traffic congestion on Interstate I-10 in the Wallace Tunnel and on the Bayway (the "Project") are of significant importance to the Alabama Power Company ("APC"). Approximately one hundred APC employees routinely cross Mobile Bay each day en route to work. In less routine situations, APC depends upon I-10 to move crews from Mobile to Baldwin County (and points east) to respond to transmission outages and other emergencies. In short, having a dependable traffic route across Mobile Bay is a necessity for APC.

At the same time, APC recognizes the profound importance of the maritime industry on Mobile's economy, both directly and indirectly. Not only does the maritime industry itself provide employment to thousands of the region's residents, but its transport of goods and materials to and from Mobile has an even broader multiplicative impact on the region's economy. For example, the coal which powers APC's Plant Barry arrives via ship to Mobile Bay and is transported by barge upriver to the facility. Thus, any transportation project across the Mobile River must take into account the impact it would have on Mobile's maritime community.

Consequently, APC has followed the development of the DEIS very carefully. It has attended each of the public hearings held in support of the DEIS, considered all comments

offered, and has reviewed the DEIS in its entirety. Such analysis has led APC to conclude that the preferred alternative, B', is indeed the best alternative for meeting the Project's purpose and need. In APC's opinion, the B' alternative meets the Project's stated purpose and need at a competitive cost while causing impacts to the environment that are comparable to, if not less than, the other alternatives under consideration.

At the same time, APC recognizes that all parties interested in the Project are best served by a robust, "hard look" environmental analysis of all feasible alternatives and, in the end, a careful and reasoned articulation of the alternative selected. Furthermore, all parties are best served if the procedural requirements of such federal statutes as the National Environmental Policy Act and the National Historic Preservation Act are fulfilled. Accordingly, APC respectfully offers the following observations with respect to the DEIS for ALDOT's consideration.

1. A number of commentators expressed a desire that the Project include a pedestrian and bicycle path associated with the relevant bridge. APC does not have an opinion as to whether such a path should be incorporated into the Project, other than to note that such a path is not part of the Project's stated purpose. Nevertheless, it would be prudent for ALDOT to carefully consider this issue. The DEIS already recognizes such interest in Section 4.4.6; accordingly, ALDOT's consideration and ultimate rationale in this regard should be addressed in the Final EIS. To that end, whereas the financial costs of including such a path can be objectively estimated, the purported benefits (economic and otherwise) of such a path should also be objectively identified and considered. On a related note, such a path, with the scenic views that it will present, could provide an opportunity (through informational panels and kiosks and the like) for mitigation opportunities to offset the Project's potential impact to historic resources. Nevertheless, such amenities should not be added to the project if the cost of such amenities renders the Project financially infeasible.

2. It has been APC's experience that the proper implementation of Best Management Practices, coupled with meeting the conditions of the Alabama Department of Environmental Management's Clean Water Act Section 401 Water Quality Certification, will successfully address water quality concerns (to include stormwater runoff) associated with the Project.

3. APC believes that ALDOT's conclusions in the DEIS regarding the project's potential impact (or lack thereof) on threatened, endangered, or federally protected species such as the Alabama sturgeon, the gulf sturgeon,<sup>1</sup> the bald eagle, the Florida manatee, and the red-bellied turtle (or their collective habitats) are correct and that proposed management

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<sup>1</sup> Technically speaking, the DEIS' discussion of the critical habitat designation for the Gulf sturgeon on page 116 is incorrect. The DEIS states that critical habitat for the Gulf sturgeon was *proposed* on March 19, 2003, by the U.S. Fish & Wildlife Service; in actuality, critical habitat was, in fact, designated at that time and, in that same action, the U.S. Fish & Wildlife Service affirmatively rejected suggestions that Mobile Bay be included in the Gulf sturgeon's critical habitat.

and/or mitigative measures are appropriate. This is particularly the case with the Alabama sturgeon, a species which has not been identified in the local waters in decades.

4. The DEIS's conclusions regarding noise impacts and air quality are sound and based on rational modeling and assumptions.

5. APC is of the opinion that likely wetlands and coastal zone impacts have been identified and are capable of being adequately mitigated.

6. APC agrees that the "other alternatives considered" as outlined in Section 3.2.4 (i.e., mass transit, light rail and ferries, commuter transit, transportation management measures, toll facilities, and moveable bridge alternatives) are not feasible alternatives for the Project. Factors that support this conclusion include the known driving habits and preferences of the region's commuters, the dispersed nature of residential communities and work sites on both sides of the bridge, and the construction and operational costs of light rail, ferries, and moveable bridge alternatives. Furthermore, mass transit, commuter transit, and transportation management measures would not reduce the growing amount of transitory I-10 east-west commercial traffic. Finally, none of the alternative measures would address a critical component of the Project's purpose (i.e., to offer a more direct and expeditious route for transporting hazardous materials through Mobile). It should be noted, however, that, while a toll system in the Wallace Tunnel would not necessarily divert adequate volumes of traffic elsewhere and thus relieve congestion on I-10, a toll system on the proposed bridge could help finance the proposed bridge's construction.

7. Section 4.4 of the DEIS addresses the socio-economic environment. A more robust analysis of this environment would explore whether improved commute capabilities between Mobile and Baldwin County will accelerate "suburban flight" from Mobile to the communities on the Eastern Shore, and the impact that such migration will have on communities, schools, tax bases, and the like in Mobile and Baldwin counties.

8. To the extent that the GulfQuest Maritime Museum is cited as a change in the environmental setting that impacts the alternatives analysis, the Final EIS should candidly and expressly reflect the current status of that facility. For example, the DEIS is likely inaccurate on page 46 when it states that the GulfQuest Maritime Museum "is scheduled to open in late 2014." Such inaccuracies may not, from a practical sense, be relevant to ALDOT's decision making with respect to the Project, but they will likely be cited by opponents of the Project as symptomatic of flawed analysis. By scrubbing and updating the DEIS to reflect the current status of Mobile's waterfront, ALDOT will deny such arguments to the Project's opponents.

9. During the public hearings on the DEIS, several commentators complained of the Project's effects on historical resources. APC takes no position on the accuracy or veracity of those commentators' arguments or statements. However, APC encourages ALDOT to provide opportunities for comment and consultation to interested parties in order to ensure that the requirements and procedures required by Section 106 of the federal National Historic



Preservation Act and Section 4(f) of the federal Department of Transportation Act are met. ALDOT's development of the project would be best served by ensuring that opponents of the project are not able to claim that a historic resource (or a direct or indirect impact on such a resource) was overlooked, an interested party was not consulted, or an alternative was not considered.

10. With respect to the foregoing comment, APC encourages ALDOT and other interested parties to recognize that the historic integrity of the Mobile viewshed has been compromised by modern projects; thus, to argue that the existing viewshed possesses integrity capable of being adversely impacted is not particularly compelling.

11. ALDOT is correct to address environmental justice concerns in the DEIS. Perhaps a follow-on meeting in the Central Texas Street Neighborhood community is warranted as part of the ongoing EIS process (given that the last meeting reportedly occurred on August 31, 2010). Understandably, the DEIS, in Section 4.9.5, focuses on the Project's potentially negative impacts to the local community. At the same time, however, relieving traffic congestion between Mobile and Baldwin Counties could have positive impacts on this community as well (e.g., reduction in vehicle-sourced ozone and improved mobility/access to job opportunities in Baldwin County). If ALDOT agrees, then such positive impacts should be factored into the environmental justice calculus as well.

12. Indirect and cumulative impacts analysis is an imperfect art. However, in the DEIS, ALDOT's efforts in this regard are in compliance with the letter and spirit of the relevant federal Council on Environmental Quality regulations. The DEIS identifies and addresses potential indirect and cumulative impacts to the maximum extent possible and practicable and its conclusions in that regard are both logical and reasonable.

Again, APC thanks ALDOT for this opportunity to comment on this most important project. Should ALDOT have any questions, please do not hesitate to contact us.

Sincerely,

A handwritten signature in cursive script that reads "R. Michael Saxon".

R. Michael Saxon



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY  
REGION 4  
ATLANTA FEDERAL CENTER  
61 FORSYTH STREET  
ATLANTA, GEORGIA 30303-8960

November 6, 2014

Ms. Alfredo Acoff  
Alabama Department of Transportation  
1409 Coliseum Boulevard Centers  
Montgomery, Alabama 36110

**Subject: EPA Comments on the Draft Environmental Impact Statement for  
I-10 Mobile River Bridge and Bayway Widening  
Mobile and Baldwin Counties, Alabama.  
Project No. DPI-0030(005); CEQ #: 20140213; ERP#: FHW-E40849-AL.**

Dear Ms. Acoff:

Pursuant to Section 309 of the Clean Air Act, and Section 102(2)(c) of the National Environmental Policy Act (NEPA), the U.S. Environmental Protection Agency (EPA) reviewed the subject document. EPA participated in site visits and meetings associated with the proposed project including a public meeting on September 23, 2014, in Mobile, Alabama. The purpose of this letter is to provide you with EPA's comments and recommendations on the draft environmental impacts statement (DEIS) that should be considered in the development of the final environmental impact statement.

According to the DEIS, the Federal Highway Administration (FHWA) and the Alabama Department of Transportation (ALDOT) propose to construct a new six-lane, 215-foot bridge across Mobile River and widen the existing I-10 bridges from four to eight lanes. The purpose of the project is to construct a structure that can accommodate current and future traffic demands as well as provide a direct route for vehicles transporting prohibited hazardous materials across the Mobile River, while minimizing impacts to Mobile Harbor. The existing traffic volumes on I-10 crossing the Mobile River currently operate at a level of Service of F with delays during peak hours (DEIS, 2014).

The DEIS evaluates several alternatives including a no-action alternative and four build alternatives (A, B, B', and C). It also identifies a preferred alternative (B'). The preferred 10-mile alignment begins at Virginia Street, crosses the Mobile River just south of the Mobile Cruise Terminal, continues across Pinto Island and joins the center of the Bayway, which will be widened by two-lanes in each direction from the bridge meeting point to Daphne. It is EPA's understanding that additional right-of-way will be required for the proposed Mobile River bridge, but that the proposed widening should occur within the existing footprint of the Bayway.

Based on a review of the project impacts, EPA provided detailed comments regarding air quality, cultural resource, water resources, noise and community impacts. The selection of B' relative to the other proposed build alternatives assessed in the DEIS minimizes impacts to communities, cultural resources and the environment. Nevertheless, we recommend that the FEIS describe efforts to further avoid, minimize, mitigate and/or clarify noise, water resource, and cultural resources impacts described in the detailed comments. EPA rates this DEIS as EC-2 i.e., EPA has "Environmental Concerns" and "Additional Information" is requested. EPA's rating system can be found online at: <http://www.epa.gov/oecaerth/nepa/comments/ratings.html>. Appropriate best management practices should be implemented and described in the FEIS to ensure that impacts to sensitive aquatic resources or species are minimized and/or appropriately mitigated. EPA should be consulted, when appropriate.

EPA appreciates the opportunity to review the proposed DEIS. If you have any questions regarding our comments, please contact Ntale Kajumba (404/562-9620) of my staff.

Sincerely,



Heinz J. Mueller, Chief  
NEPA Program Office  
Office of Environmental Accountability

Attachment:  
EPA Detailed Comments

## **EPA Detailed Comments on the DEIS for I- 10 Mobile Bridge and Bayway**

### ***Air Quality***

The DEIS provides a description of the air impacts that will result from both the construction and the use of the bridges proposed in the project. Mobile and Baldwin are currently in attainment for all National Ambient Air Quality Standards (NAAQS). The DEIS also includes a Carbon Monoxide hotspot analysis despite the areas attainment status. For section 4.17.3, Air Quality Impacts, during Construction, EPA recommends that the project implement overall diesel emission reduction activities through various measures such as: switching to cleaner fuels, retrofitting current equipment with emission reduction technologies, repowering older engines with newer cleaner engines, replacing older vehicles, and reducing idling through operator training and/or contracting policies. EPA can assist in the future development or implementation of these options.

### ***Historic Resources***

Based on the DEIS, concerns remain regarding impacts to cultural resources in Mobile. Cultural resource surveys were conducted and an initial determination of no adverse indirect effects on cultural resources has been made. The preferred alternative is the alternative that is furthest away from historic resources in downtown Mobile and avoids direct impacts to Old Union Hall, which is eligible for the National Register of Historic Places and avoids the use of a Section 4(f) resource. However, EPA notes that FHWA and ALDOT continues to consult with the State Historic Preservation Officer (SHPO) and Section 106 Consulting Parties regarding their persistent concerns and ALDOT will need to conduct additional archeological surveys on some of the alternatives. EPA recommends that the FEIS should document the results of the consultation process and any remaining survey results.

### ***Water Resources and Water Quality***

In section 4.12, Water Quality and Biological Resources, the DEIS indicates that lower congestion rates and reduced low-speed and idled traffic are likely to result in decreased leaks of oil and combustion-related pollutants because inefficient combustion at low speeds would be reduced. However, the DEIS should also consider that a widened Bayway is also likely to result in an increased volume of traffic, increasing the number of emission sources contributing to emission-related products in runoff to Mobile Bay and River.

Section 4.12.4 states, "Calculations of wetland impacts have been derived based on the structural dimensions and projected as ground impacts," and refers to Figures 8a and 8b. EPA recommends that the text provide quantified impacts. In addition, the rationale for the "wetlands avoided" area marked on Figure 8a and 8b is unclear. The DEIS should indicate why the wetland avoided areas are much wider than the proposed expanded Bayway.

Section 4.12.4.6 indicates that "Further discussion of mitigation alternatives will be conducted with ADEM, USACE, USFWS, and NMFS during development of the mitigation plan." "The draft and final mitigation plans will be developed in consultation with the USACE, USFWS, NMFS, ADEM, and local agencies, as appropriate." EPA has a critical role in reviewing compensatory mitigation proposals. ALDOT should also consult with EPA.

The DEIS indicates that construction would occur across impaired waterbodies, the Mobile River, Mobile to Spanish River, Joes's Branch from it source to D' Olive Creek, and D'Olive Creek from its source to D'Olive Bay. Joe's Branch and D'Olive Creek are both listed for siltation. Section 4.17, Construction Impacts, states that, "Best Management Practices will be utilized to control sedimentation and stormwater runoff during construction." EPA requests that additional information be provided for review in the Final EIS.

Section 4.23 Environmental Commitments states re: Wetland and SAV Surveys: "Appropriate mitigation measures will be developed in consultation with resource agencies including US Corps of Engineer (USCOE), USFWS, NMFS, and ADEM." EPA has a critical role in reviewing compensatory mitigation proposals. Therefore, we recommend that ALDOT should consult with EPA, as well.

Section 6.0 makes no mention of EPA as an agency that should be or has been coordinated with.

### ***Lighting***

Section 4.15, Lighting, describes directing lighting such that it does not fall outside of the right of ways. EPA recommends also considering how to limit light pollution that may affect wildlife, such as shielding light that would be directed upward, disrupting owls and other birds active at dusk and night. Also, lighting that would shine over water such that it could disrupt aquatic species activities should be minimized. All of the above should be addressed for construction and permanent lighting.

### ***Noise***

Section ES-10, Traffic Noise Impacts, indicates that 1,065 noise-sensitive receptor are within the vicinity of the build alternatives. The preferred alternative may result in noise impacts to 275 receptors. These site either approach or exceed the noise abatement criteria. Although noise barriers were evaluated as part of the EIS, no noise minimization strategies are proposed. Given the magnitude of the impacts, EPA recommends that the FEIS include viable strategies to minimize noise related impacts to local communities.

### ***Environmental Justice (EJ)***

EPA notes the inclusion of environmental justice (EJ) as part of the assessment process. The DEIS indicates that there are census blocks (11, 12 and 13.02 and 15.02) in and around the Central Texas Neighborhood with higher minority and low-income populations. In addition, there are two minority citizen organizations - the Central Texas Neighborhood Association and the Down the Bay Community Organization. The preferred alternative avoids relocating minority residents, but the closing to the Texas Street interchange will be inconvenient for residents that use this interchange, and the high noise levels currently experienced by residents in the Central Texas Neighborhood will increase slightly. The DEIS identifies an alternate interchange and provides information regarding various public engagement and outreach activities within the affected communities. EPA notes that some of those targeted outreach events occurred over ten years ago. We recommend that the EJ section of the FEIS summarize the existing noise levels in the EJ communities and summarize any concerns expressed by the community in relationship to the proposed project.

### ***Pedestrian and Bicycle Facilities***

While the need for additional vehicle capacity has been clearly demonstrated, EPA strongly encourages the implementation of a “Complete Streets” design in order to provide citizens—within the project limits—with safe and user-friendly facilities to support transit, bicycle, and pedestrian modes of transportation for accessing places along the corridor. These accommodations could also help reduce Mobile Source Air Toxics (MSATs). A recently-published study by the Transportation Research Board concluded that buffered bicycle lanes encourage cyclists to ride outside the door zone (see Recommended Bicycle Lane Widths for Various Roadway Characteristics (NCHRP Report 766) and Bicycles 2013: Planning, Design, Operations, and Infrastructure (Journal of the TRB, No. 2387)). EPA encourages the inclusion of this scientific research in developing appropriate urban roadway design. EPA notes ALDOT’s commitment to providing pedestrian access across the Mobile River. The DEIS states that this may be via the Cochran Bridge or the Bankhead Tunnel. EPA also notes that the public was also interested in having access to the proposed I-10 Mobile Bridge via bicycle. We encourage ongoing coordination with the appropriate stakeholders to ensure that an appropriate solution is reached.



# United States Department of the Interior



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**Office of Environmental Policy and Compliance**  
Richard B. Russell Federal Building  
75 Spring Street, S.W., Suite 1144  
Atlanta, Georgia 30303

ER 14/0670  
9043.1

December 1, 2014

William F. Adams  
State Design Engineer  
Alabama department of Transportation  
1409 Coliseum Boulevard  
Montgomery AL 36110

Attention: Alfredo Acoff

Re: Comments and Recommendations on the Draft Environmental Impact Statement (DEIS)/Section 4(f) Evaluation for I-10 Mobile River Bridge and Bayway Widening Project No. DPI-0030(005)

Dear Mr. Adams:

The Department of the Interior (Department) has reviewed the Draft Environmental Impact Statement (DEIS)/Section 4(f) Evaluation for I-10 Mobile River Bridge and Bayway Widening Project No. DPI-0030(005). The Department offers the following comments.

## **Section 4(f) Evaluation Comments**

The DEIS/Section 4(f) evaluation describes a range of avoidance alternatives and describes the affected Section 4(f) resources.

The DEIS/Section 4(f) evaluation discusses an ongoing coordination effort with the Alabama State Historical Commission (SHPO) in compliance with Section 106 of the National Historic Preservation Act. Based on the information provided in the DEIS/Section 4(f) evaluation, the preferred alternative would adversely affect one known National Register of Historic Places (NRHP) eligible resource within the Area of Potential Effect. This resource has been identified as archaeological site 1MB412. The SHPO has also requested that where landowner permission was not acquired for the Phase 1 archeological survey, that additional testing be conducted when a route has been finalized and the associated property has been acquired.



The identified preferred alternative would adversely affect historic properties listed on or eligible for the NRHP and identified as National Historic Landmarks (NHL). Visual adverse effects were identified by the SHPO for the Church Street East Historic District, Lower Dauphin Street Historic District, and the Union Hall. Visual adverse effects were also identified by the SHPO for one NHL; the Old Southern Market and City Hall. Access adverse impacts were identified for one NHL; the USS Alabama Memorial park.

As noted in the DEIS/Section 4(f) evaluation, additional opportunities to assist in developing strategies to avoid, minimize, or mitigate impacts will be provided to the agencies that have been granted Section 106 Consulting Party status by regulation. Section 106 consultation with the SHPO is ongoing.

In addition, the Department submits the following comments in accordance with the National Environmental Policy Act of 1969 (83 Stat. 852, as amended; 42 U.S.C. 4321 et seq.), the Endangered Species Act (ESA) of 1973 (87 Stat. 884, as amended; 16 U.S.C. 1531 et seq.), and the Fish and Wildlife Coordination Act (48 Stat. 401, as amended; 16 U.S.C. 661 et seq.).

The United States Fish and Wildlife Service (USFWS) entered into Endangered Species Act, Section 7 formal consultation for this proposal in 2003 and issued a biological opinion for identified impacts to the Alabama redbelly turtle and the Gulf sturgeon. Since that time another species, the West Indian manatee (*Trichechus manatus*) has become more prevalent in Mobile Bay and should be considered in an additional consultation. USFWS has been in contact with the Federal Highway Administration (FHWA) Alabama Division and recommended re-initiation of formal section 7 consultation based on new data. USFWS received FHWA's request, dated September 15, 2014, on October 3, 2014. USFWS has requested FHWA provide a biological assessment for the anticipated impacts to the West Indian manatee.

## Summary

The Department does not concur with Section 4(f) approval of this project at this time. We would be pleased to reconsider this position upon completion of the Section 106 process and the resubmission of the final Section 4(f) evaluation. We have a continuing interest in working with (FHWA and the Alabama Department of Transportation (ALDOT) to ensure that impacts to resources of concern to the Department are adequately addressed.

Thank you for the opportunity to provide comments. If you have questions, I can be reached at (404) 331-4524 or via email at [joyce\\_stanley@ios.doi.gov](mailto:joyce_stanley@ios.doi.gov).

Sincerely,



Joyce Stanley

Regional Environmental Protection Specialist

cc:

Christine Willis - USFWS

Gary Lecain - USGS

Anita Barnett – NPS

Robin Ferguson – OSRME

OEPC - WASH

SECTION	INFO	ACTION	FILE
REGIONAL ENGINEER			
ASST REGION ENGINEER			
ADMINISTRATION			
OPERATIONS-MOBILE			
OPERATIONS-GROVE HILL			
CONSTRUCTION			
COUNTY TRANSPORTATION			
EQUIPMENT			
EEO			
MATERIALS			
PRE-CONSTRUCTION			
SPECIAL PROJECTS			
DISTRICT MANAGERS			

January 18, 2018

Vince Calametti, P.E., Regional Engineer  
Alabama Department of Transportation  
Region 9<sup>th</sup> Division  
1701 West I-65 Service Rd, West  
Mobile, AL 36618-1109

Subject: Proposed I-10 Bridge

Dear Mr. Calametti:

Happy New Year and thanks for all your hard work. Since I do not believe the public comment period for the Environmental Impact Statement (EIS) has opened and it is now known the Alabama Department of Transportation (ALDOT) must build a new more elevated bayway, could we modify the current plans and build the new I-10 Bridge and Bayway south of the planned route (Attachment 1)? If we follow the more southern route, the existing I-10 could be designated the I-210 for predominately local Mobile traffic with heavier use during high traffic events, maintenance and wrecks. There would be enhanced traffic flow across the Mobile River and Mobile Bay with five lanes of interstate quality road going each direction and save the tax payers \$200-300 million which is an estimated cost to demolish the current I-10 Bayway.

I admit I'm not an engineer but the redundancy of five lanes going east and west will make Mobile a safer, healthier community and provide more opportunity for economic growth in both Mobile/Baldwin Counties. The current river front property owned by ALDOT may be more valuable to the maritime industry bringing in more high paying jobs. Trucks leaving the Alabama Port on Virginia Street could take the "Airbus Route" to Brookley and go north on the new Michigan Ave. to I-10 and then turn east to cross the new bridge. Also the slope of the bridge could be less on both sides, since there would be more length to rise and fall. Maybe the Alabama Port Authority will consider trading the ALDOT owned property for an easement across the proposed southern route or purchase it for your original investment cost. The Mobile Chamber of Commerce may be able to provide insight into the future of the coal industry, but I believe current data predicts a steady decline.

Is a 250% increase in crossing capacity better than 150% while saving hundreds of millions of dollars?

Sincerely,

Bernard H. Eichold II, M.D., Dr.P.H., F.A.C.P.  
Health Officer

BHE:vw

cc: Mr. John R. Cooper, Transportation Director, ALDOT  
Mayor Sandy Stimpson, Mobile  
Mr. Kevin Harrison, South Alabama Regional Planning Commission







Google earth  
ATTACHMENT I  
Eye alt: 0.67 mi

est 51,852 ft.

Image © 2012 TerraMetrics

30° 39' 36.82" N 87° 59' 59.46" W elev: -4 ft

Imagery Date: 11/3/2012